

ORDINANCE O-4922

AN ORDINANCE OF THE CITY OF KIRKLAND RELATING TO PLANNING, ZONING, AND LAND USE RELATED TO DESIGN REVIEW AND AMENDING THE CITY OF KIRKLAND ZONING CODE, INCLUDING REPEALING CHAPTER 92 IN ITS ENTIRETY AND REPLACING IT WITH A NEW CHAPTER 92; REPEALING CHAPTER 142 IN ITS ENTIRETY AND REPLACING IT WITH A NEW CHAPTER 142; AMENDING VARIOUS OTHER ZONING CODE PROVISIONS FOR CONSISTENCY WITH THESE NEW ZONING CODE CHAPTERS; ADOPTING REVISED DESIGN GUIDELINES FOR PEDESTRIAN ORIENTED BUSINESS DISTRICTS, NE 85TH STREET STATION SUBAREA PLAN, TOTEM LAKE BUSINESS DISTRICT, AND YARROW BAY BUSINESS DISTRICT; REPEALING DESIGN GUIDELINES FOR RESIDENTIAL DEVELOPMENTS; AND AMENDING SECTION 3.30.040 OF THE KIRKLAND MUNICIPAL CODE RELATED TO THESE DESIGN GUIDELINES.

1 WHEREAS, Washington State enacted House Bill (HB) 1293, codified at RCW  
2 36.70A.630, which requires that the City of Kirkland adopt and apply only clear and objective  
3 regulations to the exterior design of new development, that the design review process be  
4 conducted concurrently with the review and decision process for project permits, and that the  
5 design review process not include more than one public meeting; and  
6

7 WHEREAS, in order to comply with these requirements, the City must amend its  
8 existing design regulations contained in chapter 92 of the Kirkland Zoning Code (KZC), its  
9 existing design review processes and procedures contained in chapter 142 KZC, and its  
10 existing design review guidelines referenced in Section 3.30.040 of the Kirkland Municipal  
11 Code (KMC), and the City must further revise various other code provisions to address these  
12 changes and ensure consistency; and  
13

14 WHEREAS, on November 18, 2025, following public notice as required by law, the City  
15 Council held a public hearing to gather feedback on the proposed amendments to chapter 142  
16 KZC relating the the design review process and regarding the continued role of the existing  
17 Design Review Board (DRB); and  
18

19 WHEREAS, upon consideration of information received at the public hearing, City  
20 Council determined that because the required amendments to chapter 92 KZC will result in  
21 clear and objective design standards and regulations, the City's new design review process in  
22 chapter 142 KZC should require most projects to be reviewed administratively by the Planning  
23 Official but the DRB should continue to review projects through a new alternative compliance  
24 review process when an applicant requests significant design modifications; and  
25

26 WHEREAS, on December 11, 2025, following public notice as required by law, the City  
27 of Kirkland Planning Commission held a public hearing to gather feedback on the proposed  
28 design regulation amendments necessary to comply with HB 1293; and  
29

30 WHEREAS, Council has received a recommendation from the Kirkland Planning  
31 Commission to amend certain sections of the Kirkland Zoning Code, including a repeal and  
32 replacement of both chapter 92 KZC and chapter 142 KZC, as set forth in the report and  
33 recommendation of the Planning Commission; and  
34

35 WHEREAS, with regard to the City's Design Guidelines, each guideline includes a  
36 section stating that the Council shall consult with the Planning Commission prior to amending

37 them, so the Design Guidelines were included in the review by the Planning Commission and  
38 included in the Commission's recommendation to the City Council; and

39 WHEREAS, the City Council determines that some of the City's adopted Design  
40 Guidelines should be amended or repealed for consistency with the zoning code changes  
41 regarding design review, and, for ease of review and understanding, those amended design  
42 guidelines are shown in legislative change format with direction to the Director of the Planning  
43 and Building to produce final versions that incorporate the approved changes for ease of  
44 reference and future use; and

45  
46 WHEREAS, the City Council determines that KMC 3.30.040, which references and  
47 incorporates the City's Design Guidelines, should be amended to be consistent with the zoning  
48 code changes regarding design review; and

49  
50 WHEREAS, the amendments in this ordinance are exempt from review under the State  
51 Environmental Policy Act (SEPA) pursuant to Section 197-11-800(19) of the Washington  
52 Administrative Code (WAC); and

53  
54 WHEREAS, the City Council finds that the proposed amendments are with the  
55 Comprehensive Plan, bear a substantial relation to public health, safety, or welfare, and are in  
56 the best interest of the community members of Kirkland, and that they are further necessary  
57 and required to comply with state law, particularly RCW 36.70A.630.

58  
59 NOW, THEREFORE, the City Council of the City of Kirkland do ordain as follows:  
60

61 Section 1. Section 5.10.538 of the Kirkland Zoning Code (KZC) is hereby amended as  
62 follows, with the new text shown in underline, deletions shown in ~~strikethrough~~, and the  
63 intentional omission of unchanged sections or parts of tables indicated with three asterisks  
64 (\*\* \*); all other provisions of these sections remain unchanged and in full force, and these  
65 provisions for identifying changes apply throughout this ordinance.  
66

67 **5.10.538 Modulation**

68 ~~The recessing back or projecting forward of a portion of a building face or roof within specified~~  
69 ~~intervals of building width and depth, as a means of breaking up the apparent bulk of the~~  
70 ~~building's continuous exterior walls.~~

71  
72 Modulation refers to large scale massing arrangements such as upper-level setbacks and  
73 significant vertical offsets of the facade. Modulation affects the overall form and interior layout  
74 of the building.

75  
76 a. Horizontal Modulation: Horizontal modulation refers to projection or recessing of the  
77 facade in lateral segments, such as a street-level setback or an upper-level setback.  
78 Horizontal modulation helps to break up the height of a building.

79  
80 b. Vertical Modulation: Vertical modulation refers to projection or recessing of the facade  
81 in vertical segments, typically extending the entire height of the building, or the majority  
82 of the building. Vertical modulation helps to break up the length of a building.  
83

84 Section 2. KZC 30.20 is amended to read as follows:

85  
86 **30.20 Permitted Uses.**

87 \*\*\*

88 PU-41. A transit oriented development containing attached or stacked dwelling units or  
89 residential suites use in the PR 1.8 TOD zone within the Totem Lake Business District (TLBD)  
90 shall meet the following requirements:

91 Development must be part of a conceptual master plan (CMP) for the entire subject property.  
92 The proposed CMP shall be reviewed using the design review process provisions of KZC  
93 142.3530. Subsequent development proposals shall follow ~~DR or ADR~~ the process as set  
94 forth in the notice of approval for the conceptual master plan. The conceptual master plan  
95 shall ~~incorporate the design guidelines contained in the design guidelines for the Totem Lake~~  
96 ~~Business District and include the following:~~

97  
98 \*\*\*

99  
100 o. The ~~Design Review Board~~ Planning Official may approve variations of the above design  
101 standards if the proposal is consistent with the Totem Lake Business District Design  
102 Guidelines.

103  
104 \*\*\*

105  
106 Section 3. KZC 30.30 is amended to read as follows:

107  
108 **30.30 Density/Dimensions.**

109 \*\*\*

110  
111 DD 34. For transit oriented development containing attached or stacked dwelling units or  
112 residential suites use in a PR 1.8 TOD zone within the Totem Lake Business District (TLBD),  
113 the minimum required yards and where they are measured from shall be as follows:

- 114  
115 a. East: 20': The ~~Design Review Board~~ Planning Official may approve a reduction of the  
116 east required yard along 116th Way NE to 0' (zero feet) for portions of the structure  
117 where the street floor of the building contains:
- 118 1) Commercial use is designed with a pedestrian-oriented facade with direct access  
119 to 116th Way NE. Facade treatments shall include overhead weather protection;  
120 public spaces with seating, landscaping, and art; and transparent storefronts; or
  - 121 2) Residential uses or lobbies that incorporate front entries, porches, and stoops  
122 oriented to 116th Way NE.
- 123  
124 b. South: 10' along common property line with TOD.
- 125  
126 c. West: 50' See Special Regulation DS-15.
- 127  
128  
129 d. North: 20'.

130  
131  
132 \*\*\*

133

Section 4. KZC 35.10.020 is amended to read as follows:

**35.10.020 BN, BNA Zones.**

1. The following commercial frontage requirements shall apply to all development that includes dwelling units or assisted living uses:

- a. The street level floor of all buildings shall be limited to one or more of the following uses, except as allowed in subsection (1)(c) of this section: Retail; Restaurant or Tavern; Entertainment, Cultural and/or Recreational Facility; or Office. These uses shall be oriented toward fronting arterial and collector streets and have a minimum depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the building along the street).

~~The Design Review Board (or Planning and Building Director if not subject to DR)~~ Planning Official may approve a minor reduction in the depth requirements if the applicant demonstrates that the requirement is not feasible given the configuration of existing or proposed improvements and that the design of the commercial frontage will maximize visual interest. ~~The Design Review Board (or Planning and Building Director if not subject to DR)~~ Planning Official may modify the frontage requirement where the property abuts residential zones in order to create a more effective transition between uses.

\*\*\*

Section 5. KZC Section 35.10.040 is amended to read as follows:

**35.10.040 BCX Zones.**

\*\*\*

2. For development where the maximum height of the structure is 60 feet above average building elevation:

- a. Uses above the third story are limited to Assisted Living Facility and Attached or Stacked Dwelling Units.
- b. No portion of a building within 40 feet of NE 70th Place shall exceed a height of 60 feet above NE 70th Place measured at the midpoint of the frontage of the subject property on the NE 70th Place right-of-way. ~~The Design Review Board~~ Planning Official is authorized to allow rooftop amenities where this height limit is imposed.
- c. The development includes a Retail Establishment Selling Groceries and Related Items with a minimum 15,000 square feet of floor area. This requirement only applies if the subject property contains more than five acres. This use must provide for a full-service grocery store that sells food and supplies including but not limited to fresh fruits and vegetables; refrigerated and frozen foods; dairy products; canned goods; dry goods; fresh meats and cheeses; delicatessen items; beverages; nonfood items such as soaps, detergents, paper goods, and other household products; and health and beauty aids. ~~The Planning Official and Building Director~~ is authorized to allow a decrease in the square footage to not less than 10,000 square feet if the applicant demonstrates that a smaller space supports a viable full-service grocery store with all required components.

- 184 d. For all building facades facing and within 100 feet of the abutting right-of-way, all  
 185 portions of a structure greater than three stories in height, as measured from the  
 186 abutting right-of-way, shall be stepped back from the third story facade as follows:  
 187 1) From NE 70th Place and that portion of 132nd Avenue NE across from Snyder's  
 188 Corner Park by an average of eight feet;  
 189 2) From 130th Avenue NE, and NE 65th Street, and that portion of 132nd Avenue NE  
 190 south of Snyder's Corner Park by an average of 12 feet;  
 191 3) The required upper story step backs for all floors above the third story shall be  
 192 calculated as total upper story step back area as follows: Total Upper Story Step  
 193 Back Area = (Linear feet of front property line(s), not including portions of the site  
 194 without buildings that are set aside for vehicular areas) x (Required average step  
 195 back) x (Number of stories proposed above the third story). The ~~Design-Review~~  
 196 ~~Board~~ Planning Official is authorized to allow rooftop amenities within the step back  
 197 area.  
 198 4) The Planning Official is authorized to allow a reduction of the required upper story  
 199 step back by no more than five feet subject to the following:  
 200 a) Each square foot of additional building area proposed within the setback is  
 201 offset with an additional square foot of public open space (excluding area  
 202 required for sidewalk dedication) at the street level.  
 203 b) The public open space is located along the sidewalk frontage and is not  
 204 covered by buildings.  
 205 c) The design and location is consistent with applicable design regulations.  
 206  
 207 e. The development shall provide publicly accessible pedestrian oriented open space(s)  
 208 adjacent to the street or through-block pathway. The publicly accessible space(s) shall  
 209 contain a minimum of 1,000 square feet or one square foot per 200 gross square feet  
 210 of above grade building area, whichever is greater. The size calculation shall not  
 211 include the required width of abutting sidewalks or pathways. Locations, dimensions,  
 212 features and improvements (such as plazas, seating, public art, children's recreation  
 213 space) shall be reviewed and approved through by the ~~Design-Review-Board~~ Planning  
 214 Official based on applicable design guidelines.  
 215  
 216 f. Development at the corner of NE 70th Place and 132nd Avenue NE shall provide a  
 217 gateway feature, such as public art. The gateway feature shall be reviewed and  
 218 approved by the ~~Design-Review-Board~~ Planning Official based on applicable design  
 219 guidelines regulations.  
 220  
 221 g. Developments creating four or more new dwelling units shall provide at least 10  
 222 percent of the units as affordable housing units as defined in Chapter 5 KZC. See  
 223 Chapter 112 KZC for additional affordable housing incentives and requirements.  
 224  
 225 h. Development shall be designed, built and certified to achieve or exceed the high  
 226 performing building standards described in KZC 115.62.  
 227  
 228 i. The commercial floor shall be a minimum of 13 feet in height.  
 229  
 230 j. Except along NE 65th Street and 130th Avenue NE, residential uses, assisted living  
 231 uses, and parking for those uses shall not be located on the street level floor unless an  
 232 intervening commercial frontage is provided between the street and those other uses  
 233 or parking subject to the standards above. The intervening commercial frontage shall

234 have a minimum depth of 20 feet and an average depth of at least 30 feet (as measured  
 235 from the face of the building). The ~~Design Review Board (or Planning and Building~~  
 236 ~~Director if not subject to Design Review)~~ Planning Official may approve a minor  
 237 reduction in the depth requirements if the applicant demonstrates that the requirement  
 238 is not feasible given the configuration of existing or proposed improvements and that  
 239 the design of the commercial frontage will maximize visual interest.  
 240

- 241 k. Development shall contain commercial uses placed along and oriented to adjoining  
 242 arterials and through-block pathways. The location and frontages of these commercial  
 243 uses shall be reviewed through Design Review process for consistency with applicable  
 244 guidelines or regulations by the Planning Official. The Planning Official can approve a  
 245 modification of this requirement if it is determined that the commercial frontage would  
 246 be in a location where less successful commercial activity may occur.  
 247
- 248 l. Development shall provide for one north-south through-block pathway connection  
 249 between NE 70th Place and NE 65th Street and two east-west through-block pathway  
 250 connections between 130th Avenue NE and 132nd Avenue NE (see Plate 34Q). The  
 251 ~~Design Review Board~~ Planning Official shall determine the final location and  
 252 configuration of the through-block pathway connections based on convenience and  
 253 utility for nonmotorized access and orientation toward commercial uses and  
 254 pedestrian-oriented open space.  
 255
- 256 m. Lobbies and amenity space for residential or assisted living uses may be allowed within  
 257 the commercial frontage provided they do not exceed 20 percent of the building's linear  
 258 retail frontage along the street or through-block pathway. The ~~Design Review Board (or~~  
 259 ~~Planning and Building Director if not subject to Design Review)~~ Planning Official may  
 260 approve a minor increase to ground floor residential lobbies and amenities if they are  
 261 connected to retail use and the design of the ground floor frontage will maximize visual  
 262 interest.  
 263

264 Section 6. KZC 35.10.050 is amended to read as follows:  
 265

266 **35.10.050 FHNC Zone.**

- 267 1. The following commercial frontage requirements shall apply to all development that  
 268 includes dwelling units or assisted living uses:  
 269 a. The street level floor of all buildings shall be limited to one or more of the following  
 270 uses, except as allowed in subsection (1)(c) of this section:  
 271 Retail; Restaurant or Tavern; Entertainment, Cultural and/or Recreational Facility; or  
 272 Office. These uses shall be oriented toward fronting streets and have a minimum depth  
 273 of 20 feet and an average depth of at least 30 feet (as measured from the face of  
 274 the building along the street).  
 275
- 276 The ~~Design Review Board (or Planning and Building Director if not subject to DR)~~  
 277 Planning Official may approve a minor reduction in the depth requirements if  
 278 the applicant demonstrates that the requirement is not feasible given the configuration  
 279 of existing or proposed improvements and that the design of the commercial frontage  
 280 will maximize visual interest.  
 281
- 282 b. The commercial floor shall be a minimum of 15 feet in height.  
 283

284  
285  
286  
287  
288  
289  
290  
291  
292  
293  
294  
295  
296  
297  
298  
299  
300  
301  
302  
303  
304  
305  
306  
307  
308  
309  
310  
311  
312  
313  
314  
315  
316  
317  
318  
319  
320  
321  
322  
323  
324  
325  
326  
327  
328  
329  
330  
331  
332  
333  
334

c. Other uses allowed in this zone and parking shall not be located on the street level floor unless an intervening commercial frontage is provided between the street and those other uses or parking subject to the standards above. Lobbies for residential or assisted living uses are allowed within the commercial frontage provided they do not exceed 20 percent of the building's linear commercial frontage along the street.

2. Maximum height of structure is as follows:

- a. Thirty-five feet above average building elevation;
- b. Fifty-five feet above the midpoint of the subject property on the abutting right-of-way, if:
  - 1) The subject property contains a minimum of five acres and any development includes a grocery store with a minimum 20,000 square feet of floor area.
  - 2) Office uses are only allowed on the ground floor and second floor of any structure.
  - 3) For all building facades facing and within 100 feet of Juanita Drive or NE 141st Street, all portions of a structure greater than two stories in height shall be stepped back from the second story building facade by an average of 20 feet. The required upper story step backs for all floors above the second story shall be calculated as Total Upper Story Step Back Area as follows: Total Upper Story Step Back Area = (Linear feet of front property line(s), not including portions of the site without buildings that are set aside for vehicular areas) x (Required average step back) x (Number of stories proposed above the second story). The Design Review Board Planning Official is authorized to allow rooftop garden structures within the step back area.

\*\*\*

Section 7. KZC 35.10.060 is amended to read as follows:

**35.10.060 HENC 1, HENC 3 Zones.**

1. In the HENC 1 and 3 zones:

- a. At least 75 percent of the total gross floor area located on the ground floor of all structures on the subject property must contain retail establishments, restaurants, taverns, hotels or motels, or offices. These uses shall be oriented to a pedestrian-oriented street, a major pedestrian sidewalk, a through-block pathway or the Cross Kirkland Corridor.
- b. Adjacent to NE 68th Street, 106th Avenue NE, 108th Avenue NE, 6th Street South and the Cross Kirkland Corridor (CKC), any portion of a structure greater than two stories in height must be stepped back from the facade below by an average of 15 feet with a minimum step back of five feet.

The Design Review Board Planning Official is authorized to allow rooftop deck and/or garden structures within the step back area.

The Planning Official is authorized to allow a reduction of the required upper story step back by no more than five feet subject to the following:

- 1) Each square foot of additional building area proposed within the setback is offset with an additional square foot of public open space (excluding area required for sidewalk dedication) at the street level.
- 2) The public open space is located along the sidewalk frontage and is not covered by buildings.

335 3) The design and location is consistent with applicable design regulations.  
 336

- 337 c. Development adjoining the Cross Kirkland Corridor shall comply with the standards  
 338 of KZC 115.24. Safe public pedestrian connections through sites to the Cross  
 339 Kirkland Corridor are required (for approximate locations see Plate 34O).  
 340  
 341 d. Minimum 14-foot-wide sidewalks are required along 106th Avenue NE, 108th  
 342 Avenue NE and 6th Street South on the side of the right-of-way that abuts HENC  
 343 1; and on both sides of NE 68th Street.  
 344  
 345 e. Drive-in and drive-through facilities are allowed for gas stations and drug stores. All  
 346 other drive-in and drive-through facilities are prohibited.  
 347

348 2. In the HENC 1 zone:

- 349 a. No more than 20 percent of the gross floor area for any building may include office  
 350 uses. This requirement does not apply to the area in HENC 1 that is located north  
 351 of NE 68th Street between the Cross Kirkland Corridor and what would be the  
 352 northern extension of 106th Avenue NE.  
 353  
 354 b. Structure height may be increased to 35 feet above ABE if:  
 355 1) A development of four acres or less includes at least one grocery store,  
 356 hardware store, or drug store containing a minimum of 20,000 square feet of  
 357 gross floor area.  
 358 2) A development of more than four acres includes at least one grocery store,  
 359 hardware store, or drug store containing a minimum of 20,000 square feet of  
 360 gross floor area and one grocery store, hardware store, or drug store containing  
 361 a minimum of 10,000 square feet of gross floor area.  
 362 3) The site plan is approved by the ~~Design Review Board~~ Planning Official and  
 363 includes public gathering places and community plazas with public art. At least  
 364 one of these public areas must measure a minimum of 1,500 square feet with  
 365 a minimum width of 30 feet.  
 366

367 \* \* \*

368  
 369 Section 8. KZC 50.10 is amended to read as follows:  
 370

371 **50.10 General Regulations – CBD 1 Zones.**

372 The following regulations apply to all uses in this zone unless otherwise noted:  
 373

- 374 1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the  
 375 subject property.  
 376  
 377 2. The maximum height of structure shall be measured at the midpoint of the frontage of the  
 378 subject property on the abutting right-of-way, excluding First Avenue South. See KZC 50.62  
 379 for additional building height provisions.  
 380  
 381 3. Except along alleys and similar service access streets, the street level floor of  
 382 all buildings shall be limited to one or more of the following uses: Retail; Restaurant or Tavern;  
 383 Banking and Related Financial Services; Entertainment, Cultural and/or Recreational Facility;  
 384 Parks; Government Facility; or Community Facility. The street level floor of buildings south of  
 385 Second Avenue South may also include Office Use. The required uses shall have a minimum  
 386 depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the  
 387 building on the abutting right-of-way, not including alleys and similar service access streets).

388 Buildings proposed and built after April 1, 2009, and buildings that existed prior to April 1, 2009,  
 389 which are at least 10 feet below the maximum height of structure, shall have a minimum depth  
 390 of 10 feet and an average depth of at least 20 feet containing the required uses listed above.  
 391

392 ~~The Design Review Board (or Planning and Building Director if not subject to D.R.)~~ Planning  
 393 Official may approve a minor reduction in the depth requirements if the applicant demonstrates  
 394 that the requirement is not feasible given the configuration of existing or proposed  
 395 improvements and that the design of the retail frontage will maximize visual interest. Lobbies  
 396 for residential, hotel, and office uses may be allowed within ~~this~~ the required retail space  
 397 subject to applicable design guidelines, provided that the street frontage of the lobby is limited  
 398 to 20 percent of the building linear frontage along the street. The Planning Official may approve  
 399 a minor increase to ground floor residential lobbies and amenities if they are connected to retail  
 400 use and the design of the ground floor frontage will maximize visual interest.  
 401

402 4. Where public improvements are required by Chapter 110 KZC, sidewalks on pedestrian-  
 403 oriented streets within CBD 1A and 1B shall be as follows:  
 404

405 Sidewalks shall be a minimum width of 12 feet. The average width of the sidewalk along the  
 406 entire frontage of the subject property abutting each pedestrian-oriented street shall be 13  
 407 feet. The sidewalk configuration shall be approved through D.R.  
 408

409 5. Upper story setback requirements are listed below. For purposes of the following  
 410 regulations, the term "setback" shall refer to the horizontal distance between the property line  
 411 and any exterior wall of the building. The measurements shall be taken from the property line  
 412 abutting the street prior to any potential right-of-way dedication.

- 413 a. **Lake Street:** No portion of a building within 30 feet of Lake Street may exceed a  
 414 height of 28 feet above Lake Street except as provided in KZC 50.62.
- 415 b. **Central Way:** No portion of a building within 30 feet of Central Way may exceed a  
 416 height of 41 feet above Central Way except as provided in KZC 50.62.
- 417 c. **Third Street and Main Street:** Within 40 feet of Third Street and Main Street, all  
 418 stories above the second story shall maintain an average setback of at least 10 feet  
 419 from the front property line.
- 420 d. **All other streets:** Within 40 feet of any front property line, other than Lake Street,  
 421 Central Way, Third Street, or Main Street, all stories above the second story shall  
 422 maintain an average setback of at least 20 feet from the front property line.
- 423 e. The required upper story setbacks for all floors above the second story shall be  
 424 calculated as Total Upper Story Setback Area as follows:  
 425

426 Total Upper Story Setback Area = (Linear feet of front property line(s), not including  
 427 portions of the site without buildings that are set aside for vehicular areas) x  
 428 (Required average setback) x (Number of stories proposed above the second  
 429 story). See Plate 35.

- 430 f. ~~The Design Review Board~~ Planning Official is authorized to allow a reduction of the  
 431 required upper story setback by no more than five feet subject to the following:  
 432 1) Each square foot of additional building area proposed within the setback is  
 433 offset with an additional square foot of public open space (excluding area  
 434 required for sidewalk dedication) at the street level.
- 435 2) The public open space is located along the sidewalk frontage and is not  
 436 covered by buildings.
- 437 3) For purposes of calculating the offsetting square footage, along Central  
 438 Way, the open space area at the second and third stories located directly  
 439 above the proposed ground level public open space is included. Along all

440  
441  
442  
443  
444  
445  
446  
447  
448  
449  
450  
451  
452

other streets, the open space area at the second story located directly above the proposed ground level public open space is included.

4) The design and location is consistent with applicable design guidelines regulations.

g. The Design Review Board Planning Official is authorized to allow rooftop garden structures within the setback area.

6. May also be regulated under the Shoreline Master Program; refer to Chapter 83 KZC.

Section 9. KZC 50.35.070 is hereby amended to read as follows:

**50.35 Use Zone Chart – CBD 5 Zones.**

Section 50.35	USE	REGULATIONS	DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS										
			Required Review Process	Lot Size	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
					REQUIRED YARDS (See Ch. 115)			Lot Coverage	Height of Structure				
				Front	Side	Rear							
***													
.070	Office Use	D.R., Chapter 142 KZC	None	20'	0'	0'	80%	67' above average building elevation. See Spec. Reg. 3.	C	D	One per each 350 sq. ft. of gross floor area.	1. Ancillary assembly and manufacture of goods on premises may be permitted as part of an office use if: a. The ancillary assembled or manufactured goods are subordinate to and dependent on this office use; and b. The outward appearance and impacts of this office use with ancillary assembly and manufacturing activities must be no different from other office uses. 2. The following regulations apply to veterinary office only: a. May only treat small animals on the subject property. b. Outside runs and other outside facilities for the animals are not permitted. c. Site must be designed so that noise from this use will not be audible off the subject property. A certification to this effect, signed by an Acoustical Engineer,	

Section 50.35	USE ↓	REGULATIONS ↓	DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS									
			Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
				Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage				
Front	Side	Rear										
												<p>must be submitted with the D.R. and building permit applications.</p> <p>d. A veterinary office is not permitted if the subject property contains dwelling units.</p> <p>3. For property adjoining Peter Kirk Park, 80 feet above average building elevation, if following is provided:</p> <p>a. At least 50 percent of gross floor area is office use.</p> <p>b. A minimum 54-foot-wide improved easement street from Parkplace to Kirkland Way which meets the requirements for the Park Promenade in the Parkplace Master Plan. The design must be approved by the Planning and Building Department and the Public Works Department.</p> <p>c. The street level of all buildings shall be limited to one or more of the following uses: Retail; Restaurant or Tavern; Banking and Related Financial Services; Entertainment, Cultural and/or Recreational Facility; Parks; Government Facility; or Community Facility. At least one of the street level tenant spaces must be a minimum area of 9,000 square feet. The required uses shall have a minimum depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the building on the Park Promenade).</p>

Section 50.35	USE & REGULATIONS	DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS									
		Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
			Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage				
Front	Side	Rear									
											<p>The Design Review Board (or Planning and Building Director if not subject to D.R.) Planning Official may approve a minor reduction in the depth requirements if the applicant demonstrates that the requirement is not feasible given the configuration of existing or proposed improvements and that the design of the retail frontage will maximize visual interest. Lobbies for office uses may be allowed within this space subject to applicable design guidelines regulations.</p> <p>d. A minimum 2,000-square-foot public plaza that relates to Kirkland Way, the Kirkland Performance Center and Peter Kirk Park. The design must be approved by the Design Review Board Planning Official.</p> <p>e. Public art on site valued at a minimum of \$10,000. The art must be reviewed and approved by the Kirkland Cultural Arts Commission.</p> <p>f. The project shall be designed, built and certified to achieve or exceed the high performing building standards described in KZC 115.62.</p>
***											

453  
454  
455

Section 10. KZC 50.35.100 is hereby amended to read as follows:

**50.35 Use Zone Chart – CBD 5 Zones.**

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS													
Section 50.35	USE Ø	REGULATIONS ↓	Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
				Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage					Height of Structure
					Front	Side	Rear						
***													
.100	Assisted Living Facility See Spec. Reg. 3.	D.R., Chapter 142 KZC	None	20'	0'	0'	80%	67' above average building elevation.	C	A	1.7 per independent unit. 1 per assisted living unit.	<p>1. A facility that provides both independent dwelling units and assisted living units shall be processed as an assisted living facility.</p> <p>2. A nursing home use may be permitted as part of an assisted living facility use in order to provide a continuum of care for residents. If a nursing home use is included, the following parking standard shall apply to the nursing home portion of the facility:</p> <p>a. One parking stall shall be provided for each bed.</p> <p>3. This use only allowed:</p> <p>a. On properties with frontage on Second Avenue.</p> <p>b. For property adjoining Peter Kirk Park, if the following are provided:</p> <p>1) A minimum 54-foot-wide improved easement street from Parkplace to Kirkland Way which meets the requirements for the Park Promenade in the Parkplace Master Plan. The design must be approved</p>	

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS													
Section 50.35	USE Ø	REGULATIONS	Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
				Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage					Height of Structure
					Front	Side	Rear						
											by the Planning and Building Department and the Public Works Department.  2) The street level of all buildings shall be limited to one or more of the following uses: Retail; Restaurant or Tavern; Banking and Related Financial Services; Entertainment, Cultural and/or Recreational Facility; Parks; Government Facility; or Community Facility. At least one of the street level tenant spaces must be a minimum of 9,000 square feet. The required uses shall have a minimum depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the building on the Park Promenade). The Design Review Board (or Planning and Building Director if not subject to D.R.) Planning Official may approve a minor reduction in the depth requirements if the applicant demonstrates that the requirement is not feasible given the configuration of existing or proposed improvements and that the design of the retail frontage will maximize visual interest. Lobbies for residential uses		

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS													
Section 50.35	USE ↓	REGULATIONS ↓	Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
				Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage					Height of Structure
					Front	Side	Rear						
											<p>may be allowed within this space subject to applicable design guidelines regulations.</p> <p>3) A minimum 2,000-square-foot public plaza that relates to Kirkland Way, the Kirkland Performance Center and Peter Kirk Park. The design must be approved by the Design-Review Board-Planning Official.</p> <p>4) Developments creating four or more new dwelling units shall provide at least 10 percent of the units as affordable housing units as defined in Chapter 5 KZC. See Chapter 112 KZC for additional affordable housing incentives and requirements.</p> <p>5) Public art on site valued at a minimum of \$10,000. The art must be reviewed and approved by the Kirkland Cultural Arts Commission.</p> <p>6) The project shall be designed, built and certified to achieve or exceed the high performing building standards described in KZC 115.62.</p>		
***													

456  
457  
458  
459

Section 11. KZC 50.35.110 is hereby amended to read as follows:

**50.35 Use Zone Chart – CBD 5 Zones**

Section 50.35	USE Ø	REGULATIONS Ø	DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS									
			Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
				Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage				
				Front	Side	Rear						
***												
.110	Stacked or Attached Dwelling Units  See Spec. Reg. 1.	D.R., Chapter 142 KZC	None	20'	0'	0'	80%	67' above average building elevation.	C	A	1.2 per studio unit.  1.3 per 1 bedroom unit.  1.6 per 2 bedroom unit.  1.8 per 3 or more bedroom unit.  See KZC 105.20 for visitor parking requirements.	1. This use only allowed:  a. On properties with frontage on Second Avenue.  b. For property adjoining Peter Kirk Park, if the following are provided:  1) A minimum 54-foot-wide improved easement street from Parkplace to Kirkland Way which meets the requirements for the Park Promenade in the Parkplace Master Plan. The design must be approved by the Planning and Building Department and the Public Works Department.  2) The street level of all buildings shall be limited to one or more of the following uses: Retail; Restaurant or Tavern; Banking and Related Financial Services; Entertainment, Cultural and/or Recreational Facility; Parks; Government Facility; or Community Facility. At least one of the street level tenant spaces must be a minimum of 9,000 square feet. The

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS													
Section 50.35	USE ↓	REGULATIONS ↓	Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
				Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage					Height of Structure
					Front	Side	Rear						
											<p>required uses shall have a minimum depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the building on the Park Promenade).</p> <p>The Design Review Board (or Planning and Building Director if not subject to D.R.) Planning Official may approve a minor reduction in the depth requirements if the applicant demonstrates that the requirement is not feasible given the configuration of existing or proposed improvements and that the design of the retail frontage will maximize visual interest. Lobbies for residential uses may be allowed within this space subject to applicable design guidelines regulations.</p> <p>3) A minimum 2,000-square-foot public plaza that relates to Kirkland Way, the Kirkland Performance Center and Peter Kirk Park. The design must be approved by the Design Review Board Planning Official.</p> <p>4) Developments creating four or more new dwelling units shall provide at least 10 percent</p>		

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS													
Section 50.35	USE ↓	REGULATIONS ↓	Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
				Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage					Height of Structure
					Front	Side	Rear						
											of the units as affordable housing units as defined in Chapter 5 KZC. See Chapter 112 KZC for additional affordable housing incentives and requirements.  5) Public art on site valued at a minimum of \$10,000. The art must be reviewed and approved by the Kirkland Cultural Arts Commission.  6) The project shall be designed, built and certified to achieve or exceed the high performing building standards described in KZC 115.62.		

460  
461  
462  
463  
464  
465  
466  
467  
468  
469  
470  
471  
472  
473  
474  
475  
476  
477  
478  
479  
480  
481  
482

Section 12. KZC 51.18 is hereby amended to read as follows:

**51.18 General Regulations – MSC 2 Zone.**

The following regulations apply to all uses in this zone unless otherwise noted:

1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the subject property.
2. For structures located within 30 feet of a parcel in a low density zone (or a low density use in PLA 17), KZC 115.136 establishes additional limitations on structure size.
3. Some development standards or design regulations may be modified as part of the design review process. See Chapters 92 and 142 KZC for requirements.
4. The following commercial frontage requirements shall apply to all development that includes dwelling units or assisted living uses:
  - a. The street level floor of all buildings shall be limited to one or more of the following uses: Retail; Restaurant or Tavern; Entertainment, Cultural and/or Recreational Facility; or Office. These uses shall be oriented toward Market Street and have a minimum depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the building along Market Street).

483 ~~The Design Review Board (or Planning and Building Director if not subject to D.R.)~~  
 484 Planning Official may approve a minor reduction in the depth requirements if the  
 485 applicant demonstrates that the requirement is not feasible given the configuration  
 486 of existing or proposed improvements and that the design of the commercial  
 487 frontage will maximize visual interest.  
 488

- 489 b. The commercial floor shall be a minimum of 13 feet in height. The height of the  
 490 structure may exceed the maximum height of structure by three feet for a three-  
 491 story building with the required 13-foot commercial floor.  
 492
- 493 c. Other uses allowed in this zone and parking shall not be located on the street level  
 494 floor unless an intervening commercial frontage is provided between the street and  
 495 those other uses or parking subject to the standards above. Lobbies for residential  
 496 or assisted living uses may be allowed within the commercial frontage provided  
 497 they do not exceed 20 percent of the building's linear commercial frontage along  
 498 Market Street.  
 499

500 5. Surface parking areas shall not be located between the street and building unless no  
 501 feasible alternative exists. Parking areas located to the side of the building are allowed;  
 502 provided, that the parking area and vehicular access occupies less than 30 percent of the  
 503 property frontage and design techniques adequately minimize the visibility of the parking.  
 504

505 6. Where Landscape Category B is specified, the width of the required landscape strip shall  
 506 be 10 feet and all other provisions of Chapter 95 KZC shall apply.  
 507

508 7. Developments may elect to provide affordable housing units as defined in Chapter 5 KZC  
 509 subject to the voluntary use provisions of Chapter 112 KZC.  
 510

511 Section 13. KZC 51.28 is hereby amended to read as follows:  
 512

513 **51.28 General Regulations – MSC 3 Zone.**

514 The following regulations apply to all uses in this zone unless otherwise noted:

- 515 1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the  
 516 subject property.  
 517
- 518 2. For structures located within 30 feet of a parcel in a low density zone (or a low density use  
 519 in PLA 17), KZC 115.136 establishes additional limitations on structure size.  
 520
- 521 3. Some development standards or design regulations may be modified as part of the design  
 522 review process. See Chapters 92 and 142 KZC for requirements.  
 523
- 524 4. The following commercial frontage requirements shall apply to all development that  
 525 includes dwelling units or assisted living uses:  
 526 a. The street level floor of all buildings shall be limited to one or more of the following  
 527 uses: Retail; Restaurant or Tavern; Hotel or Motel; Entertainment, Cultural and/or  
 528 Recreational Facility; or Office. These uses shall be oriented toward Market Street  
 529 and have a minimum depth of 20 feet and an average depth of at least 30 feet (as  
 530 measured from the face of the building along the street).  
 531

532 ~~The Design Review Board (or Planning and Building Director if not subject to D.R.)~~  
 533 Planning Official may approve a minor reduction in the depth requirements if the  
 534 applicant demonstrates that the requirement is not feasible given the configuration  
 535 of existing or proposed improvements and that the design of the commercial

536  
537  
538  
539  
540  
541  
542  
543  
544  
545  
546  
547  
548  
549  
550  
551  
552  
553  
554  
555  
556  
557  
558

frontage will maximize visual interest. The ~~Design Review Board (or Planning and Building Director if not subject to D.R.)~~ Planning Official may modify the frontage requirement where the property abuts residential zones in order to create a more effective transition between uses.

- b. The commercial floor shall be a minimum of 13 feet in height. The height of the structure may exceed the maximum height of structure by three feet for a three-story building with the required 13-foot commercial floor.
- c. Other uses allowed in this zone and parking shall not be located on the street level floor unless an intervening commercial frontage is provided between the street and those other uses or parking subject to the standards above. Lobbies for residential or assisted living uses may be allowed within the commercial frontage provided they do not exceed 20 percent of the building's linear commercial frontage along the street.

5. Surface parking areas shall not be located between the street and building unless no feasible alternative exists. Parking areas located to the side of the building are allowed provided that the parking area and vehicular access occupies less than 30 percent of the property frontage and design techniques adequately minimize the visibility of the parking.

Section 14. KZC 53.84.050 is hereby amended to read as follows:

**53.84 Use Zone Chart – RH 8 Zone.**

Section 53.84	USE	REGULATIONS	DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS										
			Required Review Process	Lot Size	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
					REQUIRED YARDS (See Ch. 115)			Lot Coverage	Height of Structure				
				Front	Side	Rear							
***													
.050	Stacked Dwelling Units See Spec. Reg. 1.	D.R., Chapter 142 KZC	None	10' adjacent to NE 85th St., otherwise 20'.	0'	15'	70%	30' above average building elevation. See Gen. Regs. 3 and 5.	A	A	1.2 per studio unit. 1.3 per 1 bedroom unit. 1.6 per 2 bedroom unit. 1.8 per 3 or more bedroom unit. See KZC 105.20 for visitor parking requirements.	1. At least 60% of the linear frontage of the property along NE 85th Street shall only include commercial use. The commercial use shall be at the street level floor and oriented toward NE 85th Street. Commercial uses shall have a minimum depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the building along the street). Stacked	

Section 53.84	USE ↓	REGULATIONS ↓	DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS									
			Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
				Lot Size	REQUIRED YARDs (See Ch. 115)			Lot Coverage				
Front	Side	Rear										
												<p>Dwelling Units are not permitted on the street level floor within 30 feet of the property line along NE 85th Street. The Planning and Building Director or Design Review Board Planning Official may approve a minor reduction in the depth requirements if the applicant demonstrates that the requirement is not feasible given the configuration of existing or proposed improvements and the design of the retail frontage will maximize visual interest.</p> <p>2. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use.</p>
***												

559  
560  
561  
562  
563  
564  
565  
566

Section 15. KZC 54.04 is hereby amended to read as follows:

**54.04 General Regulations – NRHBD 1A Zone.**

The following regulations apply to all uses in this zone unless otherwise noted:

1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the subject property.

- 567 2. In cases where the height of a structure is specified in number of stories, the following  
568 applies:
- 569 a. Height measured at the midpoint of the frontage of the subject property on the  
570 abutting right-of-way. If the site abuts more than one right-of-way, the applicant  
571 may select the right-of-way from which to measure.
- 572 b. The following heights per story are allowed:
- 573 1) Ground floor retail; ground floor restaurant and tavern; ground floor  
574 entertainment/cultural and/or recreational facility shall be a minimum of 13  
575 feet in height and a maximum of 15 feet.
- 576 2) Office; private club or lodge; faith-based facility; school; day-care center;  
577 public utility, government facility, or community facility; public park, ground  
578 floor hotel or motel; retail above the ground floor shall be a maximum of 13  
579 feet.
- 580 3) Residential; hotel or motel above the ground floor shall be a maximum of  
581 10 feet.
- 582 c. To determine the allowed height of a structure, determine the number of stories  
583 allowed in the use zone charts and apply the allowed height per story specified in  
584 subsection (2)(b) of this section. For example, if three stories are allowed and the  
585 proposed use is ground floor retail with two stories of residential above, the  
586 allowed height would be 35 feet.
- 587 d. Height shall be measured above the point of measurement (e.g., above average  
588 building elevation, or above right-of-way) as specified in the particular use zone  
589 charts. For purposes of measuring building height above the abutting right(s)-of-  
590 way, alleys shall be excluded.
- 591 e. In addition to the height exceptions established by KZC 115.60, the following  
592 exceptions to height regulations in NRHBD zones are established:
- 593 1) Decorative parapets may exceed the height limit by a maximum of four feet;  
594 provided, that the average height of the parapet around the perimeter of the  
595 structure shall not exceed two feet.
- 596 2) For structures with a peaked roof, the peak may extend eight feet above the  
597 height limit if the slope of the roof is equal or greater than four feet vertical  
598 to 12 feet horizontal.
- 600 3. The minimum required front yard is 10 feet, unless otherwise prescribed in the use zone  
601 chart. Ground floor canopies and similar entry features may encroach into the front yard;  
602 provided, the total horizontal dimension of such elements may not exceed 25 percent of the  
603 length of the structure. No parking may encroach into the required 10-foot front yard.
- 604
- 605 4. A pedestrian connection should be developed to link Slater Avenue NE with NE 116th  
606 Street.
- 607
- 608 5. The following commercial frontage requirements shall apply to all development that  
609 includes dwelling units or assisted living uses:
- 610 a. The street level floor of all buildings shall be limited to one or more of the following  
611 uses: Retail; Restaurant or Tavern; Hotel or Motel; Entertainment, Cultural and/or  
612 Recreational Facility; or Office. These uses shall be oriented toward NE 116th  
613 Street and 124th Avenue NE and have a minimum depth of 20 feet and an average  
614 depth of at least 30 feet (as measured from the face of the building along the  
615 street).
- 616

617 ~~The Design Review Board (or Planning and Building Director if not subject to D.R.)~~  
 618 Planning Official may approve a minor reduction in the depth requirements if the  
 619 applicant demonstrates that the requirement is not feasible given the configuration  
 620 of existing or proposed improvements and that the design of the commercial  
 621 frontage will maximize visual interest.  
 622

- 623 b. Other uses allowed in this zone and parking shall not be located on the street level  
 624 floor unless an intervening commercial frontage is provided between the street and  
 625 those other uses or parking subject to the standards above. Lobbies for residential  
 626 or assisted living uses may be allowed within the commercial frontage provided  
 627 they do not exceed 20 percent of the building's linear commercial frontage along  
 628 the street.  
 629

630 Section 16. KZC 54.10 is hereby amended to read as follows:  
 631

632 **54.10 General Regulations – NRHBD 1B Zone.**

633 The following regulations apply to all uses in this zone unless otherwise noted:

- 634 1. Refer to Chapter 1 KZC to determine what other provision of this code may apply to the  
 635 subject property.  
 636
- 637 2. In cases where the height of a structure is specified in number of stories, the following  
 638 applies:
- 639 a. Height measured at the midpoint of the frontage of the subject property on the  
 640 abutting right-of-way. If the site abuts more than one right-of-way, the applicant may  
 641 select the right-of-way from which to measure.  
 642
- 643 b. The following heights per story are allowed:
- 644 1) Ground floor retail; ground floor restaurant and tavern; ground floor  
 645 entertainment/cultural and/or recreational facility shall be a minimum of 13  
 646 feet in height and a maximum of 15 feet.  
 647 2) Office; private club or lodge; faith-based facility; school; day-care center;  
 648 public utility, government facility, or community facility; public park, ground  
 649 floor hotel or motel; retail above the ground floor shall be a maximum of 13  
 650 feet.  
 651 3) Residential; hotel or motel above the ground floor shall be a maximum of  
 652 10 feet.
- 653 c. To determine the allowed height of a structure, determine the number of stories  
 654 allowed in the use zone charts and apply the allowed height per story specified in  
 655 subsection (2)(b) of this section. For example, if three stories are allowed and the  
 656 proposed use is ground floor retail with two stories of residential above, the allowed  
 657 height would be 35 feet.
- 658 d. Height shall be measured above the point of measurement (e.g., above average  
 659 building elevation, or above right-of-way) as specified in the particular use zone  
 660 charts. For purposes of measuring building height above the abutting right(s)-of-  
 661 way, alleys shall be excluded.
- 662 e. In addition to the height exceptions established by KZC 115.60, the following  
 663 exceptions to height regulations in NRHBD zones are established:
- 664 1) Decorative parapets may exceed the height limit by a maximum of four feet;  
 665 provided, that the average height of the parapet around the perimeter of the  
 structure shall not exceed two feet.

- 666 2) For structures with a peaked roof, the peak may extend eight feet above the  
 667 height limit if the slope of the roof is equal or greater than four feet vertical  
 668 to 12 feet horizontal.  
 669
- 670 3. For structures located within 30 feet of a parcel in a low density zone (or a low density use  
 671 in PLA 17), KZC 115.136 establishes additional limitations on structure size.  
 672
- 673 4. The minimum required front yard is 10 feet, unless otherwise prescribed in the use zone  
 674 chart. Ground floor canopies and similar entry features may encroach into the front yard;  
 675 provided, the total horizontal dimension of such elements may not exceed 25 percent of the  
 676 length of the structure. No parking may encroach into the required 10-foot front yard.  
 677
- 678 5. The following commercial frontage requirements shall apply to all development that  
 679 includes dwelling units or assisted living uses:
- 680 a. The street level floor of all buildings shall be limited to one or more of the following  
 681 uses: Retail; Restaurant or Tavern; Hotel or Motel; Entertainment, Cultural and/or  
 682 Recreational Facility; or Office. These uses shall be oriented toward NE 116th  
 683 Street and 124th Avenue NE and have a minimum depth of 20 feet and an average  
 684 depth of at least 30 feet (as measured from the face of the building along the  
 685 street).  
 686
- 687 ~~The Design Review Board (or Planning and Building Director if not subject to D.R.)~~  
 688 Planning Official may approve a minor reduction in the depth requirements if the  
 689 applicant demonstrates that the requirement is not feasible given the configuration  
 690 of existing or proposed improvements and that the design of the commercial  
 691 frontage will maximize visual interest.  
 692
- 693 b. Other uses allowed in this zone and parking shall not be located on the street level  
 694 floor unless an intervening commercial frontage is provided between the street and  
 695 those other uses or parking subject to the standards above. Lobbies for residential  
 696 or assisted living uses may be allowed within the commercial frontage provided  
 697 they do not exceed 20 percent of the building's linear commercial frontage along  
 698 the street.  
 699

700 Section 17. KZC 55.33.100 is hereby amended to read as follows:  
 701

702 **55.33 Use Zone Chart – TL 4A, TL 4B and TL 4C Zones.**  
 703

Section 55.33	USE ↓	REGULATIONS ↓	DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS									
			Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
				Lot Size	REQUIRED YARD (See Ch. 115)			Lot Coverage				
				Front	Side	Rear						
***												
.100	Attached or Stacked Dwelling Units See Spec. Reg. 1.	D.R., Chapter 142 KZC	None	10'	0'	0'	80%	75' above average building elevation. See Gen. Reg. 2. and Spec. Reg. 4.	D	E	1.2 per studio unit. 1.3 per 1 bedroom unit. 1.6 per 2 bedroom unit.	1. At least 20 percent of the total gross floor area located on the street level floor of the building shall include commercial use. The

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS													
Section 55.33	USE Ø	REGULATIONS	Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
				Lot Size	REQUIRED YARD (See Ch. 115)			Lot Coverage					Height of Structure
					Front	Side	Rear						
										1.8 per 3 or more bedroom unit. See KZC 105.20 for visitor parking requirements.	commercial use shall be oriented toward fronting streets and have a minimum depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the building along the street). In TL4A, parcel 282605-9043 is exempt from this special regulation, due to the limited visibility and access to this parcel. The Design Review Board Planning Official may approve a minor reduction in the depth requirements if the applicant demonstrates that the requirement is not feasible given the configuration of existing or proposed improvements and that the design of the commercial frontage will maximize visual interest. Lobbies for this use are allowed within the commercial frontage provided they do not exceed 20 percent of the building's linear commercial frontage along the street.  2. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use.		

Section 55.33	USE Ø	REGULATIONS	DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS									
			Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
				Lot Size	REQUIRED YARD (See Ch. 115)			Lot Coverage				
Front	Side	Rear										
											<p>3. At least 10 percent of the units provided in new residential developments of four units or greater shall be affordable housing units, as defined in Chapter 5 KZC. See Chapter 112 KZC for additional affordable housing requirements and incentives.</p> <p>4. The equivalent of the additional gross floor area constructed above 35 feet over ABE must be dedicated to residential use. Residential use may be located anywhere in the building above the ground floor.</p> <p>5. Ancillary assembly and manufacture of goods on the premises of this use are permitted only if:</p> <p>a. The ancillary assembled or manufactured goods are subordinate to and dependent on this use.</p> <p>b. The outward appearance and impacts of this use with ancillary assembly or manufacturing activities must be no different from other office uses.</p>	
***												

704  
705  
706  
707  
708

**Section 18.** KZC Section 55.45.100 is hereby amended to read as follows:

**55.45 Use Zone Chart – TL 6A and TL 6B Zones.**

Section 55.45	USE ↓	REGULATIONS ↓	DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS									
			Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
				Lot Size	REQUIRED YARD (See Ch. 115)			Lot Coverage				
				Front	Side	Rear						
***												
.100	Attached or Stacked Dwelling Units. See Spec. Reg. 1.	D.R., Chapter 142 KZC	None	10'	0'	0'	80%	75' above average building elevation. See Gen. Regs. 3 and 10.	D	A	1.2 per studio unit. 1.3 per 1 bedroom unit. 1.6 per 2 bedroom unit. 1.8 per 3 or more bedroom unit. See KZC 105.20 for visitor parking requirements.	1. The street level floor of the building shall include commercial use. The commercial use shall be oriented toward fronting streets, major pedestrian sidewalks and through-block pathways, and have a minimum depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the building along the street, major pedestrian sidewalk or through-block pathway). Lobbies for this use are allowed within the commercial frontage provided they do not exceed 20 percent of the building's linear commercial frontage along the street. The Design-Review Board Planning Official may determine the orientation of the commercial use where multiple pedestrian circulation routes exist (see Plate 34C). The Design-Review Board Planning Official may approve a minor reduction in the depth requirements if the applicant demonstrates that the requirement is not feasible given the configuration of

Section 55.45	USE ↓	REGULATIONS ↓	DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS										
			Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
				Lot Size	REQUIRED YARD (See Ch. 115)			Lot Coverage					Height of Structure
					Front	Side	Rear						
											existing or proposed improvements and that the design of the commercial frontage will maximize visual interest. This requirement does not apply to ground floor residential use in TL 6A where over 80 percent of the total units in the development are affordable to households earning no more than 60 percent of King County median income, adjusted for household size.  2. At least 10 percent of the units provided in new residential developments of four units or greater shall be affordable housing units, as defined in Chapter 5 KZC. See Chapter 112 KZC for additional affordable housing requirements and incentives.  3. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use.		
***													

709  
710  
711  
712  
713  
714  
715

Section 19. KZC 55.49 is hereby amended to read as follows:

**55.49 General Regulations – TL 7 Zone.**

The following regulations apply to all uses in this zone unless otherwise noted:

1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the subject property.

716  
717  
718  
719  
720  
721  
722  
723  
724  
725  
726  
727  
728  
729  
730  
731  
732  
733  
734  
735  
736  
737

2. A hazardous liquid pipeline extends through the TL 7 zone. Refer to Chapter 118 KZC for regulations pertaining to properties near hazardous liquid pipelines.
3. ~~In TL 7A, Design Review, Chapter 142 KZC. In TL 7B, any development activities requiring Design Review approval pursuant to KZC 142.15 in this zone shall be reviewed through Administrative Design Review (ADR), pursuant to KZC 142.25. The guidelines contained in the Design Guidelines for the Totem Lake Business District, adopted by KMC 3.30.040, shall be applied in lieu of the design regulations in Chapter 92 KZC.~~
4. Access for drive-through facilities must be approved by the Public Works Official. See Chapter 105 KZC for requirements.
5. Some development standards or design regulations may be modified as part of the design review process. See Chapters 92 and 142 KZC for requirements.
6. Development adjoining the Cross Kirkland Corridor or Eastside Rail Corridor shall comply with the standards of KZC 115.24.

Section 20. KZC 55.51.005 is hereby amended to read as follows:

**55.51 Use Zone Chart – TL 7 Zone.**

Section 55.51	USE ↓	REGULATIONS ↓	DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS									
			Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
				Lot Size	REQUIRED YARD (See Ch. 115)			Lot Coverage				
				Front	Side	Rear						
.005	Mixed Use Development Containing Attached or Stacked Dwelling Units	D.R., Chapter 142 KZC.  See Gen. Reg. 3.	1.5 acres	10'	0'	0'	80%	80' above average building elevation. See Spec. Reg. 3.	See Spec. Reg. 5.	C	1.2 per studio unit.  1.3 per 1 bedroom unit.  1.6 per 2 bedroom unit.  1.8 per 3 or more bedroom unit.  See KZC 105.20 for visitor parking requirements.	1. This use is permitted only in TL 7A.  2. At least 20 percent of the total gross floor area located on the street-level floor along NE 124th Street of the building shall include commercial use. Residential use may be located on the street level floor of a structure only if there is an intervening commercial use with a minimum depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the building on NE 124th Street),

Section 55.51	USE ↓	REGULATIONS ↓	DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS									
			Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations  (See also General Regulations)
				Lot Size	REQUIRED YARD (See Ch. 115)			Lot Coverage				
Front	Side	Rear										
											between this use and NE 124th Street. The <u>Design Review Board-Planning Official</u> may approve a minor reduction in the depth requirements if the applicant demonstrates that the requirement is not feasible given the configuration of existing or proposed improvements and that the design of the commercial frontage will maximize visual interest.  ***	

738  
739  
740  
741

Section 21. KZC 56.10.010 is hereby amended to read as follows:

**56.10 Use Zone Chart – YBD 1 zone.**

Section 56.10	USE ↓	REGULATIONS ↓	DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS										
			Required Review Process	Lot Size	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
					REQUIRED YARD (See Ch. 115)			Lot Coverage	Height of Structure				
Front	Side	Rear											
.010	Attached or Stacked Dwelling Units	D.R., Chapter 142 KZC	None	5' See Spec. Reg. 2.	0'	0'	100%	65' above average building elevation. See Spec. Reg. 11.	C	E See Spec. Reg. 15.	Residential: 1.1 per unit, plus guest parking: .05 per unit. Restaurant/tavern: 1 per each 125 sq.ft. of gross floor area. Retail: 1 per each 350 sq.ft. of gross floor area. Office: 1 per each 350 sq.ft. of gross floor area. Entertainment, cultural, recreational: KZC 105.25. See also KZC 105.103.	1. The required minimum front yard for any portion of the structure containing parking facilities shall be 10 feet.  2. The front setback may be reduced to zero feet where retail uses or other ground floor space is designed to provide direct pedestrian access to the street and located adjacent to a pedestrian-oriented street, major pedestrian pathway or adjacent to a transit facility.	

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS												
Section 56.10	USE ↓ REGULATIONS	Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
			Lot Size	REQUIRED YARD (See Ch. 115)			Lot Coverage					Height of Structure
				Front	Side	Rear						
											<p>3. May include one or more of the other uses allowed in this zone.</p> <p>4. The following uses are prohibited:</p> <p>a. Drive-through facilities.</p> <p>b. The outdoor storage, sale, service and/or rental of motor vehicles, sailboats, motor boats, and recreational trailers.</p> <p>5. At least 50 percent of the linear frontage of the ground floor along NE 38th Place must include one or more of the following uses: Retail uses selling goods or providing services, including restaurants or taverns; Banking and Related Financial Services; School, Day-Care or Mini School or Mini Day-Care Center; Government Facility; Community Facility; and retail establishments providing entertainment, cultural and/or recreational activities. The required uses shall have a minimum depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the building on the abutting right-of-way). The Design Review Board (or Planning and Building Director if not subject to D.R.) Planning Official may approve a minor reduction in the depth requirements if</p>	

Section 56.10	USE ↓	REGULATIONS ↓	DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS									
			Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
				Lot Size	REQUIRED YARD (See Ch. 115)			Lot Coverage				
Front	Side	Rear										
											the applicant demonstrates that the requirement is not feasible given the configuration of existing or proposed improvements and that the design of the retail frontage will maximize visual interest. Lobbies for residential are allowed within this space subject to applicable design guidelines.  * * *	

742  
743  
744  
745  
746  
747  
748  
749  
750  
751  
752  
753  
754  
755  
756  
757  
758  
759  
760  
761  
762  
763  
764  
765  
766

Section 22. KZC 57.05.050 is amended to read as follows:

**57.05.050 Review Process.**

This chapter shall be administered by the Planning and Public Works officials through the related development permit process. Design Board review is required for projects that meet the criteria established in KZC 142.15, and which are located in the following zones: Commercial Mixed Use, Neighborhood Mixed Use, Civic Mixed Use. In cases where a development project is subject to Design Board review and this chapter establishes design departures and variation from the requirements in this chapter, the final standard shall be determined by the Design Review Board Planning Official as established in KZC 142.3738, unless otherwise noted. Standards that may be granted design departures and variations by the Design Review Board Planning Official are:

1. Maximum street level facade width;
2. Minimum facade break width and depth;
3. Required setbacks;
4. Minimum upper story street setbacks;
5. Maximum floor plate;
6. Minimum ground floor parking setbacks;
7. Plaza/public space dimensions;
8. Overhead weather protection.

The Planning Official may modify the minimum and/or maximum setback requirement based on the location of existing access and/or utility easements that cannot be changed.

767 Section 23. KZC 105.103 is amended to read as follows:  
768

769 **105.103 Modifications.**  
770 \*\*\*

771 2. *Authority to Grant and Duration.*

772 a. If the proposed development of the subject property requires approval through  
773 Process I, IIA, or IIB, described in Chapters 145, 150 and 152 KZC, respectively, a  
774 request for a modification will be considered as part of that process under the provisions  
775 of this section. The City must find that the applicant meets the criteria listed below in  
776 subsection (3) of this section. If granted under Process I, IIA, or IIB, the modification is  
777 binding on the City for all development permits issued for that development under the  
778 Building Code within five (5) years of the granting of the modification.

779 b. For projects requiring Design Review described in Chapter 142 KZC, a request to  
780 modify the requirements in KZC 105.18 or 105.19 will be considered as part of the  
781 Design Review process. The ~~Design Review Board~~ Planning Official must find that the  
782 applicant meets the criteria listed below in subsection (3)(b) of this section.

783 c. If subsection (2)(a) and/or (2)(b) of this section does not apply, the Planning  
784 Official may grant a modification in writing under the provisions of this section.

785 Section 24. KZC 110.60 is amended to read as follows:  
786

787 **110.60 Additional Requirements.**  
788 \*\*\*

789 11. Entry or Gateway Features in Design Districts – In Design Districts, if the Comprehensive  
790 Plan ~~or Design Guidelines~~ designates the subject property for an entry or gateway feature,  
791 then the applicant shall design and install an entry feature area on the subject property. The  
792 size of the entry feature area shall be at least 100 square feet, and may include landscaping,  
793 art, signage or lighting. The design shall be reviewed by the City and decided upon as part of  
794 the Design Review for the proposed development. The applicant shall provide an easement or  
795 dedication of property surrounding the entry feature.  
796

797 Section 25. KZC 115.24 is amended to read as follows:  
798

799 **115.24 Cross Kirkland Corridor/Eastside Rail Corridor – Supplemental Development**  
800 **Standards for Adjoining Properties.**  
801 \*\*\*

802 3. Design Standards – Development on properties adjoining the Corridor shall comply with the  
803 following design standards. Compliance with these standards shall be administered by the  
804 Planning Official in conjunction with review of an applicable development permit ~~unless the~~  
805 ~~proposal is subject to Design Board review, in which case the Design Review Board shall~~  
806 ~~review the proposal for compliance.~~ Applications involving additions or modifications to existing  
807 buildings shall comply with these standards to the extent feasible depending on the scope of  
808 the project. The Planning Official ~~or Design Review Board~~ may modify compliance with a  
809 particular regulation if the applicant demonstrates that it is not feasible given the existing  
810 development and scope of the project.

811 a. Site Design: Development adjoining the Corridor shall be designed to complement  
812 the public nature of the Corridor though the following site design and pedestrian  
813 improvements; provided, that subsections (3)(a)(i), (3)(a)(iii) and (3)(a)(iv) of this  
814 section shall not apply to A Retail Establishment Providing Vehicle or Boat Sales:

- 815 i) Landscape islands required pursuant to KZC 95.44 (Internal Parking Lot  
816 Landscaping Requirements) shall be provided such that there are no more  
817 than eight (8) contiguous parking stalls along the corridor.  
818 ii) In addition to providing the screening and buffering functions required by the  
819 KZC, landscape design shall integrate with and complement corridor  
820 functions.  
821 iii) A pedestrian entrance facing the Corridor shall be provided with a  
822 pedestrian walkway connecting from the entrance to the Corridor. The  
823 walkway shall be installed pursuant to the standards of KZC 105.18(2)(a),  
824 except any stairs shall be equipped with a bicycle runnel. The Planning  
825 Official may modify the connection requirement where grade or other natural  
826 features preclude reasonable access to the Corridor.  
827 iv) Bicycle parking as required by KZC 105.32 shall be provided at a ratio of  
828 one (1) bicycle space for each six (6) required motor vehicle parking spaces  
829 and shall be accessible by bicycle to the Corridor.
- 830 b. Building Design: Building design adjoining the Corridor shall acknowledge the high  
831 visibility from this active public space through the following building design  
832 standards:
- 833 i) All buildings shall be designed so that facades visible from the Corridor  
834 comply with the provisions of KZC 92.15(3) (Blank Wall Treatment).  
835 ii) All buildings shall be designed so that parking garages visible from the  
836 Corridor comply with the provisions of KZC 92.15(4)(a) and (b) (Parking  
837 Garages).  
838 iii) Building facades visible from the Corridor shall incorporate similar building  
839 materials and window treatment as other facades of the building.  
840 iv) Building facades facing the Corridor shall not exceed 120 feet without  
841 vertical definitions. Vertical definition may be in the form of changes in color  
842 and materials, modulations of sufficient width and depth to define the vertical  
843 element, or some combination of these techniques. This vertical element  
844 should carry through all floors of the building.

845  
846 Section 26. KZC 115.115 is amended to read as follows:  
847

848 **115.115 Required Yards.**

849 \*\*\*

850 5. *Driveways and Parking Areas.* Driveways and parking areas are not allowed in required  
851 yards except as follows:

852 a. *Detached Dwelling Units.*

- 853 1) *General.* Vehicles may be parked in the required front or rear yard if parked on a  
854 driveway and/or parking area. For the purpose of this section, vehicles are limited  
855 to those devices or contrivances which can carry or convey persons or objects and  
856 which are equipped as required by federal or state law for operation on public roads.  
857 A driveway and/or parking area shall not exceed 20 feet in width in any required  
858 front yard, and shall be separated from other hard-surfaced areas located in the  
859 required front yard by a landscape strip at least 18 inches in width. This landscape  
860 strip may be interrupted by a walkway or pavers providing a lateral connection from  
861 the driveway to other hard-surfaced areas, as long as such walkway or pavers do  
862 not exceed five feet in width. A driveway and/or parking area shall not be closer  
863 than five feet to any side property line (see Plate 14); provided:
- 864 a) That where access to a legally established lot is provided by a panhandle or  
865 vehicle access easement measuring less than 20 feet in width, a driveway not  
866 exceeding 10 feet in width, generally centered in the panhandle or access  
867 easement, shall be permitted (see Plate 14A); and

- 868 b) That for flag lots, a five-foot setback is not required from any side property line  
 869 that abuts a neighboring lot that was part of the same plat.  
 870 c) That any driveway which generally parallels a right-of-way or easement road  
 871 shall be set back at least five feet from the right-of-way or easement, except for  
 872 a 20-foot-wide section where the driveway connects with the right-of-way or  
 873 easement. Such driveway shall not have a width of more than 10 feet within the  
 874 front or rear yard (see Plate 14B) and shall be separated from other hard-  
 875 surfaced areas located in the front or rear yard by a landscape strip at least five  
 876 feet in width. Where more than one driveway is permitted within a front or rear  
 877 yard, those driveways shall be separated by a landscape strip at least five feet  
 878 in width.
- 879 2) *Exception.* Driveways and/or parking areas may exceed 20 feet in width if:  
 880 a) The driveway/parking area serves a three-car garage; and  
 881 b) The subject property is at least 60 feet in width; and  
 882 c) The garage(s) is (are) located no more than 40 feet from the front property line;  
 883 and  
 884 d) The driveway/parking area flares from 20 feet at the property line to a  
 885 maximum of 30 feet in width.
- 886 3) The Planning Official may approve a modification to the driveway and/or setback  
 887 requirements in subsection(5)(a)(1) of this section if:  
 888 a) The Public Works Department requires an on-site vehicular turnaround  
 889 adjacent to the driveway, which must be the minimum necessary dimension  
 890 as determined by the Public Works Department; or  
 891 b) The existing topography of the subject property or the abutting property  
 892 decreases or eliminates the need for the setback; or  
 893 c) The location of pre-existing improvements or vegetation on the abutting site  
 894 eliminates the need for or benefit of a setback; and  
 895 d) The modification will not have any substantial detrimental effect on abutting  
 896 properties or the City as a whole.  
 897
- 898 b. Vehicle parking areas for schools and day-care centers greater than 12 students and  
 899 located in residential zones shall have a minimum 20-foot setback from all property lines.  
 900
- 901 c. *Other Uses.* Parking areas and driveways for uses other than those addressed in  
 902 subsections (5)(a) and (5)(b) of this section may be located within required setback yards, but,  
 903 except for the portion of any driveway which connects with an adjacent street, not closer than  
 904 five feet from any property line. Where this provision conflicts with a regulation of a specific  
 905 zone, the regulation of the specific zone shall govern.  
 906
- 907 d. *Shared Parking and Shared Driveways.* If a parking area or driveway serves two adjacent  
 908 uses, the shared parking area or driveway may be anywhere in the required setback yard  
 909 between the uses.  
 910
- 911 e. *Exceptions for Projects Requiring Design Review.* If a project is reviewed through  
 912 design review pursuant to Chapter 142 KZC, the driveway shall comply with parking area  
 913 location and design requirements as determined by the ~~Design Review Board~~ Planning  
 914 Official.  
 915

Section 27. KZC 141.70 is amended to read as follows:

**141.70 Procedures.**

**1. Substantial Development Permits.**

**a. General.**

1) Applications for a shoreline substantial development permit shall follow the procedures for a Process I permit review pursuant to Chapter 145 KZC, except as otherwise provided in this section.

2) If the proposal that requires a substantial development permit is part of a proposal that requires additional approval through Process IIA or Process IIB under Chapter 150 KZC or Chapter 152 KZC, respectively, the entire proposal will be decided upon using that other process.

3) If the proposal that requires a substantial development permit is part of a proposal that requires additional approval through the Design Review Board ~~(DRB)~~ under Chapter 142 KZC, the design review ~~proceedings before the DRB~~ process shall be conducted in accordance with Chapter 142 KZC.

\* \* \*

Section 28. Chapter 92 of the Kirkland Zoning Code, entitled "Design Regulations," is hereby repealed in its entirety and replaced with a new Chapter 92 to read as follows:

**Chapter 92  
DESIGN REGULATIONS**

**92.01 General Applicability**

**1. *Applicable Zoning Districts.***

a. This chapter establishes the design regulations that apply to new development in the following Design Districts:

- 1) Central Business District (CBD). See Section 92.01.2 for CBD 5A requirements.
- 2) Finn Hill Neighborhood Center (FHNC)
- 3) Market Street Corridor (MSC)
- 4) Neighborhood Business Districts (BN, BNA)
- 5) Bridle Trails Neighborhood Center (BCX zone)
- 6) Houghton/Everest Neighborhood Center (HENC)
- 7) Juanita Business District (JBD)
- 8) North Juanita Neighborhood Center (NJNC)
- 9) NE 85th St. Station Area
- 10) Totem Lake Business District (TLBD)
- 11) North Rose Hill Business District (NRHBD)
- 12) Business District Core (BDC) in the TLBD
- 13) Yarrow Bay Business District (YBD)

**2. *CBD 5A Requirements***

a. Projects located in Park Place district (CBD 5A) will be reviewed for compliance with the Kirkland Parkplace Mixed Use Development Master Plan and Design Guidelines, referenced in KZC 3.30.040.

967 **92.03 Review Authority**

968 1. *Authority*

969 The Planning Official will conduct the review of all projects for compliance with the  
 970 standards in this chapter. The Design Review Board (DRB) is engaged only when a  
 971 project seeks Alternative Compliance pursuant to KZC Section 142.38.2. The following  
 972 standards, and the standards in Chapter 142 KZC, guide the application and modification  
 973 of this chapter:

974 a. **Chapter 92 KZC**

975 Chapter 92 KZC Design Standards is the primary reference for the  
 976 administrative design review process outlined in Chapter 142.

977 b. **Design Guidelines**

978 The Design Guidelines apply only when a project seeks a Design Modification  
 979 by the Planning Official or Alternative Compliance through the DRB. They serve  
 980 as a supplemental tool for proposing modifications to Chapter 92 KZC or other  
 981 applicable zoning standards.  
 982

983 2. *Dedication*

984 The City may require the applicant to dedicate development rights, air space, or an  
 985 easement to the City to ensure compliance with any of the requirements of this  
 986 chapter.  
 987

988 3. *Relationship to Other Regulations*

989 a. **Installation of Sidewalks, Public Pedestrian Pathways, and Public  
 990 Improvements**

991 Chapter 110 KZC describes the requirements for the installation of public  
 992 sidewalks, major pedestrian sidewalks, pedestrian-oriented sidewalks, or other  
 993 public improvements on or adjacent to the subject property in zones subject to  
 994 Design Review. Plate 34 in Chapter 180 KZC provides the location and  
 995 designation of the sidewalk, pedestrian walkways, pathways or other required  
 996 public improvements within each Design District.

997 b. **Pedestrian Access to Buildings, Installation of Pedestrian Pathways,  
 998 Pedestrian Weather Protection**

999 Chapter 105 KZC describes the requirements for pedestrian access to buildings  
 1000 and between properties, through parking areas and requirements for pedestrian  
 1001 weather protection. See also Plate 34 in Chapter 180 KZC.

1002 c. **Landscaping**

1003 Chapter 95 KZC describes the requirements for installation and maintenance of  
 1004 landscaping on the subject property.

1005 d. **Parking Area Location and Design, Pedestrian and Vehicular Access**

1006 Chapter 105 KZC describes the requirements for parking lot design, number of  
 1007 driveways, and pedestrian and vehicular access through parking areas.

1008 e. **Screening of Loading Areas, Outdoor Storage Areas and Garbage  
 1009 Receptacles**

1010 Chapter 95 KZC describes the location and screening requirements of outdoor  
 1011 storage. Chapter 115 KZC describes the screening of loading areas, waste  
 1012 storage and garbage disposal facilities.

1013 f. **Design Review Process**

1014 The provisions of Chapters 142 and 162 KZC regarding Design Review and  
 1015 nonconformance establish which of the regulations of this chapter apply to  
 1016 developed sites. Where provisions of this chapter conflict with provisions in any

1017 other section of the code, this chapter prevails. For planning vision and more  
 1018 information on each Design District, refer to the Design Guidelines applicable  
 1019 to that Design District adopted by reference in KMC 3.30.040.

1020

1021

## 92.06 Definitions

1022

1023

1024

1025

### 1. *Articulation*

1026

1027

1028

1029

1030

1031

1032

1033

### 2. *Massing*

1034

1035

Massing refers to the overall form, shape, and volume of a building; the way in which the mass is arranged. See **Error! Reference source not found.**

**Figure 92.06.A Intent Diagram Demonstrating Massing Options**



1036

1037

1038

1039

1040

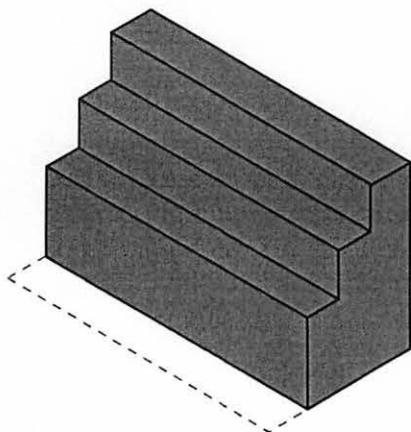
### 3. *Modulation*

Modulation refers to large scale massing arrangements such as upper-level stepbacks and significant vertical offsets of the facade. Modulation affects the overall form and interior layout of the building.

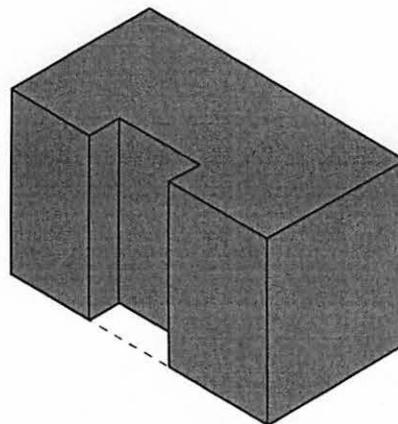
1041  
1042  
1043  
1044  
1045  
1046  
1047  
1048  
1049  
1050

- a. **Horizontal Modulation**  
Horizontal modulation refers to projection or recessing of the facade in lateral segments, such as a street-level setback or an upper-level setback. Horizontal modulation helps to break up the height of a building. See Figure 92.06..B.
- b. **Vertical Modulation**  
Vertical modulation refers to projection or recessing of the facade in vertical segments, typically extending the entire height of the building, or the majority of the building. Vertical modulation helps to break up the length of a building. See Figure 92.06.C.

**Figure 92.06.B - Horizontal Modulation**



**Figure 92.06.C - Vertical Modulation**



1051  
1052  
1053  
1054  
1055  
1056  
1057  
1058  
1059  
1060  
1061  
1062  
1063  
1064  
1065  
1066  
1067  
1068  
1069  
1070  
1071  
1072  
1073  
1074

#### 4. *Upper-Level Setback*

A form of horizontal modulation where a building's upper stories are built further back from the street or floors below. Setbacks can help reduce the perceived scale of taller buildings, improve pedestrian comfort by increasing light access and reducing wind impacts, and introduce opportunities for terraces, plazas, and roof decks.

### 92.11 Site Planning Standards

#### 1. *Site Planning Applicability*

- a. All projects subject to this chapter must comply with site planning standards.
- b. This section is to ensure that building placement and frontage design are responsive to street context and contribute to an active, safe, and pedestrian-friendly environment. Pedestrian-Oriented Street and Major Pedestrian Sidewalks referred to in this section are indicated in Plate 34 of Chapter 180 KZC.
- c. Section 92.11.5 contains zone specific site planning standards that apply to the RH8, TLBD, YBD and NRHBD zone.

#### 2. *Site Planning Standards*

- a. **Sites Abutting Pedestrian-Oriented Street or Major Pedestrian Sidewalk**  
If the subject property abuts a right-of-way identified as a Pedestrian-Oriented Street or Major Pedestrian Sidewalk the following apply:
  - 1) *Building Orientation*: Buildings must be oriented toward the Pedestrian-Oriented Street or Major Pedestrian Sidewalk.
  - 2) *Pedestrian-Oriented Facade*: The ground floor facade facing a Pedestrian-

- 1075 Oriented Street or Major Pedestrian Sidewalk in Plate 34 of Chapter 180  
 1076 KZC must meet the requirements for a pedestrian-oriented facade (see KZC  
 1077 92.11.3) including but not limited to transparency, pedestrian entries, and  
 1078 weather protection.
- 1079 3) *Setback*: The ground floor of the building must be located at the front  
 1080 property line with zero setback, except where pedestrian space is required,  
 1081 in which case the building may be set back to accommodate additional  
 1082 sidewalk width or a pedestrian-oriented space.
- 1083 4) *Pedestrian-Oriented Space*: A portion of the frontage on the subject property  
 1084 must incorporate a pedestrian-oriented space as defined in KZC 92.11.4.
- 1085 5) *Street Corner Design*: For buildings on a corner, building corner design must  
 1086 comply with KZC 92.11.5.
- 1087 6) *Parking Location*: Ground level surface parking is discouraged between the  
 1088 building and the street. Indoor parking garages at street level are  
 1089 discouraged and must be architecturally treated per KZC 92.11.7. For  
 1090 vehicle sales uses, display showrooms are encouraged to front the street,  
 1091 with parking located to the side or rear. See KZC Section 105.58 for  
 1092 applicable parking area requirements for specific zones.  
 1093

1094 **b. Sites Not Abutting Pedestrian-Oriented Street or Major Pedestrian**  
 1095 **Sidewalk**

1096 If the subject property does not abut a right-of-way identified as a Pedestrian-  
 1097 Oriented Street or Major Pedestrian Sidewalk :

- 1098 1) *Building Orientation*: Buildings must be oriented toward the adjacent right-  
 1099 of-way. In the JBD zone, buildings may also be oriented toward a through-  
 1100 block pathway where present (see Plate 34 of Chapter 180 KZC).
- 1101 2) *Pedestrian-Oriented Facade*: Sites that abut a right-of-way that is not a  
 1102 pedestrian-oriented street or major pedestrian sidewalk may utilize the  
 1103 regulations in subsection 2.a above. If subsection 2.a is being utilized, it  
 1104 must be utilized in its entirety.
- 1105 3) *Setback Option*:
- 1106 i. Setback and landscaping requirements in the underlying zone apply.
- 1107 ii. If a pedestrian-oriented facade is provided, the building may be placed  
 1108 at the front property line and the front setback and landscape  
 1109 requirements in the underlying zone may be waived by the Planning  
 1110 Official.  
 1111

1112 **3. Pedestrian Oriented Facade Design**

1113 A pedestrian-oriented facade on the ground floor is required for development abutting a  
 1114 designated pedestrian oriented street or major pedestrian sidewalk in Plate 34 of Chapter  
 1115 180 KZC (see Figure 92.11.A). It is optional on other street types. The facade must meet  
 1116 all of the following standards:

- 1117 a. Provide a primary pedestrian entrance facing the street or sidewalk. For  
 1118 buildings on a corner, see KZC 92.11.5.
- 1119 b. Treat at least 60% of the facade area between two (2) and 10 feet above the  
 1120 sidewalk (see Figure 92.11.B) with:
- 1121 1) transparent windows and/or doors,  
 1122 2) display areas, or  
 1123 3) artwork.
- 1124 4) Alternative design treatments may be approved through Administrative

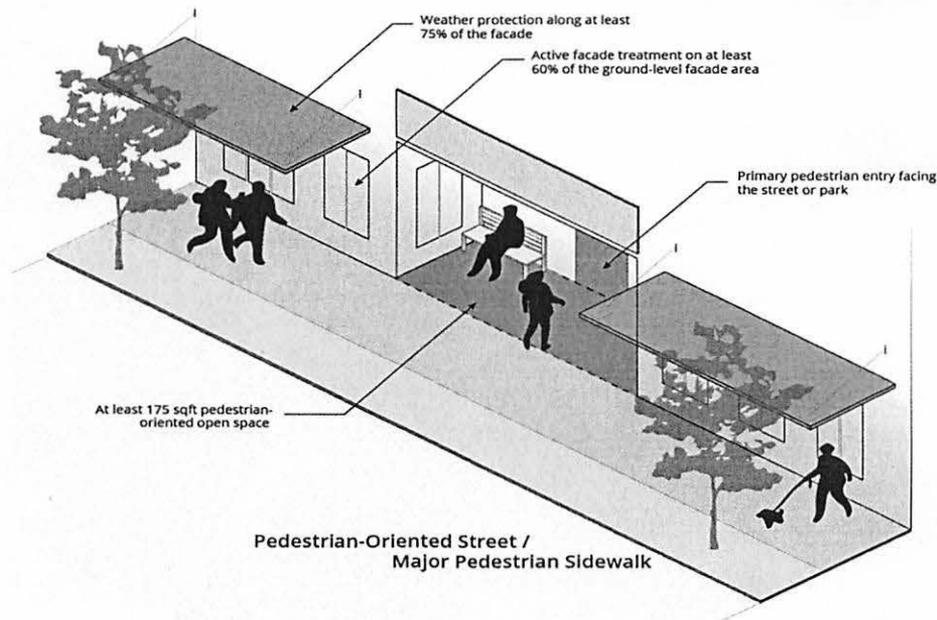
1125  
1126  
1127  
1128  
1129  
1130  
1131  
1132  
1133  
1134

Design Review process if the design treatment is consistent with the design guidelines.

- c. Provide weather protection over at least 75% of the facade, projecting a minimum of five (5) feet over the sidewalk and be at least eight (8) feet above grade (see Figure 92.11.C). Refer to KZC 105.18(3)(c) for configuration standards.

- 1) Exceptions apply in the CBD, RHBD, BN, BNA, BCX, MSC 2, FHNC, HENC 1, HENC 3, TLBD, and JBD zones. See KZC 105.18(3)(b) for detailed requirements.

**Figure 92.11.A Pedestrian-Oriented Building Facade Requirements**



1135

Figure 92.11.B Facade Treatment

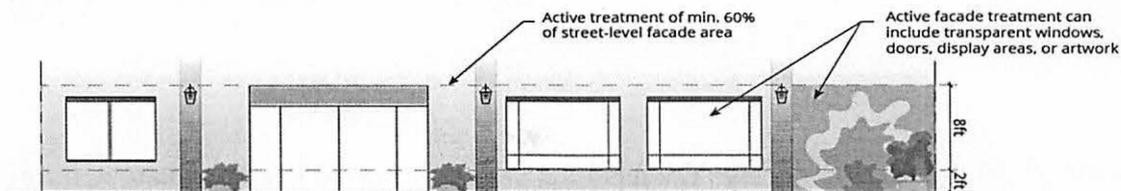
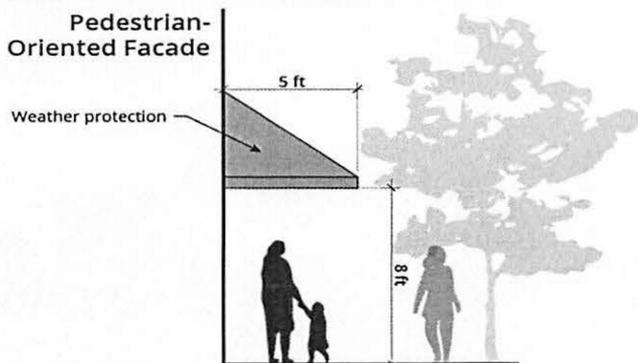


Figure 92.11.C Weather Protection



1136  
 1137  
 1138  
 1139  
 1140  
 1141  
 1142  
 1143  
 1144  
 1145  
 1146  
 1147  
 1148  
 1149  
 1150  
 1151  
 1152  
 1153  
 1154  
 1155  
 1156  
 1157  
 1158  
 1159  
 1160  
 1161  
 1162

4. Pedestrian Oriented Space Design

If the subject property abuts a Pedestrian-Oriented Street or Major Pedestrian Sidewalk (see Plate 34, Chapter 180 KZC), the project must provide a minimum of 175 square feet of pedestrian-oriented space on the subject property at the main building entrance.

a. Required Features

The pedestrian-oriented space must follow the below criteria (see Figure 92.11.D):

- 1) Provide clear pedestrian access from the street, private drive, or nonvehicular courtyard.
- 2) Use concrete or approved unit pavers for walking surfaces.
- 3) Include pedestrian-scaled lighting (maximum 15 feet in height) averaging at least 2 foot-candles.
- 4) Provide at least two (2) linear feet of seating, or one (1) seat, per 65 square feet of space.
- 5) Locate space where it serves areas of high pedestrian activity, such as near building entries.
- 6) Provide landscaping covering at least 20% of the space, such as trees, shrubs, trellises, or potted plants.

b. Encouraged Features

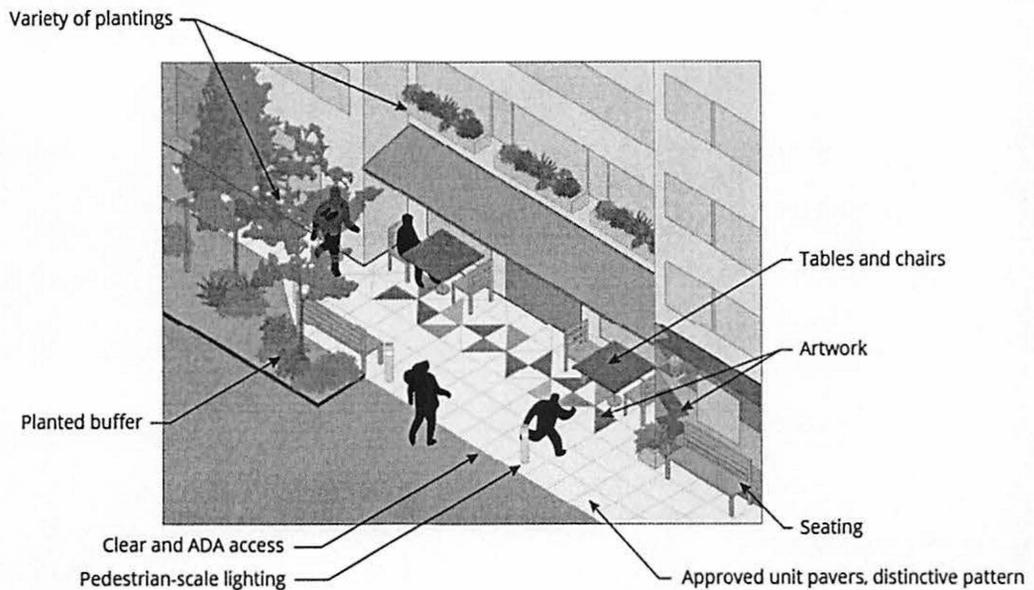
The following features are encouraged in a pedestrian-oriented space and may be considered acceptable alternatives:

- 1) Pedestrian amenities such as a water feature, a drinking fountain, tables and chairs, and distinctive paving or artwork.
- 2) Pedestrian-oriented facades on all buildings facing the space.
- 3) Transitional zones along building edges to allow for outdoor eating areas and a planted buffer.

1163  
1164  
1165  
1166  
1167  
1168  
1169  
1170  
1171  
1172  
1173  
1174  
1175  
1176

- 4) Movable seating.
  - 5) Consideration of solar orientation and wind patterns in the design of the open space and the choice of landscaping.
- c. Prohibited Features**  
The following features are prohibited within and/or adjacent to a pedestrian-oriented space:
- 1) Asphalt or gravel pavement as a primary walking surface. ADA accessible crushed gravel may be used in landscaped areas intended for secondary pedestrian use such as garden paths.
  - 2) Unscreened parking lots.
  - 3) Dumpsters or service areas.
  - 4) Outdoor storage or retail sales that do not contribute to the pedestrian environment.

**Figure 92.11.D Pedestrian-Oriented Space Features**



1177  
1178  
1179  
1180  
1181  
1182  
1183  
1184  
1185  
1186  
1187  
1188  
1189  
1190

5. *Special Requirements in the RH8, TLBD, YBD and NRHBD Zones*
- a. **Building Location Featuring Pedestrian-Oriented Facades in RH8, TLBD and YBD Zones** – Buildings may be located adjacent to the sidewalk of any street and in YBD (except for Lake Washington Boulevard and Northup Way), if they contain a pedestrian-oriented facade along that street frontage pursuant to the standards in subsection (2) of this section. As part of the Design Review process, required yards, setbacks or other development standards may be modified along the street frontage. Buildings not featuring a pedestrian-oriented facade along a street must provide a building setback of at least 10 feet from any public street (except areas used for pedestrian or vehicular access) landscaped with a combination of trees, shrubs, and groundcover per the requirements of supplemental landscape standards of KZC 95.41(2).
  - b. **For All Other Building Facades in RH8 and TLBD Zones (Non-Pedestrian-Oriented Facade)** – Building facades not featuring a pedestrian-oriented

1191 facade described in subsection (4) of this section must provide at least three (3)  
 1192 feet of landscaping between any vehicular access area or walkway and the  
 1193 building. Exceptions: 1) Alleys and other areas generally not visible to the  
 1194 public, as determined by the City; 2) Other design options may be considered  
 1195 through the Design Review process, provided they meet the intent of the  
 1196 guidelines.

1197 c. **Vehicle Sales Showrooms in TLBD Zone** – Vehicle sales uses are  
 1198 encouraged to locate their showrooms toward the street with parking to the side  
 1199 or rear.

1200 d. **Properties in RH8 – NE 85th St. Building Frontage Options and**  
 1201 **Preferences.**

1202 1) Preferred Option: Buildings may be located adjacent to the sidewalk on NE  
 1203 85th St. if they contain a pedestrian-oriented façade.

1204 2) Second Option: Locate and orient building towards the sidewalk on NE 85th  
 1205 St.. In this option, the development features a 10-foot minimum landscaped  
 1206 front yard, a clear pathway between the sidewalk and the building, and a  
 1207 building entry and windows facing the street.

1208 3) Least Preferred Option: Locate the building at the rear of the property with  
 1209 parking between NE 85th St. and the building as long as the following  
 1210 standards are applied:

1211 a) Provide a perimeter parking landscape buffer between the sidewalk and  
 1212 parking area per Chapter 95 KZC.

1213 b) Provide clear pedestrian access from the sidewalk to the building entry.

1214 c) Provide a walkway along the building facade meeting through-block  
 1215 pathway standards as described in KZC 105.19.

1216 e. **Properties in RH8 – Rear Yard Building Placement** – Pursuant to KZC 95.40  
 1217 through 95.45, in most cases, commercial uses shall install a required  
 1218 landscaped buffer adjacent to single-family properties. By requesting a  
 1219 modification to these provisions, the property owners may negotiate an  
 1220 agreement to reduce the landscape buffer/setback in a way that can benefit  
 1221 both parties.

1222  
 1223 Where buildings are sited towards the rear of the property, the applicant must  
 1224 utilize one of the following standards to minimize impacts to adjacent residential  
 1225 areas (see Figure 92.11.E and options below):

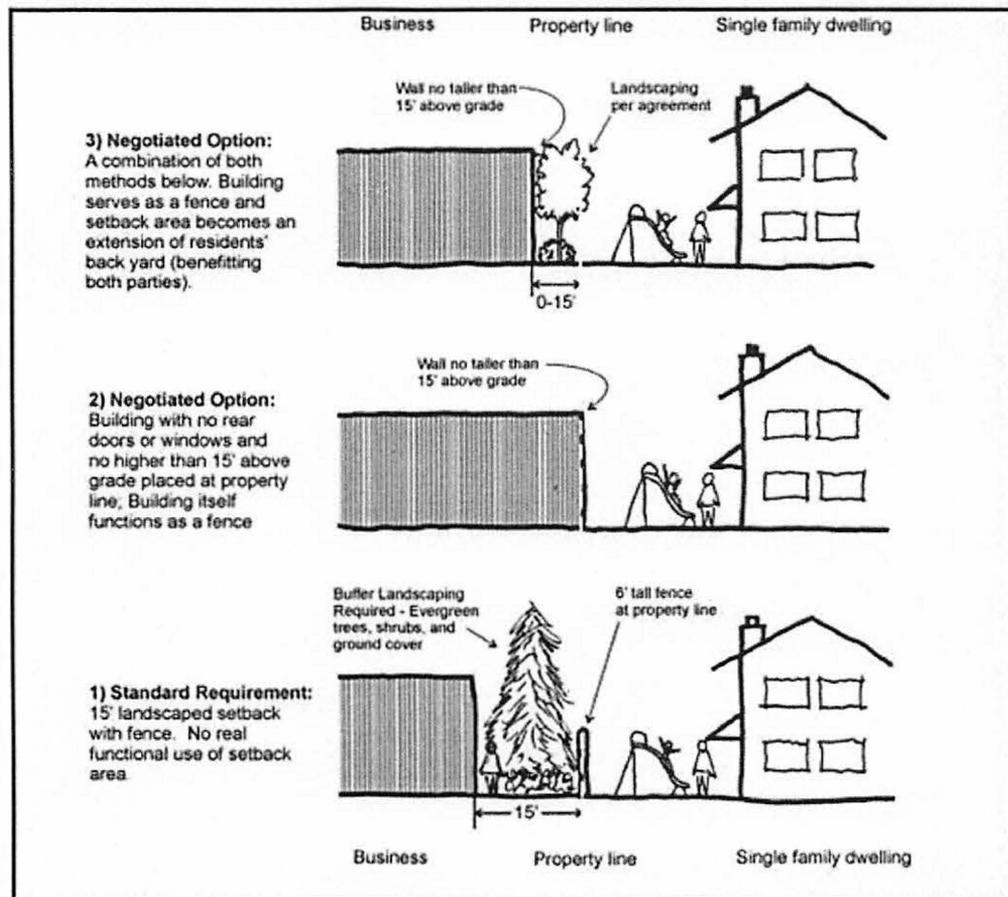
1226 1) Meet the required landscape buffer pursuant to KZC 95.42.

1227 2) Provide a blank wall no taller than 15 feet in height with no openings placed  
 1228 at the rear property line (building itself serves as a wall, uses are inside the  
 1229 building, shielded from adjacent residential uses). To qualify for this method,  
 1230 the treatment must be agreed to by the adjoining property owners per the  
 1231 modifications section of Chapter 95 KZC.

1232 3) Provide a combination of both methods above. For example, provide a blank  
 1233 wall no taller than 15 feet in height between zero and 15 feet from the  
 1234 property line and landscape the applicable area between the building and  
 1235 the property line. In addition, an unfenced design option would effectively  
 1236 enlarge the adjacent homeowners' rear yard (a mutually beneficial  
 1237 arrangement). To qualify for these methods, the treatment must be agreed  
 1238 to by the adjoining property owners per the landscape buffer modifications  
 1239 section of Chapter 95 KZC.  
 1240

1241

Figure 92.11.E Rear Yard Building Placement Options in RH8



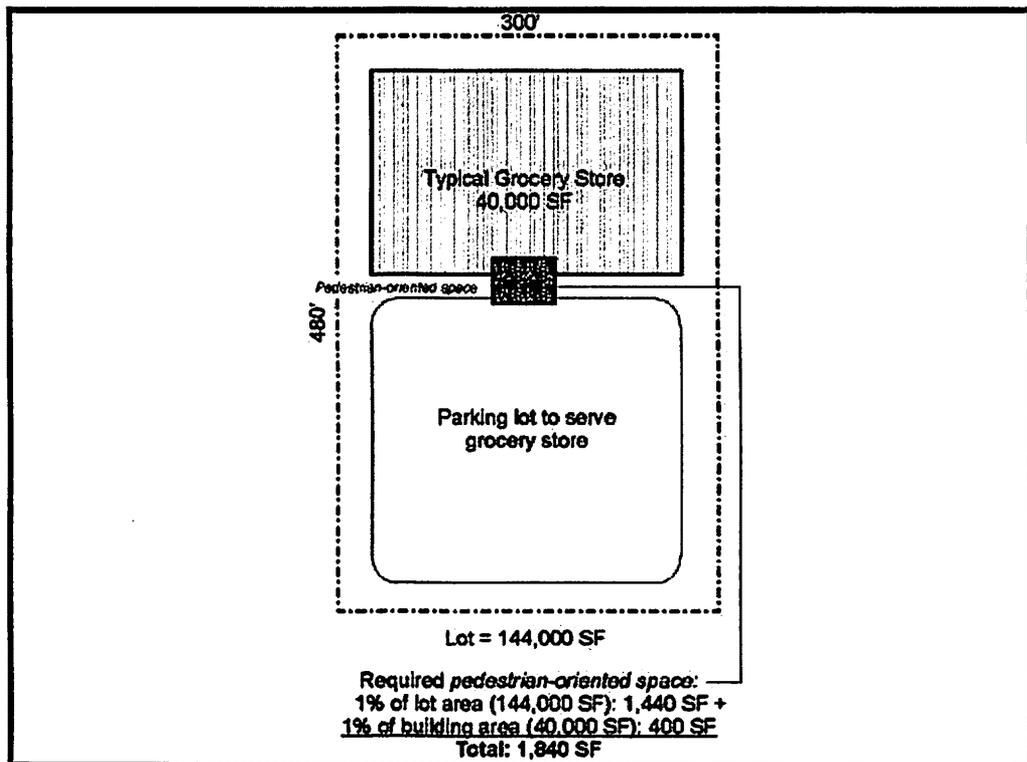
1242  
1243  
1244  
1245  
1246  
1247  
1248  
1249  
1250  
1251  
1252  
1253  
1254  
1255  
1256  
1257  
1258  
1259  
1260  
1261  
1262

- f. **Multi-Story Buildings on Sites Adjacent to a Low Density Zone in RH8 and TLBD** – Multi-story buildings on sites adjacent to a low density zone in RH8 and TLBD shall be configured and designed to minimize privacy impacts on adjacent low density uses. For example, a development may meet this requirement by orienting upper floors towards the street and/or towards interior courtyards.
- g. **Multifamily Buildings Located in TLBD** – Multifamily buildings located in TLBD adjacent to NE 120th Street must be oriented toward this street and to Slater Avenue NE. To meet this requirement, common and/or individual unit entries must face the street. The building must include windows that face the street. Parking areas between the building and the street are prohibited. Alternative configurations may be considered in the Design Review process.
- h. **In the TL 6B zone** – Single purpose residential buildings should be configured and oriented to take advantage of the greenbelt area to the north. For example, buildings could be arranged in a courtyard layout with the courtyard opening towards the greenbelt area.
- i. **In the NRHBD Zones** – If the subject property abuts a major pedestrian sidewalk on the southwest corner of NE 116th Street and 124th Avenue NE (see Plate 34 in Chapter 180 KZC), the space, if any, between the sidewalk and the building must be developed consistent with the following criteria:
  - 1) Enhance visual and pedestrian access, including handicapped access, onto

1263  
1264  
1265  
1266  
1267  
1268  
1269  
1270  
1271  
1272  
1273  
1274  
1275  
1276  
1277  
1278  
1279  
1280  
1281  
1282

- the subject property from the sidewalk.
- 2) Contain paved walking surface of either concrete or approved unit pavers.
  - 3) Contain on-site or building-mounted lighting which provides adequate illumination.
  - 4) Contain two linear feet of seating area or one individual seat per 65 square feet of area between the sidewalk and the building.
  - 5) Contain landscaping, such as trees, shrubs, trellises, or potted plants.
  - 6) In the alternative, the pedestrian-oriented space can be integrated with a pedestrian connection linking Slater Avenue NE and NE 116th Street, anywhere on the subject property, consistent with the criteria in subsections (2)(b)(1) through (5) of this section.
- j. All nonresidential uses in the RH8 and TLBD zones must provide pedestrian-oriented space in conjunction with new development according to the formula below. For the purposes of this section, required pathways will not count as pedestrian-oriented space. However, the City may allow those portions of pathways widened beyond minimum requirements to count towards the required pedestrian-oriented space as long as such space meets the definition of pedestrian-oriented space.
- 1) Size: One percent of the applicable lot area plus one percent of the nonresidential building gross floor area (see Figure 92.11.FF).

**Figure 92.11.F Pedestrian-Oriented Space Requirement for Large Nonresidential Buildings**



1283  
1284  
1285  
1286

- 2) The Planning Official may exempt uses that are likely to generate very little customer/pedestrian activity and have few or no employees. These may include warehouse, storage, industrial, and other similar uses.

1287  
1288  
1289  
1290  
1291  
1292  
1293  
1294  
1295  
1296  
1297  
1298  
1299  
1300  
1301  
1302  
1303  
1304  
1305  
1306  
1307  
1308  
1309  
1310  
1311  
1312  
1313  
1314  
1315  
1316  
1317  
1318  
1319  
1320  
1321

6. *Pedestrian Street Corner Design*

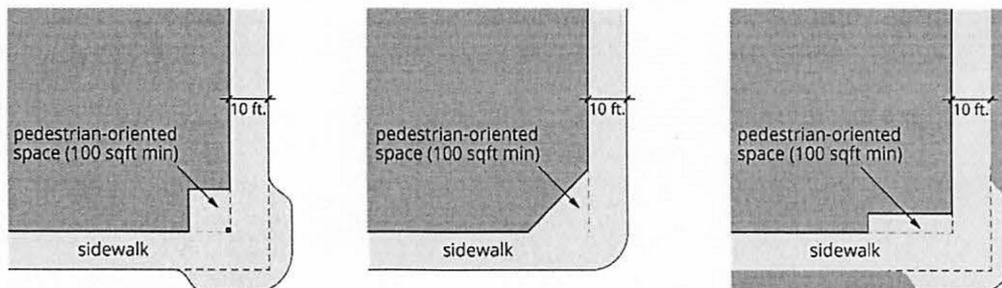
Projects at a street intersection where at least one street is a Pedestrian-Oriented Street or a Major Pedestrian Sidewalk, must incorporate at least one corner modulation treatment or at least two articulation treatments. Gateway sites must also reflect the vision in the Comprehensive Plan and applicable district-specific design guidelines.

Corner treatments listed below do not satisfy general modulation or articulation requirements in KZC 92.16 and 92.20 and must be provided in addition to those standards.

**a. Corner Modulation Treatment Options**

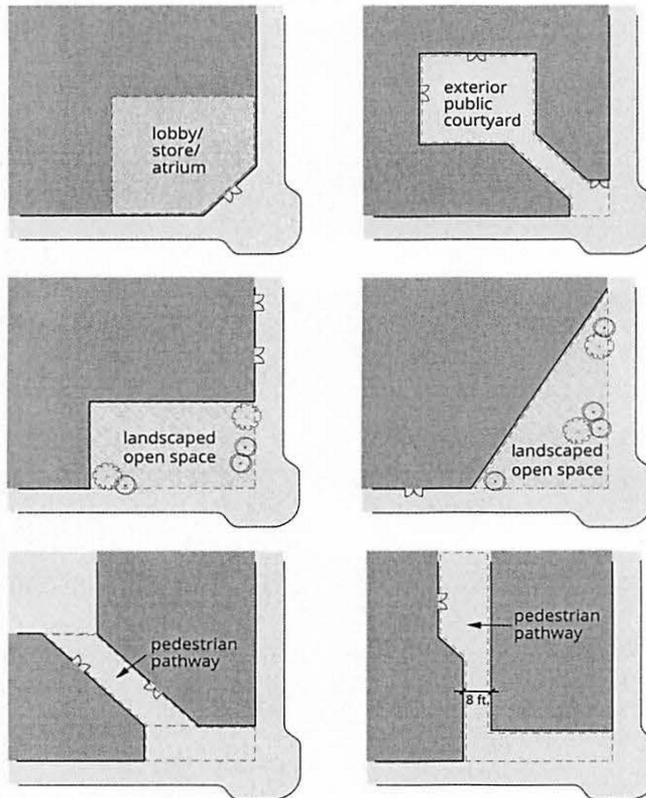
- 1) Provide a pedestrian-oriented space of at least 100 square feet at the corner with a setback and direct connection to a building entry, in addition to the area required for the sidewalk width as required under KZC 110.52 (see Figure 92.11.G).
- 1) Projects may locate the main building entrance at the corner and use this option to satisfy pedestrian-oriented space requirements if all standards in KZC 92.11.4 are met. In such cases, the corner space must be at least 175 square feet.
- 2) Provide an entrance to a store, building atrium or lobby, or exterior public courtyard (see Figure 92.11.H).
- 3) Provide a landscaped open space of at least 800 square feet, with a minimum width of 20 feet along each street-facing façade, framing the corner (see Figure 92.11.H).
- 4) Provide a pedestrian pathway connecting to another street, public feature, or building through the site interior (see Figure 92.11.H).
  - i. The pedestrian pathway must be a minimum of 8 feet wide, and must meet the standards as specified in KZC 105.18 (Pedestrian Access) or KZC 105.19 (Public Pedestrian Walkways).
  - ii. This option may satisfy the requirement for a “through-block pedestrian pathway” if designated in Plate 34 of Chapter 180 KZC, or for public pedestrian walkways on private property with public access easements. Refer to KZC 105.19(2)(b) (Through-Block Pathway Standards) and KZC 105.19(2)(c) (Standards for Certain Design Districts) for additional requirements.

**Figure 92.11.G Examples of Corner Setback Configurations**



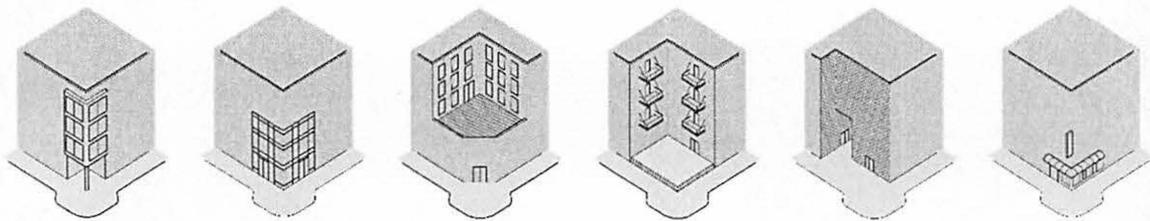
1322

**Figure 92.11.H Examples of Corner Entry Elements**



1323

**Figure 92.11.I Examples of Design Elements for Corners**



1324  
1325  
1326  
1327  
1328  
1329  
1330  
1331  
1332  
1333  
1334  
1335

**b. Corner Articulation Treatment Options**

- 1) Provide a special awning or canopy design at the corner of the building (see Figure 92.11.I). Also refer to KZC 105.18(3)(b) and (c) for detailed overhead protection standards.
- 2) Incorporate a notable design feature such as a significant amount of window area into a retail or restaurant space, a change in the fenestration pattern, or a notable color palette that adds identity or demarcation of the area. Such a feature may have a sign incorporated into it as long as the sign does not identify an individual business or businesses (see Figure 92.11.I).
- 3) Provide one or more of the elements listed below on both sides of the corner of the building (see Figure 92.11.I).
  - i. A bay window.

1336  
1337  
1338  
1339  
1340  
1341  
1342  
1343  
1344  
1345  
1346  
1347  
1348  
1349  
1350  
1351  
1352  
1353  
1354  
1355  
1356  
1357  
1358  
1359  
1360  
1361  
1362  
1363  
1364  
1365  
1366  
1367  
1368  
1369  
1370  
1371  
1372  
1373  
1374  
1375  
1376  
1377  
1378  
1379  
1380  
1381  
1382  
1383  
1384  
1385

- ii. Balconies above the ground floor.
- iii. Sculpture or artwork.
- iv. Distinctive use of facade materials.

**c. Gateway Features.**

- 1) Gateway features per KZC 110.60(11) are required at locations identified on the urban design feature map within Chapter XV of the City's Comprehensive Plan, which mark key entry points into neighborhoods and business districts.
- 2) Gateway design must reflect the vision for the Comprehensive Plan and applicable design district. Gateway features may include some or all of the following:
  - i. Distinctive landscaping
  - ii. Artwork
  - iii. Decorative lighting elements.
  - iv. Distinctive architectural features that are unique to the neighborhood or provide open space.
- 3) Additional design standards, public improvements, and/or amenities may be required by the City to implement the vision for the design district.
- 4) Special Consideration for TOD in PR 1.8 Zone: In addition to the above guidelines, the gateway design at the northeast corner of the site at NE 132nd Street/116th Way NE intersection should provide:
  - i. Hardscape and vegetation materials to create colorful and attractive open spaces.
  - ii. Wayfinding signage directing visitors to locations in the Totem Lake Business District.

**d. Special Considerations**

- 1) For RH8 properties located east of 128th Avenue NE, buildings must be located at the street corner and provide pedestrian-oriented facades along both streets. Setbacks will be allowed only where the space between the sidewalk and the building meets the definition of a pedestrian-oriented space. This treatment will fill any other requirements for pedestrian-oriented space as long as the specifics of those requirements are also met by this space.

**7. Parking Location and Screening**

**a. Parking Location**

- 1) In all Design Districts, surface parking shall not be located between the building and the street unless no feasible alternative exists determined by the Planning Official. KZC section 105.58 addresses the location of parking areas in specific design districts.
- 2) Surface parking lots shall be screened pursuant to the requirements in KZC Section 95.45.3.
- 3) In the RH8 and TLBD zones, structures containing parking on the ground floor must comply with the following standards:
  - i. Parking structures on designated pedestrian-oriented streets shall provide space for ground-floor commercial uses along street frontages at a minimum of 75 percent of the frontage width. The entire facade facing a pedestrian-oriented street must feature a pedestrian-oriented facade.
  - ii. Parking structures adjacent to non-pedestrian-oriented streets may be

1386  
 1387  
 1388  
 1389  
 1390  
 1391  
 1392  
 1393  
 1394  
 1395  
 1396  
 1397  
 1398  
 1399  
 1400  
 1401  
 1402  
 1403  
 1404  
 1405  
 1406  
 1407  
 1408  
 1409  
 1410  
 1411  
 1412  
 1413  
 1414  
 1415  
 1416  
 1417  
 1418  
 1419  
 1420  
 1421  
 1422  
 1423  
 1424  
 1425  
 1426  
 1427  
 1428  
 1429  
 1430

located adjacent to a sidewalk where they provide space for ground-floor commercial uses along street frontages at a minimum of 75 percent of the frontage width and include a pedestrian-oriented facade along the applicable frontage.

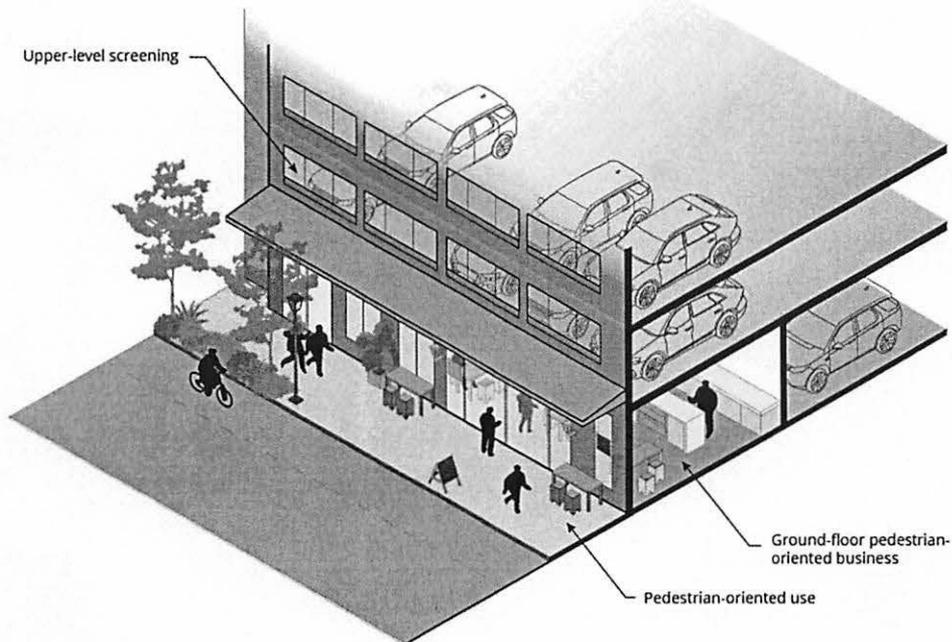
- iii. Parking structures adjacent to non-pedestrian-oriented streets and not featuring a pedestrian-oriented facade shall be set back at least 10 feet from the sidewalk and feature substantial landscaping between the sidewalk and the structure. This includes a combination of evergreen and deciduous trees (one (1) per 20 lineal feet), shrubs (one (1) per 20 square feet), and ground cover (sufficient to cover 90 percent of the area within three (3) years). Other treatments will be considered in the Design Review process.
- iv. Parking garage entries shall be designed and sited to complement, not subordinate, the pedestrian entry. If possible, locate the parking entry away from the primary street, to either the side or rear of the building.
- v. The design of structured parking at finished grade under a building shall minimize the apparent width of garage entries.
- vi. Parking within the building shall be enclosed or screened through any combination of walls, decorative grilles, or trellis work with landscaping.
- vii. Parking garages shall be designed to be complementary with adjacent buildings. Use similar forms, materials, and/or details to enhance garages.
- viii. Parking structure service and storage functions shall be located away from the street edge and generally not be visible from the street or sidewalks.

**b. Parking Design and Screening**

- 1) Ground-Level Treatment: Garage facades facing a street or pedestrian path must reduce visual impacts by incorporating at least one of the following methods:
  - i. Pedestrian-Oriented Use: Provide ground-floor space along the entire garage facade (excluding driveways) for pedestrian-oriented businesses (see Figure 92.11.J). The parking area must be located behind the commercial space; or
  - ii. Screening Treatment: Cover at least 60% of the facade with the following features:
    - (1). Vegetation, such as a vertical trellis with climbing plants or a planting bed or raised planter.
    - (2). Integrated artwork, such as murals, sculptural panels, or bas-relief.
- 2) Upper-Level Treatment
  - i. Garage openings above ground level must include architectural treatment such as modulation, articulation, or screening to reduce visual impact.
- 3) Alternative design methods may be approved through Administrative Design Review process if the design is consistent with the design guidelines to mitigate parking garage impacts.

1431

**Figure 92.11.J Parking Design and Providing Space for Pedestrian-Oriented Business**



1432

## 92.16 Modulation Standards

1433

### 1. *Modulation Applicability*

1434

Buildings with street- or park-facing facades longer than 70 feet are subject to modulation requirements as specified in 92.16.A, unless exempted.

1435

1436

#### a. **Zone-Specific Standards**

1437

If the applicable zoning district includes modulation standards (see Chapters 50–57 KZC and 92.16.A), the zone-specific standards apply. The modulation standards in this chapter do not apply, unless stated otherwise in 92.16.A.

1438

1439

1440

#### b. **Height-Based Applicability**

1441

For zones without specific Modulation standards:

1442

1) Buildings two stories or fewer, modulation is not required.

1443

2) Buildings three stories or more, modulation is required, per 92.16.A.

1444

i. Prescribed Option: If Figure 92.16.A identifies a prescribed modulation option, the project must implement that specific option.

1445

ii. Optional Selection: If Figure 92.16.A allows flexibility, the applicant must select one of the modulation options listed in KZC 92.16.

1446

1447

iii. Alternative approaches may be approved through the Alternative Compliance (Design Review Board) process per KZC 142.38.2.

1448

1449

#### c. **Transition Stepbacks**

1450

Where transitional stepbacks are required elsewhere in the code, such as height limitations where a higher intensity zone abuts a lower intensity zone, any resulting stepback modulation along public streets or parks may be counted towards the modulation standards within this chapter.

1451

1452

1453

1454

1455

**Figure 92.16.A Building Design Standards Applicability Matrix**

Design District & Zone	Modulation	
	Zone-specific Code, Other Chapters.	Ch. 92 Design Standards - Upper-Level Stepbacks or Other Modulation Options
CBD 1A, CBD 1B	KZC 50.10	Multiple Smaller Forms required per KZC 92.16.4
CBD 2	KZC 50.15	Refer to Chapter 50 KZC for stepbacks, also Multiple Smaller Forms required per KZC 92.16.4
CBD 3, CBD 4, CBD 5	Chapter 50 KZC	CBD 3, 5: Refer to Chapter 50 KZC for stepbacks, also Multiple Smaller Forms required per KZC 92.16.4 CBD 6: District Specific Massing Standards 92.16.7
CBD 6, CBD 7, CBD 8	None	CBD 7: Multiple Smaller Forms required per KZC 92.16.4 CBD 6 and 8: District Specific Massing Standards 92.16.7
Market Street Corridor (MSC)		
MSC 1, MSC 2, MSC 3, MSC 4	None	Pick 2 for 3+ stories
Juanita Business District (JBD)		
JBD 1	None	Upper-Level Stepback + Pick 2 for 3+ stories
JBD 2, JBD 3, JBD 5, JBD 6	None	Buildings up to 26 feet above ABE are exempt. In the JBD 2, 3 and 6, buildings up to 39 feet above ABE must comply with zone specific view corridor standard.
JBD 4	None	Pick 2 for 3+ stories
North Rose Hill Business District (NRHBD)		
NRH 1A, NRH 1B, NRH 4, NRH 5, NRH 6	None	Pick 2 for 3+ stories
Rose Hill Business District (RHBD)		
RM 3.6, RM 2.4	None	Pick 2 for 3+ stories
RH 8	None	District Specific Massing Standards 92.16.7
PLA 17A	None	Pick 2 for 3+ stories
85 <sup>th</sup> St. Station Area		
UF, NMU, CMU, CVU	KZC 57	Exempt
South Rose Hill Business District		

Design District & Zone	Modulation	
	Zone-specific Code, Other Chapters.	Ch. 92 Design Standards - Upper-Level Stepbacks or Other Modulation Options
BCX	KZC 35.10.040	Refer to Chapter 35 KZC for stepbacks, otherwise pick 1 for 3+ stories
BN	None	Pick 2 for 3+ stories
Totem Lake Neighborhood		
TL 4A, TL 4B, TL 4C, TL 5, TL 6A, TL 6B, TL 7A, TL 7B, TL 8	None	District Specific Massing Standards 92.16.7
PR 1.8,	None	Pick 2 for 3+ stories
TL 10A, TL 10D	None	Pick 2
TL 10B	KZC 55.73(6), KZC 115.24(3)(b)	Pick 2
TL 10C	KZC 55.81: Special Regulation 3	Otherwise, Pick 2
TL 10E	KZC 55.91	Otherwise, Pick 2
Totem Center		
TI 1A, TL 1B, TL 2, TL 3A, TL 3B, TL 3C, TL 3D	Tower massing standards (Chapters 55-61 KZC & Totem Lake Design Guidelines)	Exempt
Inglewood Business District & Finn Hill Business District		
FHNC	KZC 35.10.050	Refer to Chapter 35 KZC for stepbacks, otherwise pick 2 for 3+ stories
BNA	None	Pick 2 for 3+ stories
North Juanita Neighborhood Center		
BC 1	None	Pick 2 for 3+ stories
Houghton Everest Neighborhood Center		
HENC 1, HENC 3	KZC 35.10.060	Refer to Chapter 35 KZC for stepbacks, otherwise pick 1 for 3+ stories
HENC 2	None	Pick 2 for 3+ stories
Moss Bay Business District		
BN	None	Pick 2 for 3+ stories
Yarrow Bay Business District		
YBD 1	None	Upper-Level Stepback + Pick 2 for 3+ stories
YBD 2, YBD 3	KZC 56.18	Refer to Chapter 56 KZC for stepbacks + Pick 2 for 3+ stories
PR 8.5	None	Pick 2 for 3+ stories

1457  
1458  
1459  
1460  
1461  
1462  
1463  
1464  
1465  
1466  
1467  
1468  
1469  
1470  
1471  
1472  
1473  
1474  
1475  
1476  
1477  
1478  
1479  
1480  
1481  
1482  
1483  
1484  
1485  
1486  
1487  
1488  
1489  
1490  
1491  
1492

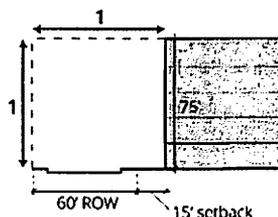
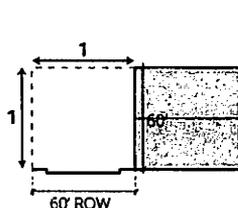
2. *Option 1. Street Width to Building Height Ratio Standards*

This standard is for all facades of buildings subject to modulation standards and over three (3) stories. The building must be tall enough to incorporate the required upper level stepback. Overall massing of the building should complement the proportions of the primary street. This technique allows for more flexibility in the size and location of a setback or upper-level stepback, subject to the following requirements:

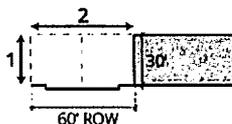
- a. Building height cannot exceed the combined width of the street right-of-way (ROW) and front yard setback. For example, a 60-foot-tall building set at the front property line along a 60-foot-wide ROW would have a 1:1 ratio. Increasing the height of this building beyond 60 feet would exceed a 1:1 ratio and would require upper-level modulation, or stepbacks, above 60 feet. See Figure 92.16.B.
  - 1) To determine if a building meets this requirement, use the following formula: Total building height / (ROW width + required setback). Building height is measured from sidewalk grade to top of vertical wall, not including parapet. If the resulting number is greater than 1.0, an upper-level stepback is required.
  - 2) The minimum depth of the stepback must be sufficient to maintain a ratio of 1.0 or less, calculated as: Total building height / (ROW width + setback + stepback). The upper-level stepback required to achieve an acceptable facade ratio may be provided at any level of the building above the ground floor level. See Figure 92.16.C.
  - 3) Where the underlying zoning maximum height is lower than the height that would trigger a 1:1 stepback, the zoning height limit applies and no stepback is required.
- b. The required upper-level stepback may be averaged along the primary frontage, provided that no continuous portion of the facade without a stepback exceeds 30 feet in width.
- c. Designs are encouraged to utilize the resulting stepback at any level to create decks, balconies, rooftop gardens and other activities overlooking the streets.
- d. In any Design District that requires an upper-level stepback, the requirement may be reduced if the lot is less than 75 feet deep and the stepback would reduce the upper-floor depth below 55 feet. A smaller stepback may be granted through the design review process to allow a functional upper story floor plate. See 92.16.D for reference.

**Figure 92.16.B Street Width to Building Height Ratio Examples**

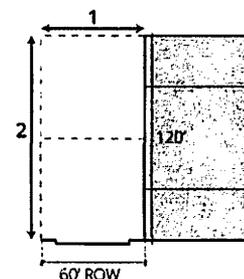
preferred:



allowed:



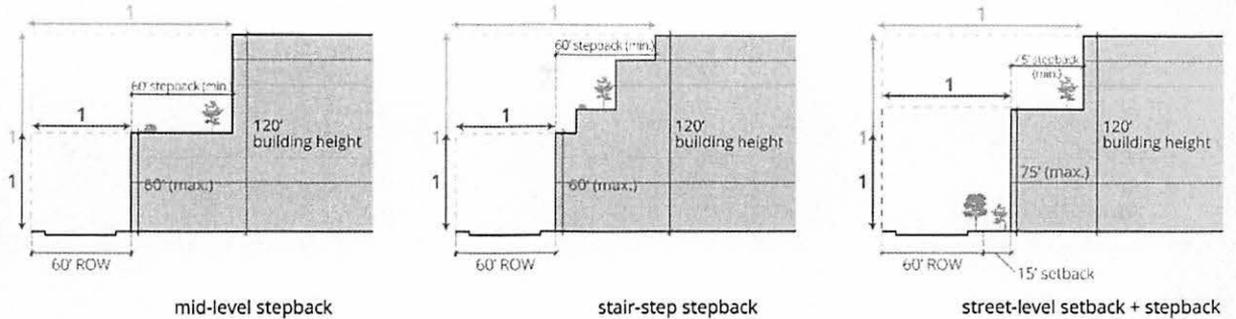
not allowed:



1493

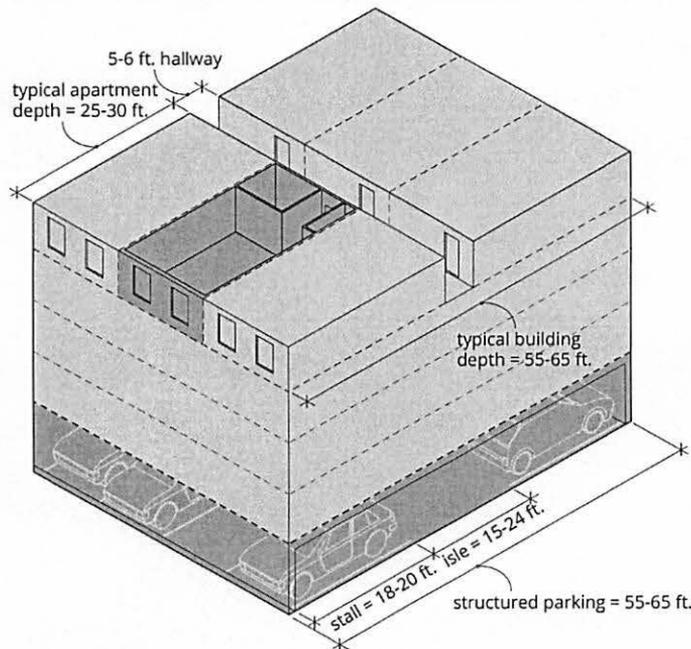
**Figure 92.16.C Stepback Options**

options for compliance:



1494

**Figure 92.16.D Typical Residential Building Example**



1495  
1496  
1497  
1498  
1499  
1500  
1501  
1502  
1503  
1504  
1505  
1506  
1507

3. *Option 2. Roofline Variation*

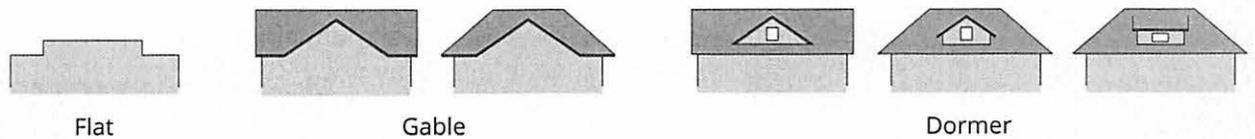
Create visual interest with changes in the roof line of all facades of the building adjoining a street or public park that are 70 feet wide or more. If the roofline and corresponding building form are less than 70 feet wide, this massing option may still be counted towards one of the required techniques as the scale of the building has already been reduced.

- a. For buildings with flat, mansard, side-gabled, or hipped roofs, the maximum length of any continuous roof line is 70 feet without variation (see Figure 92.16.E). If variation is necessary, the following methods must be used:
- b. There should be at least 25 feet of space between roof variations. See Figures 92.16.F and 92.16.G.
  - 1) Changes in roof line should follow the vertical modulations and overall

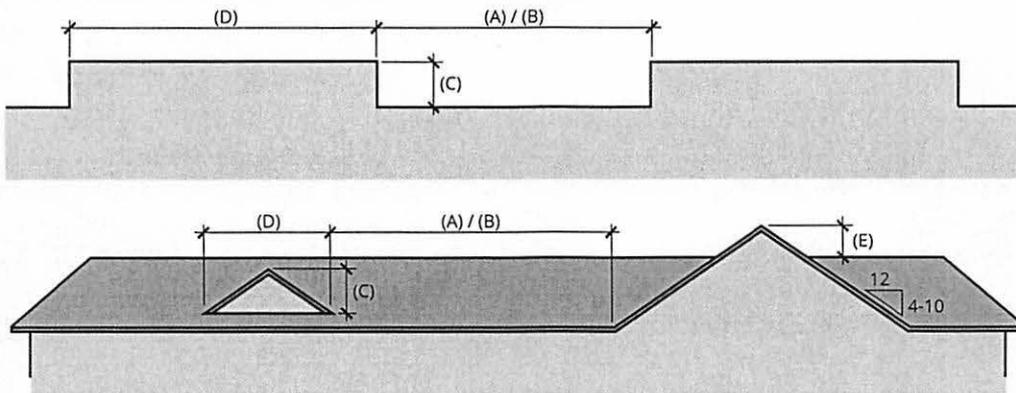
1508  
 1509  
 1510  
 1511  
 1512  
 1513  
 1514  
 1515  
 1516  
 1517  
 1518  
 1519  
 1520  
 1521  
 1522  
 1523

- form of the building.
- 2) The height of the variation must be at least five (5) feet.
  - 3) The minimum width of variations of flat roofs is 30 feet. The minimum width of variations on mansard or hipped roofs is 15 feet.
  - 4) The variation must take the form of a flat roof line, gable, or dormer. Dormers must be paired with a real window.
  - 5) For gables and dormers, the minimum slope must be between four (4) feet vertical in 12 feet horizontal, and 10 feet vertical in 12 feet horizontal. Sloped roofs used to meet this standard may utilize additional height allowances as provided elsewhere in this code.
  - 6) Dormers may take the form of a shed roof or a gabled roof.
- c. Buildings where the primary roof form is sloped, such as front-gabled or shed, are permitted without roof line variation as long as this form accounts for at least 60 percent of the street or park-facing roof line and is paired with real windows such as clerestories, gable windows, or picture windows. See Figure 92.16.G.

**Figure 92.16.E Roofline Variation Options**



**Figure 92.16.F Roofline Variation Dimensions**



**Figure 92.16.G Roofline Variations for Sloped Forms**



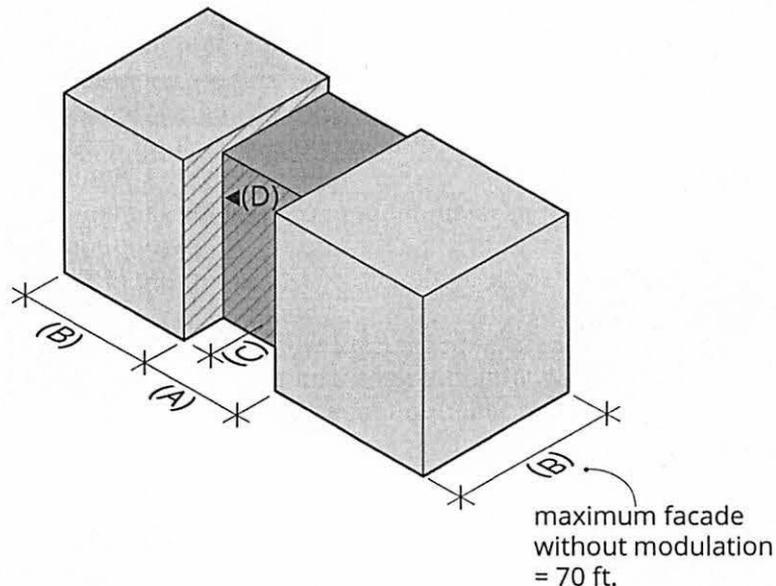
1524  
1525  
1526  
1527  
1528  
1529  
1530  
1531  
1532  
1533  
1534  
1535  
1536  
1537  
1538  
1539  
1540  
1541  
1542  
1543  
1544  
1545  
1546

4. *Option 3. Multiple Smaller Forms*

The overall massing of the building must be broken down into smaller forms to reduce the perceived scale. All building facades more than 70 feet in length and visible from a street or public park must provide vertical modulation consistent with all of the following standards:

- a. The minimum distance between modulations is 30 feet. See Figure 92.16.H(A).
- b. The maximum width of each modulation is 70 feet. The minimum width of each modulation is 20 feet. See Figure 92.16.H(B).
- c. The minimum required modulation is measured as a total area of 300 square feet. The applicant may satisfy this requirement using any configuration, provided the same total area is achieved through any combination of depths and lengths. See Figure 92.16.H(C).
- d. If modulations are paired with a change in color or material, the minimum required modulation area may be reduced to 100 square feet. Changes in color or material must occur at inside corners of the modulation only. See Figure 92.16.H(D).
- e. Changes in roof line should follow the vertical modulations and overall form of the building to contribute to the impression of multiple smaller forms.
- f. Modulations are not required at the first or second story. Above the second story, modulations must extend through all upper floors.

**Figure 92.16.H Multiple Smaller Form Dimensions**



1547  
1548  
1549  
1550  
1551  
1552  
1553

5. *Option 4- Upper Story Setback*

All stories above the second story must be set back at least 10 feet from the ground floor facade along at least two (2) facades of the building.

6. *Option 5- Upper Story Stepback*

All stories above the third story must be stepped back an average of 10 feet from the ground floor street facade of the building and a minimum of five feet.

1554  
1555  
1556  
1557  
1558  
1559  
1560  
1561  
1562  
1563  
1564  
1565  
1566  
1567  
1568  
1569  
1570  
1571  
1572

## 7. District Specific Massing Standards

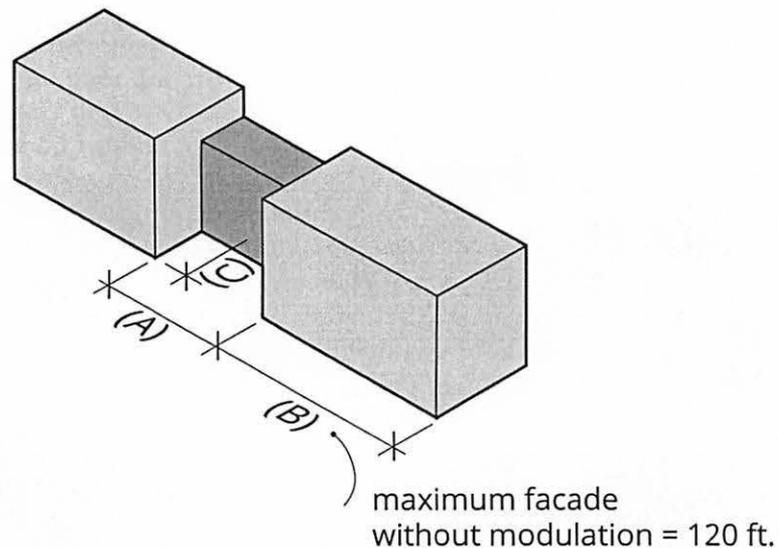
### a. Central Business District (CBD)

#### 1) Maximum Facade Width in CBD 4, CBD 6, CBD 8

Along First Street, Second Street South, First Avenue South, and Fifth Street, the maximum length of a facade is 120 feet. Any facade that exceeds 120 feet along the right-of-way must comply with the following requirements (see Figure 92.16.I):

- i. Must be divided by a 30-foot-wide modulation of the exterior wall so the maximum length of the facade is 120 feet without this modulation.
- ii. The modulation must be at least 10 feet in depth and must start at finished grade and extend through all floors.
- iii. The minimum required modulation is measured as a total area of 300 square feet. The applicant may satisfy this requirement using any configuration, provided the same total area is achieved through any combination of depths and lengths.
- iv. Decks and roof overhangs may encroach up to three (3) feet (per side) into the modulation.

**Figure 92.16.I Vertical Modulation**



1573  
1574  
1575  
1576  
1577  
1578  
1579  
1580  
1581  
1582  
1583  
1584

#### 2) Exceptions in CBD 6, CBD 8

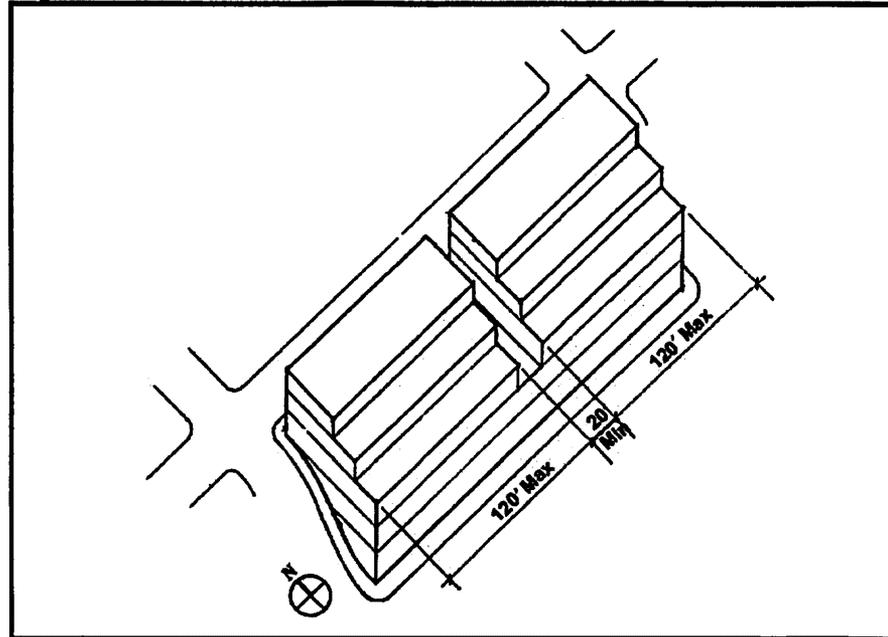
Along the axes of all buildings which are predominantly east-west and/or most closely parallel to Central Way, Third Avenue, Fourth Avenue, or Sixth Avenue, the maximum length of a building is 120 feet. The following exceptions apply (see Figure 92.16.J):

- i. Portions of buildings which are below the elevation of Third Avenue, Fourth Avenue, or Sixth Avenue, as measured at the midpoint of the frontage of the subject property on the applicable right-of-way, may exceed the 120-foot limitation.
- ii. Portions of the building above Third Avenue, Fourth Avenue, or Sixth Avenue must be divided into two (2) or more distinct building masses with a maximum length of 120 feet separated by at least 20 feet in width.

1585  
1586  
1587  
1588

- iii. Decks, bay windows, roof overhangs, and chimneys may encroach up to three (3) feet (per side) into the separation.

**Figure 92.16.J Vertical Modulation in CBD 6 and 8**



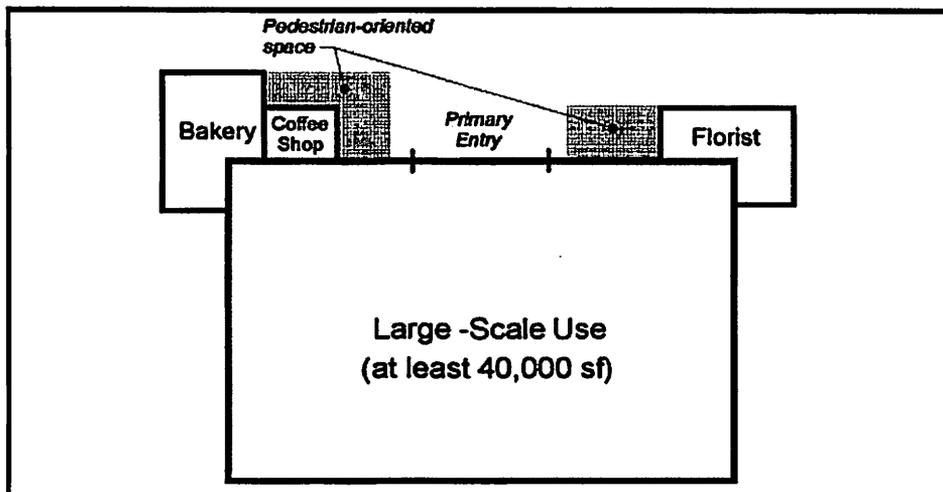
1589  
1590  
1591  
1592  
1593  
1594  
1595  
1596  
1597  
1598  
1599  
1600

**b. Nonresidential Uses in the RH8 and TLBD Zones**

- i. Nonresidential uses (office, retail, industrial, etc.) in the RH8 and TLBD with over 40,000 square feet of floor area must incorporate the following human scale features on the facade featuring the primary building entry:
- (1). Provide the required pedestrian-oriented space near the building entry (see Figure 92.05.K) This must include a covered area at least 15 feet long by 15 feet wide and is available for outdoor display, outdoor vendors, and/or seating; and
  - (2). Compose smaller building forms near the entry (see Figure 92.16.K).
  - (3). Through the administrative design review process, the applicant may propose other mechanisms for providing human scale to such buildings, consistent with the design guidelines.

1601

**Figure 92.16.K Composing Smaller Building Elements Near the Entry**



1602  
1603  
1604  
1605  
1606  
1607  
1608  
1609  
1610  
1611  
1612  
1613  
1614  
1615  
1616  
1617  
1618  
1619  
1620  
1621  
1622  
1623  
1624  
1625  
1626  
1627  
1628  
1629  
1630  
1631  
1632

**c. Totem Lake Business District (TLBD),RH8, FHNC, HENC, BCX (Bridle Trails)**

**1) Maximum Facade Width**

Along all streets, through-block pathways, and public open spaces, the maximum length of a facade is 120 feet. Any facade that exceeds 120 feet along the right-of-way must comply with the following requirements (see Figure 92.16.I):

- i. Must be divided by a 30-foot-wide modulation of the exterior wall so the maximum length of the facade is 120 feet without this modulation.
- ii. The modulation must be at least 10 feet in depth and must start at finished grade and extend through all floors. In the FHNC, HENC, BCX (Bridle Trails) the modulation requirement is above the ground floor.
- iii. The minimum required modulation is measured as a total area of 300 square feet. The applicant may satisfy this requirement using any configuration, provided the same total area is achieved through any combination of depths and lengths
- iv. Decks and roof overhangs may encroach up to three (3) feet (per side) into the modulation.

**d. CBD 1A and CBD 1B**

1) Buildings may be allowed to cantilever over sidewalks if a sidewalk dedication and/or easement is required consistent with following guidelines:

- i. The total length of cantilevered portions of a building shall be no more than 1/3rd of the entire length of the building façade.
- ii. The cantilevered portions of a building shall be spread out and not consolidated in a single area on the building façade.
- iii. Unobstructed pedestrian flow shall be maintained through the subject property to adjoining sidewalks and appear and function as part of the public realm.

1633  
1634  
1635  
1636  
1637  
1638  
1639  
1640  
1641  
1642  
1643  
1644  
1645  
1646  
1647  
1648  
1649  
1650  
1651  
1652  
1653  
1654  
1655  
1656  
1657  
1658  
1659  
1660  
1661  
1662  
1663  
1664  
1665  
1666

**92.20 Articulation Standards**

1. *Applicability*

Articulation standards in this section apply to all building facades facing a public right-of-way, through-block pathway, or a public park.

- a. Two techniques are required for facades that are:
  - 1) Three or more stories tall, or
  - 2) More than 100 feet long.
- b. One technique is required for all other applicable facades not meeting the requirements of KZC Section 92.20.1.a.
- c. Facades along internal service roads not visible from these areas may be exempted by the Planning Official.
- d. As an alternative, the applicant may propose other techniques, elements or other methods that provide articulation to the building and are consistent with the applicable design guidelines.

2. *Articulation Techniques for All Zones*

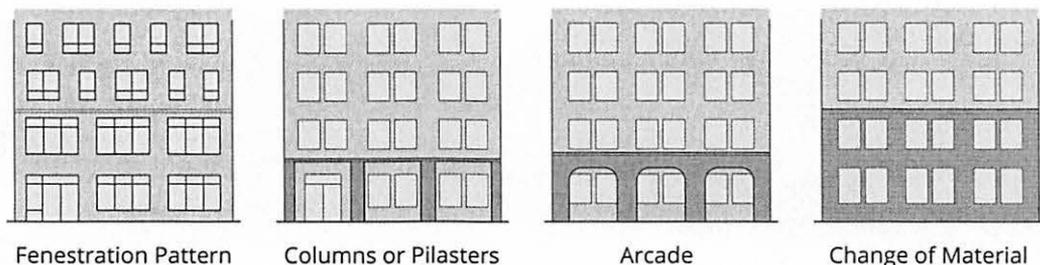
a. **Option 1. Horizontal Definition**

Define the base of the building with changes in material, form, details, window design, or other elements. The base of the building should be the first one or two floors, and should relate to the construction and use of the building. Where appropriate, the applicant should coordinate the horizontal elements (i.e., material changes, window lines, arcades, etc.) in a pattern and height to reflect similar elements on neighboring buildings.

The following elements may be used to differentiate the base from the rest of the building. At least two must be used (see Figure 92.20.A):

- 1) Fenestration pattern such as larger windows on the base and smaller windows above.
- 2) Engaged columns or pilasters.
- 3) An arcade stretching the majority of the frontage. The sidewalk level within the arcade must be at or within 1 foot of the public sidewalk. Columns must be at least 10 feet apart to allow easy flow of pedestrians into and out of the arcade.
- 4) Use of masonry on the base and a change of material at the upper levels.

**Figure 92.20.A Horizontal Definition Examples**



b. **Option 2. Balconies**

On each story above the ground floor, provide balconies or decks. Balconies must be consistent with the following standards:

- 1) Balconies must be placed on every floor above the ground floor. The top

1667  
1668  
1669  
1670  
1671

1672  
 1673  
 1674  
 1675  
 1676  
 1677  
 1678  
 1679  
 1680  
 1681  
 1682  
 1683  
 1684  
 1685  
 1686  
 1687  
 1688  
 1689  
 1690  
 1691  
 1692  
 1693  
 1694  
 1695  
 1696  
 1697  
 1698  
 1699  
 1700  
 1701  
 1702  
 1703  
 1704  
 1705

floor may also be exempt if it is set back from the facade below.

- 2) Be at least three (3) feet deep.
- 3) The maximum distance between balconies, measured horizontally, is 100 feet; and
- 4) The minimum amount of floor area for each balcony is 18 square feet.

**c. Option 3. Bay Windows**

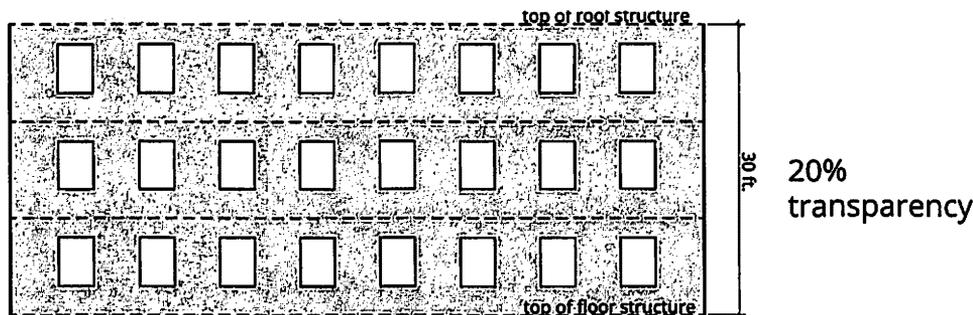
On each story above the ground floor, provide bay windows that extend out at least one foot, measured horizontally, from each facade of the building.

**d. Option 4. Window Design**

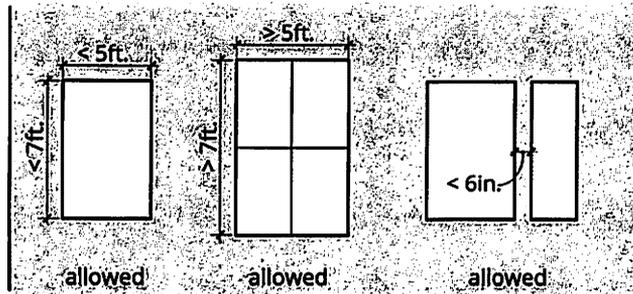
The majority of windows above the ground floor of each facade must be consistent with all of the following criteria:

- 1) Windows must achieve at least 20 percent transparency, measured as a percentage of the total area of the facade of the upper floors, from top of floor structure to top of roof structure or bottom of roof soffit (see Figure 92.20.B).
- 2) The windows must have glazed areas with dimensions less than five (5) feet by seven (7) feet. Larger windows may be used if they are divided into smaller glazed areas by mullions (see Figure 92.20.C).
- 3) Individual window units must be separated from adjacent window units by at least six (6) inches.
- 4) For at least half of the upper-level windows, window frames must contrast the surrounding facade material, or be surrounded by casing that contrasts the facade material.
- 5) Windows must be arranged in logical patterns that relate to the interior spaces as well as the surrounding fenestration and other facade details.
- 6) Ribbon windows and extensive use of mirrored glass are not permitted.
- 7) Consideration for Market Street Corridor: Window treatment in the historic district should reflect the trim detailing, size, proportions, location and number of windows in the existing historic buildings in the district. Compliance with this preference may be counted towards one of the required articulation strategies.

**Figure 92.20.B Facade Transparency**



1706

**Figure 92.20.C Window Sizes**

1707

1708

1709

1710

1711

1712

1713

1714

1715

1716

1717

1718

1719

1720

1721

1722

1723

1724

1725

**92.25 Blank Walls Treatment**

1726

**1. Blank Wall Definition**

1727

A blank wall is any wall or portion of a wall that meets either of the following criteria (see Figure 92.25.A).

1728

1729

1730

1731

1732

1733

1734

**e. Option 5. Covered Entry Feature**

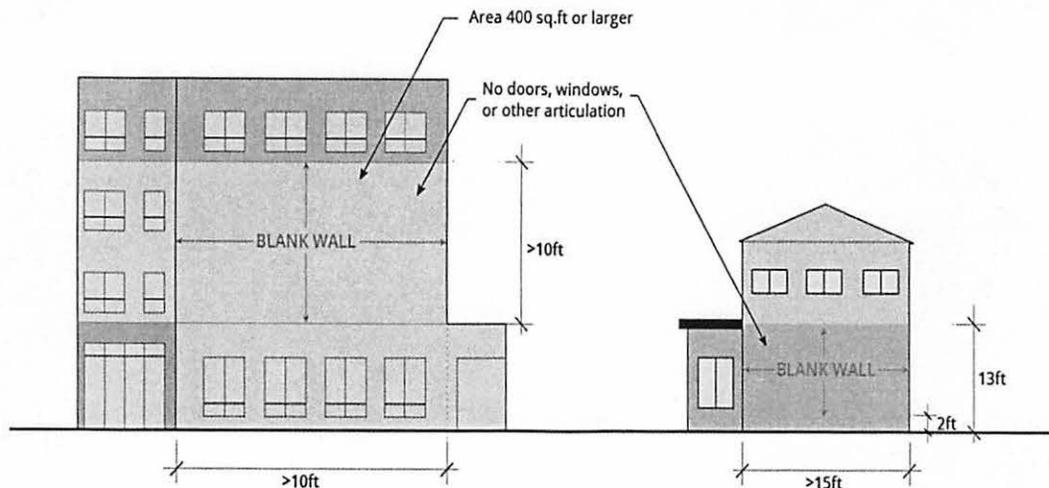
If the main entrance of the building is on the facade of the building facing a street, through-block pathway, or public park, provide a covered porch or entry on the subject property at the building's main entrance. The covered porch or entry area must be at least 10 feet deep and at 150 square feet total.

Pedestrian weather protection required under KZC 105.18 may not be used to meet this requirement unless the required pedestrian weather protection covers an area at least 15 feet long by 15 feet wide and is available for outdoor display or outdoor vendors or contains pedestrian-oriented improvements or amenities beyond what is otherwise required.

**f. Option 6. Functional Sunshades**

Include functional sunshades on the majority of all windows. The effectiveness of the sunshades in blocking direct sun during the warmest parts of the year must be demonstrated through a rendering or diagram to be submitted with the application.

1735

**Figure 92.25.A Blank Wall Definition**

1736

**2. Blank Wall Treatments for All Zones**

1737

At least one (1) of the following techniques must be used to treat or screen blank walls:

1738

- a. Install and maintain a vertical trellis with climbing plants; or

1739

- b. Provide a planting bed at least five (5) feet wide or a raised planter at least two (2) feet high and three (3) feet wide, planted to screen at least 50% of the facade; or

1741

1742

- c. Incorporate artwork, such as murals, sculptural panels, or bas-relief; or

1743

- d. Propose alternative techniques as part of the administrative design review process. The proposed technique shall be consistent with the applicable design guidelines.

1744

1745

1746

1747

**92.28 Building Material, Color and Detail**

1748

**1. Required Elements - All Zones**

1749

The applicant must incorporate at least three of the following elements on each facade of a building that faces a street, through-block pathway, pedestrian-oriented space or a public park. Applicants may propose other mechanisms for providing interesting visual detail to buildings, consistent with the applicable design guidelines, through the administrative design review process.

1750

1751

1752

1753

1754

- a. Decorative cornices, including ornamental molding, frieze or other roof line devices visible from the ground. Linear features must be at least eight inches wide, measured vertically.

1755

1756

- b. Decorative molding or framing details around all ground floor doors and windows. The molding or trim may have a traditional, contemporary, geometric or sculptural design.

1757

1758

1759

1760

- c. Decorative glazing on all ground floor doors and windows, including stained glass, mullions, colored glass, crystal cut glass, etched glass or similar individualized and permanent treatment, but excluding opaque glass or plastic. On all ground floor windows, this decorative glazing must have a surface area of at least 30 square feet.

1761

1762

1763

1764

1765

1766

- d. Decorative railings, grill work, landscape guards or other similar elements that go beyond the normal appearance of the element.

- 1767  
1768  
1769  
1770  
1771  
1772  
1773  
1774  
1775  
1776  
1777  
1778  
1779  
1780  
1781  
1782  
1783  
1784  
1785  
1786  
1787  
1788  
1789  
1790  
1791  
1792  
1793  
1794  
1795  
1796  
1797  
1798  
1799  
1800  
1801  
1802  
1803  
1804  
1805  
1806  
1807  
1808  
1809  
1810  
1811  
1812  
1813  
1814  
1815  
1816
- e. Trellises or arbors having an area of at least 100 square feet and planted consistent with the requirements of KZC 95.41 to achieve at least 30 percent coverage of the trellis or arbor with plant material within three years.
  - f. Decorative light fixtures, either one if one-of-a-kind or custom-built or at least one every 30 feet along the facade of the building if not one-of-a-kind or custom-built, that meet either of the following criteria:
    - 1) Includes a diffuse, visible light source, such as a globe.
    - 2) Contains a shade or mounting that includes some use of material, configuration, shape, embellishment or detail that exceeds the normal functional requirement for the shade or mounting.
  - g. Use of any of the following decorative materials:
    - 1) Decorative masonry patterns, other than running bond pattern.
    - 2) Bricks, tile, stone, cast stone or other masonry units of at least two colors installed in layers or tiers to form a geometric pattern.
    - 3) Individualized wood patterns or continuous wood details in a geometric pattern, decorative moldings, brackets, eave trim or lattice work.
    - 4) Ceramic tile, stone, glass blocks, camera glass or other similar materials incorporated into other compatible surface materials and used to form or create, or in conjunction with, a geometric pattern, distinctive shape, unusual surface treatment, special lighting or other decorative or textural element.
    - 5) Other materials with decorative or textural qualities, as demonstrated by architectural drawings and material samples, approved by the City as part of Administrative Relief or Alternative Compliance.
  - h. Decorative unit paving, including at least 50 square feet of multicolored tile, paver blocks, brick or other paving material in a decorative pattern, installed in a pedestrian-circulation area adjacent to the facade.
  - i. Artwork in the form of a mosaic mural, bas-relief sculpture, light sculpture, water sculpture, fountain, freestanding sculpture, art in pavement, murals, graphics or other forms, either freestanding in front of the facade or attached to the facade.
2. *Special Considerations*
- a. In Bridle Trails Neighborhood Center (BCX Zone), special attention to the use of colors and materials should be used on a building's upper stories to reduce the appearance of taller buildings.
  - b. In the MSC 3 zone, emphasis on building features such as doors, windows, cornice treatment, bricks and ornamental masonry should be taken into consideration when designing new or remodeled buildings. These features should be in keeping with the building materials, colors and details of the existing historic buildings.
3. *Prohibited Materials – All Zones*
- The following materials may not be used on any exterior surface which is visible from any area beyond the subject property:
- a. Mirrored glass and other highly reflective materials.
  - b. Corrugated fiberglass.
  - c. Chain link fencing, except for temporary purposes, such as during construction.

- 1817 4. *Metal Siding – All Zones*  
 1818 Corner and edge trim must be used to cover exposed edges of metal siding. If metal siding  
 1819 covers more than 25 percent of a building's facade, the following regulations apply:  
 1820 a. The siding must have a matted finish.  
 1821 b. The siding must be in a neutral, earth tone or dulled color such as buff, grey,  
 1822 beige, tan, creme, white, barn-red, blue-grey, burgundy or ocher.  
 1823 c. The facade must have visible window and door trim painted or finished in a  
 1824 color which is complementary to the siding color.  
 1825
- 1826 5. *Concrete Block – All Zones*  
 1827 Any concrete block, masonry unit or cinder block wall which is visible from a street or public  
 1828 park must contain one (1) or more of the following features or elements:  
 1829 a. Use of textured blocks with surfaces such as split-faced or grooved.  
 1830 b. Use of colored mortar complementary to the color of the blocks.  
 1831 c. Use of other surface material such as bricks, glass blocks or tile as a significant  
 1832 feature of the wall.  
 1833
- 1834 6. *Awnings – All Zones*  
 1835 See below general standards and Chapter 105.18 for other pedestrian overhead weather  
 1836 protection requirements.  
 1837 a. The design of awnings should complement the architecture of the building.  
 1838 Steel and glass, fabric, and other materials of a more permanent nature are  
 1839 encouraged. Vinyl or plastic awnings and awnings used predominantly for  
 1840 advertising are discouraged.  
 1841 b. Translucent awnings can not be backlit. Lights directed downward mounted  
 1842 from internal awning frames are permitted. Lights mounted above awnings and  
 1843 directed downward are permitted.  
 1844
- 1845 7. *Covering of Existing Facades – All Zones*  
 1846 Existing brick or cast stone masonry facades may not be covered with metal siding, metal  
 1847 screening, plastic siding, fiberglass siding, plywood siding, or wood siding materials. Other  
 1848 existing facades may be covered if consistent with the provisions of this subsection (4)(g).  
 1849 As part of Design Review for remodels, the City may require the removal of coverings.  
 1850
- 1851 8. *Building Cornerstone or Plaque – All Zones*  
 1852 All commercial buildings designed for use by more than one (1) tenant must have a building  
 1853 cornerstone or plaque, placed in a prominent location, consistent with the following  
 1854 standards:  
 1855 a. Building cornerstones must be constructed in carved stone, cast stone, carved  
 1856 masonry, terra cotta or other vandal-resistant material.  
 1857 b. Building plaques must be mounted no lower than two (2) feet and no higher  
 1858 than 10 feet above ground and must be made of bronze, brass, anodized  
 1859 aluminum, porcelain enamel-covered steel or aluminum or other corrosion-  
 1860 resistant material.  
 1861 c. Building cornerstones and plaques must indicate the name of the building and,  
 1862 if known, the date of construction and architect.  
 1863 d. Building cornerstones and plaques may include the owner's name and other  
 1864 historical information.  
 1865

1866 9. *Required On-Site Improvements – All Zones*  
 1867 Water spigots must be provided on all building facades along sidewalks for cleaning and  
 1868 plant watering.  
 1869

1870 Section 29. Chapter 142 of the Kirkland Zoning Code, entitled "Design Review," is  
 1871 hereby repealed in its entirety and replaced with a new Chapter 142 to read as follows:  
 1872

1873 **Chapter 142**  
 1874 **DESIGN REVIEW**  
 1875

1876 **142.05 User Guide**

1877 Various places in this code indicate that certain developments, activities, or uses are required  
 1878 to be reviewed through design review (D.R). All design review will be administrative design  
 1879 review (ADR). Applicants may also elect to go through Design Board Review (DBR) if they are  
 1880 seeking a modification through the Alternative Compliance process as outlined in Section  
 1881 142.26. This chapter describes these design review processes.  
 1882

1883 **142.26 Administrative Design Review (ADR) Process**

1884 1. *Timing and Authority* – The Planning Official will conduct ADR in conjunction with a related  
 1885 development permit or as part of early design review pursuant to this chapter. An applicant  
 1886 may submit a building permit application at any time during the design review process. The  
 1887 Planning Official will review the ADR application for compliance with the relevant design  
 1888 standards in Chapter 92 KZC.  
 1889

1890 2. *Application Types*

1891 a. ADR 1: The following development activities shall be reviewed through the ADR 1  
 1892 process:

- 1893 1) New buildings greater than one (1) story in height or greater than 10,000 square  
 1894 feet of gross floor area;
- 1895 2) New buildings in the Market Street Corridor Historic District (MSC 3 Zone);
- 1896 3) Additions to existing buildings where:
  - 1897 a) The new gross floor area is greater than 10 percent of the existing  
 1898 building's gross floor area; and
  - 1899 b) The addition is greater than 2,000 square feet of gross floor area; and
  - 1900 c) Either:
    - 1901 1) The existing building and addition total more than 10,000 square feet  
 1902 of gross floor area; or
    - 1903 2) The addition adds another story; or
    - 1904 3) Is in the Market Street Corridor Historic District (MSC 3 zone).
- 1905 4) Renovations to existing facades, where the building is identified by the City as an  
 1906 historic structure or is in the Market Street Corridor Historic District (MSC 3 zone).

1907 b. ADR 2: The following development activities shall be reviewed through the ADR 2  
 1908 process:

- 1909 1) Any project below the minimum thresholds for ADR 1 review.
- 1910 2) Any development within the Totem Lake (TL) 7B zone and Rose Hill Business  
 1911 District (RH) 8 zone.
- 1912 2) Any development in the MSC 1 and MSC 4 zones located within the Market Street  
 1913 Corridor.

1914 c. Alternative Compliance: Any ADR 1 or ADR 2 application that will be reviewed and  
 1915 decided on by the Design Review Board (DRB).  
 1916

- 1917  
1918  
1919  
1920  
1921  
1922  
1923  
1924  
1925  
1926  
1927  
1928  
1929  
1930  
1931  
1932  
1933  
1934  
1935  
1936  
1937  
1938  
1939  
1940  
1941  
1942  
1943  
1944  
1945  
1946  
1947  
1948  
1949  
1950  
1951  
1952  
1953  
1954  
1955  
1956  
1957  
1958  
1959  
1960  
1961  
1962  
1963  
1964  
1965  
1966
3. *Exemptions from Design Review.* The following development activities shall be exempt from design review and compliance with the design regulations of chapter 92 KZC:
    - a. Any activity which does not require a building permit; or
    - b. Interior work that does not alter the exterior of the structure; or
    - c. Normal building maintenance including the repair or maintenance of structural members; or
    - d. Any development listed as exempt in the applicable Use Zone Chart; or
    - e. Development of a fire station.
  4. *Pre-Design Meeting* – Before applying for an ADR 1 application (with or without Alternative Compliance), the applicant shall complete a pre-design meeting with the Planning Official. A pre-design meeting is not required for an ADR 2 project but is recommended. The meeting will be scheduled by the Planning Official upon written request by the applicant. The purpose of this meeting is to provide an opportunity for an applicant to discuss the project concept with the Planning Official and for the Planning Official to designate which design regulations apply to the proposed development based primarily on the location and nature of the proposed development.
  5. *Application*
    - a. As part of any application for a development permit requiring ADR, the applicant shall show compliance with the design regulations in Chapter 92 KZC by submitting an ADR application on a form provided by the Planning and Building Department. The application shall include all documents and exhibits listed on the application form.
    - b. An applicant may request early design review, but such review shall not be considered a development permit or authorization to commence a use or development activity.
  6. *Determination of Completeness of Application for ADR 1 Applications*
    - a. **General** – Within 28 calendar days after the date of submittal of the application, the Planning Official shall mail to the applicant, or provide in person to the applicant, a written determination of whether the application is complete, and if the application is not complete, what must be submitted by the applicant in order for the application to be complete. In this written determination, the Planning Official shall also identify, to the extent known to the City, the other agencies of local, state or federal government that may have jurisdiction over some aspect of the proposed development activity. The Planning Official may also include other information with this determination that will assist in the review and decision upon the application.
    - b. **Standard for Determining Completeness** – An application is complete for purposes of this section when it contains the information required by subsection 5 of this section and is sufficient for continued processing even though additional information may be required or modification of the proposal may be subsequently undertaken. A determination of completeness under this section does not preclude the City from requesting additional information or studies either at the time of providing the written determination of completeness or subsequently if new information is required.
    - c. **Review of Additional Information** – The Planning Official shall provide a written determination under subsection 6(a) of this section within 14 calendar days of the date the applicant submits additional information required under this section.
    - d. **Application Considered Withdrawn** – Unless the notice specifying additional information to be submitted for a complete application establishes a longer period, the application will be considered withdrawn for all purposes if the applicant has not submitted the required information to the City within 90 days after the date of the notice

or, if applicable, after any extension granted in writing by the Planning and Building Director. The Planning and Building Director may grant an extension if, based on information submitted by the applicant with the request for the extension, the Planning and Building Director concludes that the applicant is making reasonable progress toward submitting the required information.

7. *Public Notice*

a. ADR 1 applications shall be noticed pursuant to the requirements in Section 7.c:

b. ADR 2 applications shall not be noticed.

c. The Planning Official shall prepare a notice of application containing the following information:

1) Contents of Notice

a) The street address of the subject property, or if this is not available, a locational description in nonlegal language along with a vicinity map that identifies the subject property.

b) The date of application and completeness.

c) The date of the notice.

d) A description of the proposed development activity and the decision that will be made under this chapter; a list of the permits and approvals included in the application; and an identification of other necessary permits not included in the application to the extent known by the City.

e) The location where the official file may be examined.

f) The time limit, which will not be less than 18 calendar days nor more than 30 calendar days after the date of the notice, for submitting comments to the Planning Official, which the Planning Official shall consider prior to making a recommendation on the application.

g) Information regarding how to request a copy of the decision once it is made.

h) General information regarding how to appeal decisions made under this chapter.

i) The name of the applicant.

2) Distribution of Notice

a) Not more than 10 calendar days after the Planning Official determines that the application is complete, and at least 18 calendar days prior to the end of the comment period, the Planning Official shall distribute this notice as follows:

1. The notice, or a summary thereof, including a vicinity map, will be distributed to the owners of all property within 300 feet of any boundary of the subject property.

2. The notice, or a summary thereof, including a vicinity map, will be distributed to all residents within 300 feet of any boundary of the subject property.

3. The notice, or a summary thereof, will be published in the official newspaper of the City. The published notice does not require a vicinity map.

4. The notice, or a summary thereof, including a vicinity map, will be posted on each of the official notification boards of the City.

5. The notice, or a summary thereof, will be distributed to each local, state and federal agency that the City knows has jurisdiction over the proposed development activity.

6. The notice will be posted on the City's website.

b) Not more than 10 calendar days after the Planning Official determines that the

- 2017 application is complete, and at least 18 calendar days prior to the end of the  
 2018 comment period, the applicant shall provide for and erect public notice signs as  
 2019 follows:
- 2020 1. The signs shall be designed and constructed to City standards. A copy of  
 2021 the notice described in subsection c(1) of this section and a site plan and/or  
 2022 vicinity map shall be attached to each sign.
  - 2023 2. The Planning and Building Department is authorized to develop the  
 2024 standards for the public notice signs necessary for implementation of this  
 2025 section.
  - 2026 3. One (1) sign shall be erected on or near the subject property facing each  
 2027 public right-of-way adjacent to the subject property and private easement or  
 2028 tract road providing primary vehicular access to the subject property and to  
 2029 any property that abuts the subject property. The Planning and Building  
 2030 Department shall approve the location of each sign.
  - 2031 4. The sign(s) may not be removed until 21 calendar days after the final  
 2032 decision of the City on the application, and the applicant shall remove the  
 2033 signs within seven (7) calendar days thereafter.

#### 2034 **142.30 Decision Process**

- 2036 1. *Additions or Modifications to Existing Buildings.*
  - 2037 a. Applications involving additions or modifications to existing buildings must comply with  
 2038 the design regulations of Chapter 92 KZC to the extent feasible depending on the scope  
 2039 of the project. The Planning Official may waive compliance with a particular design  
 2040 standard if the applicant demonstrates that it is not feasible given the existing  
 2041 development and scope of the project.
  - 2042 b. The Planning Official may waive the ADR process for applications involving additions  
 2043 or modifications to existing buildings if the design standards are not applicable to the  
 2044 proposed development activity.
- 2046 2. *ADR 1 Applications:* After reviewing the ADR application and other application materials,  
 2047 the Planning Official may grant, deny or conditionally approve the proposed application.  
 2048 No development permit for the subject property requiring ADR approval will be issued until  
 2049 the proposed development is granted ADR approval or conditional approval. The terms of  
 2050 ADR approval or conditional approval will become a condition of approval on each  
 2051 subsequent development permit, and no subsequent development permit will be issued  
 2052 unless it is consistent with the ADR approval or conditional approval. The Planning Official  
 2053 shall send written notice of the ADR decision to the applicant and all other parties who  
 2054 submitted written comments during the public noticing period. If the ADR application is  
 2055 denied, the decision shall specify the reasons for denial.  
 2056  
 2057 The final ADR decision of the City on the ADR application shall be the date of distribution  
 2058 of the written ADR decision or, if the ADR decision is appealed, the date of the City's final  
 2059 decision on the appeal. Notwithstanding any other provision of this code, if an applicant  
 2060 submits a complete application for a building permit for the approved ADR development  
 2061 within 180 days of the final ADR decision, the date of vesting for the building permit  
 2062 application shall be the date of the final ADR decision.
- 2063 3. *ADR 1 or 2 with Alternative Compliance:* After reviewing the ADR and Alternative  
 2064 Compliance modification application, the DRB may grant, deny or conditionally approve  
 2065  
 2066

2067 the application. No development permit for the subject property requiring DRB approval will  
 2068 be issued until the proposed development is granted DRB approval or conditional approval.  
 2069 The terms of DRB approval or conditional approval will become a condition of approval on  
 2070 each subsequent development permit and no subsequent development permit will be  
 2071 issued unless it is consistent with the DRB approval or conditional approval. If the  
 2072 application is denied, the decision shall specify the reasons for denial.  
 2073

2074 The Planning Official shall send written notice of the DRB decision to the applicant and all  
 2075 other parties who submitted written comments during the public noticing period.  
 2076

2077 The final decision of the City on the application shall be the date of distribution of the written  
 2078 DRB decision or, if the DRB decision is appealed, the date of the City's final decision on  
 2079 the appeal. Notwithstanding any other provision of this code, if an applicant submits a  
 2080 complete application for a building permit for the approved DRB development within 180  
 2081 days of the final DRB decision, the date of vesting for the building permit application shall  
 2082 be the date of the final DRB decision.  
 2083

2084 4. *ADR 2 Applications:* After reviewing the ADR application and other application materials,  
 2085 the Planning Official may grant, deny or conditionally approve the proposed application.  
 2086 The ADR approval or conditional approval will become conditions of approval for any  
 2087 related development permit, and no development permit will be issued unless it is  
 2088 consistent with the ADR approval or conditional approval.  
 2089

2090 5. *Additional Approval Provision for TL 2 and TL 5* – The Notice of Approval for a Conceptual  
 2091 Master Plan (CMP) shall set thresholds for subsequent ADR review of projects following  
 2092 approval of a CMP in TL 2 or TL 5. The Notice of Approval shall also include a phasing  
 2093 plan for all improvements shown or described in the CMP.  
 2094

#### 2095 **142.38 Design Modifications and Alternative Compliance**

2096 General – This section provides two optional review processes for an applicant to request  
 2097 modifications to the design regulations in KZC Chapter 92.  
 2098

##### 2099 1. *Design Modifications*

2100 a. **General** – This section provides a mechanism for obtaining approval to depart from  
 2101 strict adherence to the design regulations or for requesting minor variations from  
 2102 requirements in the following zones. Required yards may reduced up to 50 percent of  
 2103 the requirement.

2104 1. In the CBD and YBD: minimum required yards; and

2105 2. In the TL 1 and TL1B: minimum required yards, floor plate maximums and building  
 2106 separation requirements; and

2107 3. In the RHBD, FHNC, the HENC, and the TLBD: minimum required yards; and

2108 4. In the MSC 1 and MSC 4 zones of the Market Street Corridor: minimum required  
 2109 front yards; and

2110 5. In the MSC 2 zone of the Market Street Corridor: height (up to an additional five (5)  
 2111 feet), and minimum required front yards.

2112 This section does not apply when a design regulation permits the applicant to propose  
 2113 an alternate method for complying with it or the Use Zone Chart allows the applicant to  
 2114 request a reduced setback administratively.

2115 b. In all design districts, the articulation standards in KZC Chapter 92 may be modified by  
 2116 up to 30%.

- 2117  
2118  
2119  
2120  
2121  
2122  
2123  
2124  
2125  
2126  
2127  
2128  
2129  
2130  
2131  
2132  
2133  
2134  
2135  
2136  
2137  
2138  
2139  
2140  
2141  
2142  
2143  
2144  
2145  
2146  
2147  
2148  
2149  
2150  
2151  
2152  
2153  
2154  
2155  
2156  
2157  
2158  
2159  
2160  
2161  
2162  
2163  
2164  
2165  
2166
- c. Design departure and minor variations to development standards are authorized as specified in Chapter 57 KZC (Form-Based Code for the NE 85th Street Station Area Plan).
  - d. **Application Information** – The applicant shall submit a complete application on the form provided by the Planning and Building Department, along with all information listed on that form, including a written response to the criteria in subsection 1(f) of this section.
  - e. **Process** – Design Modifications will be reviewed and decided upon using the ADR 2 process.
  - f. **Criteria** – The Planning Official may grant a design modification only if it finds that all of the following requirements are met:
    - 1. The request fulfills the policy basis for the applicable design regulations and design guidelines;
    - 2. The departure will not have any substantial detrimental effect on nearby properties and the City or the neighborhood.
2. *Alternative Compliance – Design Board Review*
- a. **General** – Alternative Compliance should be sought for significant modifications, including modifications that exceed the thresholds in KZC Section 142.38.1, that seek to meet the intent of the design standard through an alternative design technique or mechanism. The DRB will review the Alternative Compliance modification application for compliance with the criteria in KZC Section 142.38.2.d.
  - b. **Application Information** – The applicant shall submit a complete application on the form provided by the Planning and Building Department, along with all information listed on that form, including a written response to the criteria in subsection 2(d) of this section.
  - c. **Process**
    - 1. **Public Notice**
      - a) The complete ADR application, including the request for modification through the Alternative Compliance Process, will be noticed pursuant to the requirements in KZC Section 142.26(7).
      - b) Within 90 calendar days of the determination of a complete application, the Planning Official will schedule a design review meeting with the DRB.
      - c) The Planning Official shall distribute this notice at least 14 calendar days before the design review meeting. The distribution shall follow the requirements outlined in KZC Section 142.26(7).
    - 2. **Design Review Meeting**
      - a) All meetings of the DRB must be public meetings and open to the public.
      - b) The design review meeting allows the Design Review Board to review the alternative design plans for final approval. The applicant must present a summary of the project and proposed design alternative to the DRB. The Planning Official must present a review of the design alternative for consistency with the requirements specified in subsection 4 of this section and compliance with the design regulations in KZC Chapter 92. Seven calendar days prior to the meeting, the Planning Official shall distribute the staff report to the applicant and DRB. The staff report will also include staff's review of the ADR application for compliance with Chapter 92 requirements.
      - c) Public comment relevant to the application may be taken. Persons commenting must provide their full name and email address. The DRB may reasonably limit the extent of comments to facilitate the orderly and timely conduct of the conference.

- 2167 d) The DRB must decide whether the application complies with the requirements  
 2168 specified in subsection 4 of this section. The DRB must review and make a  
 2169 decision on the application at a single public meeting.  
 2170 d. **Approval Criteria** – An Alternative Compliance request may be granted by the DRB if  
 2171 the following criteria are met:  
 2172 1. The application of certain provisions of the design standards would result in  
 2173 practical difficulties or unnecessary hardships inconsistent with the general purpose  
 2174 and intent of the design standards;  
 2175 2. Permitting the design alternative will be consistent with the objectives of the design  
 2176 guidelines;  
 2177 3. Permitting the alternative will not be materially detrimental to the public welfare or  
 2178 injurious to the property or improvements in the area.  
 2179

#### 2180 **142.41 Appeals**

- 2181 1. *ADR 2 Applications*: No administrative appeal process.  
 2182  
 2183 2. *ADR 1 and ADR 2 Applications with Alternative Compliance*: The decision may be  
 2184 appealed using the appeal provisions of Process I of this code, KZC 145.60 through  
 2185 145.105.  
 2186

#### 2187 **142.50 Modifications to an Approved Project**

- 2188 1. The Planning Official may approve a modification to the ADR or Alternative Compliance  
 2189 approval for the proposed development if:  
 2190 a. The need for the modification was not known and could not reasonably have been  
 2191 known before the approval was granted;  
 2192 b. The modification is minor and will not, in any substantial way, change the proposed  
 2193 development or violate any requirement imposed by the Planning Official or DRB.  
 2194 The Planning Official may consult with the DRB regarding modifications to a project  
 2195 previously approved by the DRB.  
 2196 c. The development that will result from the modification will be consistent with the  
 2197 applicable design regulations in the case of an ADR approval or design guidelines in  
 2198 the case of an Alternative Compliance approval.  
 2199  
 2200 2. Except as specified in subsection (1) of this section, any modification must be reviewed  
 2201 and decided upon as a new application under this chapter.  
 2202

#### 2203 **142.56 Lapse of Approval**

- 2204 1. *ADR 1 and ADR 2 Applications with Alternative Compliance*  
 2205 a. For final approvals of under this chapter, the applicant must begin construction or  
 2206 submit to the City a complete building permit application for the development activity,  
 2207 use of land or other actions approved under this chapter within five (5) years after the  
 2208 final approval of the City of Kirkland on the matter, or the decision becomes void.  
 2209 b. For final approvals under this chapter, the applicant must substantially complete  
 2210 construction for the development activity, use of land, or other actions approved under  
 2211 this chapter and complete the applicable conditions listed on the notice of decision  
 2212 within seven (7) years after the final approval on the matter, or the decision becomes  
 2213 void.  
 2214 c. For development activity, use of land, or other actions with phased construction, lapse  
 2215 of approval may be extended when approved under this chapter and made a condition  
 2216 of the notice of decision.

2217 2. *ADR 2 Applications* – The lapse of approval for the ADR decision will be tied to the  
 2218 development permit and all conditions of the ADR approval will be included in the  
 2219 conditions of approval granted for that development permit.  
 2220

2221 **142.60 Bonds**

2222 The Planning Official may require a bond under Chapter 175 KZC to ensure compliance with  
 2223 any aspect of a D.R. approval.  
 2224

2225 Section 30. The document referenced in Section 3.30.040(1) of the Kirkland Municipal  
 2226 Code (KMC), which document is entitled "Design Guidelines for Pedestrian Oriented Business  
 2227 Districts," is hereby amended as set forth in Exhibit A to this ordinance, which is incorporated  
 2228 by reference. The Director of Planning and Building is directed to incorporate the amendments  
 2229 into a final version for signature.  
 2230

2231 Section 31. The document referenced in KMC 3.30.040(2), entitled "Design Guidelines  
 2232 for NE 85th Street Station Subarea Plan," is hereby amended as set forth in Exhibit B to this  
 2233 ordinance, which is incorporated by reference. The Director of Planning and Building is directed  
 2234 to incorporate the amendments into a final version for signature.  
 2235

2236 Section 32. The document referenced at KMC 3.30.040(3), entitled "Design Guidelines  
 2237 for the Totem Lake Business District," is hereby amended as set forth in Exhibit C to this  
 2238 ordinance, which is incorporated by reference. The Director of Planning and Building is directed  
 2239 to incorporate the amendments into a final version for signature.  
 2240

2241 Section 33. The document referenced at KMC 3.30.040(5), entitled "Design Guidelines  
 2242 for the Yarrow Bay Business District," is hereby amended as set forth in Exhibit D to this  
 2243 ordinance, which is incorporated by reference. The Director of Planning and Building is directed  
 2244 to incorporate the amendments into a final version for signature.  
 2245

2246 Section 34. The document referenced at KMC 3.30.040(6), entitled "Design Guidelines  
 2247 for Residential Development," is hereby repealed. All references to the document in the  
 2248 KMC 3.30.040(6) shall be removed.  
 2249

2250 Section 35. To reflect those changes to the City's design guidelines provided in  
 2251 Sections 28 through 32 of this ordinance, Section 3.30.040 of the Kirkland Municipal Code  
 2252 shall be amended for consistency to read as follows:  
 2253

2254 **3.30.040 Design guidelines adopted by reference.**

2255 The design review board and the planning official, pursuant to in combination with the authority  
 2256 set forth in Chapter 142 KZC, shall use the following design guidelines documents to review  
 2257 development permits:  
 2258

2259 (1) The document entitled "Design Guidelines for Pedestrian Oriented Business Districts" as  
 2260 it now exists and as it may be amended in the future, bearing the signatures of the mayor and  
 2261 the director of the department of planning and community development, dated August 3, 2004,  
 2262 and amended by Ordinance 4106, passed July 3, 2007, Ordinance 4636 to add Finn Hill  
 2263 Neighborhood Center design guidepdlines, passed January 26, 2018, Ordinance 4785, passed  
 2264 March 15, 2022, and by Ordinance 4803, passed June 28, 2022, is adopted by reference as  
 2265 though fully set forth herein. The city council shall may consult with the planning commission  
 2266 prior to amending this document.  
 2267

2268 (2) The document entitled "Design Guidelines for NE 85th Street Station Subarea Plan" as it  
2269 now exists and as it may be amended in the future, ~~bearing the signatures of the mayor and~~  
2270 ~~the director of the department of planning and community development, dated June 28, 2022,~~  
2271 is adopted by reference as though fully set forth herein. The city council shall may consult with  
2272 the planning commission prior to amending this document.  
2273

2274 (3) The document entitled "Design Guidelines for the Totem Lake Business District" as it now  
2275 exists and as it may be amended in the future, ~~bearing the signatures of the mayor and the~~  
2276 ~~director of the department of planning and community development, dated June 6, 2006, and~~  
2277 ~~amended by Ordinance 4495, passed December 8, 2015, and Ordinance 4678, passed~~  
2278 ~~September 1, 2020,~~ is adopted by reference as though fully set forth herein. The city council  
2279 shall may consult with the planning commission prior to amending this document.  
2280

2281 (4) The document entitled "Kirkland Parkplace Mixed Use Development Master Plan and  
2282 Design Guidelines" as it now exists and as it may be amended in the future, ~~bearing the~~  
2283 ~~signatures of the mayor and the director of the department of planning and community~~  
2284 ~~development, dated February 17, 2015, and amended by Ordinance 4605, passed September~~  
2285 ~~19, 2017, Ordinance 4677, passed January 15, 2019, and Ordinance 4711, passed December~~  
2286 ~~10, 2019,~~ is adopted by reference as though fully set forth herein. The city council shall may  
2287 consult with the planning commission prior to amending this document.  
2288

2289 (5) The document entitled "Design Guidelines for the Yarrow Bay Business District" as it now  
2290 exists and as it may be amended in the future, ~~bearing the signatures of the mayor and the~~  
2291 ~~director of the department of planning and community development, dated November 15,~~  
2292 ~~2014,~~ is adopted by reference as though fully set forth herein. The city council shall may consult  
2293 with the planning commission prior to amending this document.  
2294

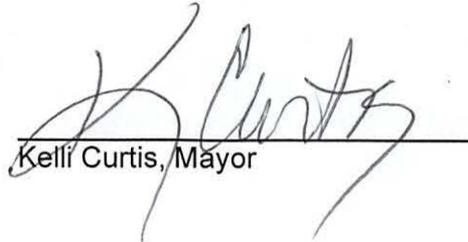
2295 ~~(6) The document entitled "Design Guidelines for Residential Development" bearing the~~  
2296 ~~signatures of the mayor and the director of the department of planning and community~~  
2297 ~~development, dated December 8, 2015, is adopted by reference as though fully set forth~~  
2298 ~~herein. The city council shall consult with the planning commission prior to amending this~~  
2299 ~~document.~~  
2300

2301 Section 36. If any provision of this ordinance or its application to any person or  
2302 circumstance is held invalid, the remainder of the ordinance or the application of the provision  
2303 to other persons or circumstances is not affected.  
2304

2305 Section 37. This ordinance shall be in force and effect five days from and after its  
2306 passage by the Kirkland City Council and publication in the summary form attached to the  
2307 original of this ordinance.

Passed by majority vote of the Kirkland City Council in open meeting this 20th day of January, 2026.

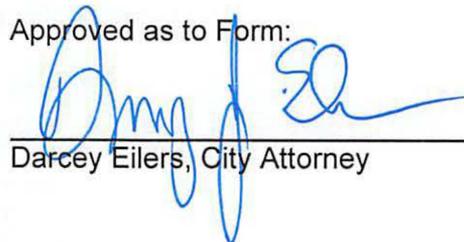
Signed in authentication there of this 20th day of January, 2026.

  
Kelli Curtis, Mayor

Attest:

  
Anastasiya Warhol, City Clerk

Approved as to Form:

  
Darcey Eilers, City Attorney

PUBLICATION SUMMARY  
OF ORDINANCE NO. O-4922

AN ORDINANCE OF THE CITY OF KIRKLAND RELATING TO PLANNING, ZONING, AND LAND USE RELATED TO DESIGN REVIEW AND AMENDING THE CITY OF KIRKLAND ZONING CODE, INCLUDING REPEALING CHAPTER 92 IN ITS ENTIRETY AND REPLACING IT WITH A NEW CHAPTER 92; REPEALING CHAPTER 142 IN ITS ENTIRETY AND REPLACING IT WITH A NEW CHAPTER 142; AMENDING VARIOUS OTHER ZONING CODE PROVISIONS FOR CONSISTENCY WITH THESE NEW ZONING CODE CHAPTERS; ADOPTING REVISED DESIGN GUIDELINES FOR PEDESTRIAN ORIENTED BUSINESS DISTRICTS, NE 85TH STREET STATION SUBAREA PLAN, TOTEM LAKE BUSINESS DISTRICT, AND YARROW BAY BUSINESS DISTRICT; REPEALING DESIGN GUIDELINES FOR RESIDENTIAL DEVELOPMENTS; AND AMENDING SECTION 3.30.040 OF THE KIRKLAND MUNICIPAL CODE RELATED TO THESE DESIGN GUIDELINES.

1        SECTION 1. Amends Section 5.10.538 of the Kirkland Zoning Code (KZC) amending  
2 the definition of Modulation.

3  
4        SECTIONS 2 – 3. Amend Chapter 30 KZC relating to office zones.

5  
6        SECTIONS 4 -7. Amend Chapter 35 KZC relating to commercial zones.

7  
8        SECTIONS 8 – 11. Amend Chapter 50 KZC relating to central business district  
9 zones.

10  
11        SECTIONS 12 – 13. Amend Chapter 51 KZC relating to Market Street corridor zones.

12  
13        SECTION 14. Amends Chapter 53 KZC relating to Rose Hill business district zones.

14  
15        SECTIONS 15 – 16. Amend Chapter 54 KZC relating to NRH business district zones.

16  
17        SECTIONS 17 – 20. Amend Chapter 55 KZC relating to Totem Lake zones.

18  
19        SECTION 21. Amends Chapter 56 KZC relating to the Yarrow Bay business district  
20 zones.

21  
22        SECTION 22. Amends KZC 57.05.050 relating to the review process for the NE 85th  
23 Street Station Area zone.

24  
25        SECTION 23. Amends KZC 105.103 relating to the review process for parking  
26 modifications for parking.

27  
28        SECTION 24. Amends Chapter 110 KZC relating to required public improvements.

29  
30        SECTIONS 25 – 26. Amend Chapter 115 KZC relating to miscellaneous use,  
31 development, and performance standards.

32  
33        SECTION 27. Amends KZC 141.70 relating to the review process for shoreline  
34 substantial development permits.  
35

1        SECTION 28. Repeals in its entirety the existing Chapter 92 of the KZC entitled  
2 "Design Regulations" and replaces it with a new Chapter 92 KZC.  
3

4        SECTION 29. Repeals in its entirety the existing Chapter 142 of the KZC "entitled  
5 "Design Review" and replaces it with a new Chapter 142 KZC.  
6

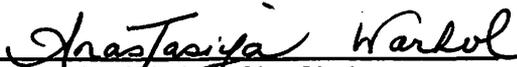
7        SECTIONS 30 – 35. Adopt amendments to the design guidelines for Pedestrian  
8 Oriented Business Districts, for NE 85th Street Station Subarea Plan, for Totem Lake Business  
9 District, and for Yarrow Bay Business District; repeal design guidelines for residential  
10 developments; and amend Section 3.30.040 of the Kirkland Municipal Code relating to reflect  
11 changes to the design guidelines adopted by reference therein.  
12

13        SECTION 36. Provides a severability clause for the ordinance.  
14

15        SECTION 37. Authorizes publication of the ordinance by summary and establishes the  
16 effective date as five days after publication of summary.  
17

18        The full text of this Ordinance will be mailed without charge to any person upon request  
19 made to the City Clerk for the City of Kirkland. The Ordinance was passed by the Kirkland City  
20 Council at its meeting on the 20th day of January, 2026.  
21

22        I certify that the foregoing is a summary of Ordinance No. 4922 approved by the  
23 Kirkland City Council for summary publication.

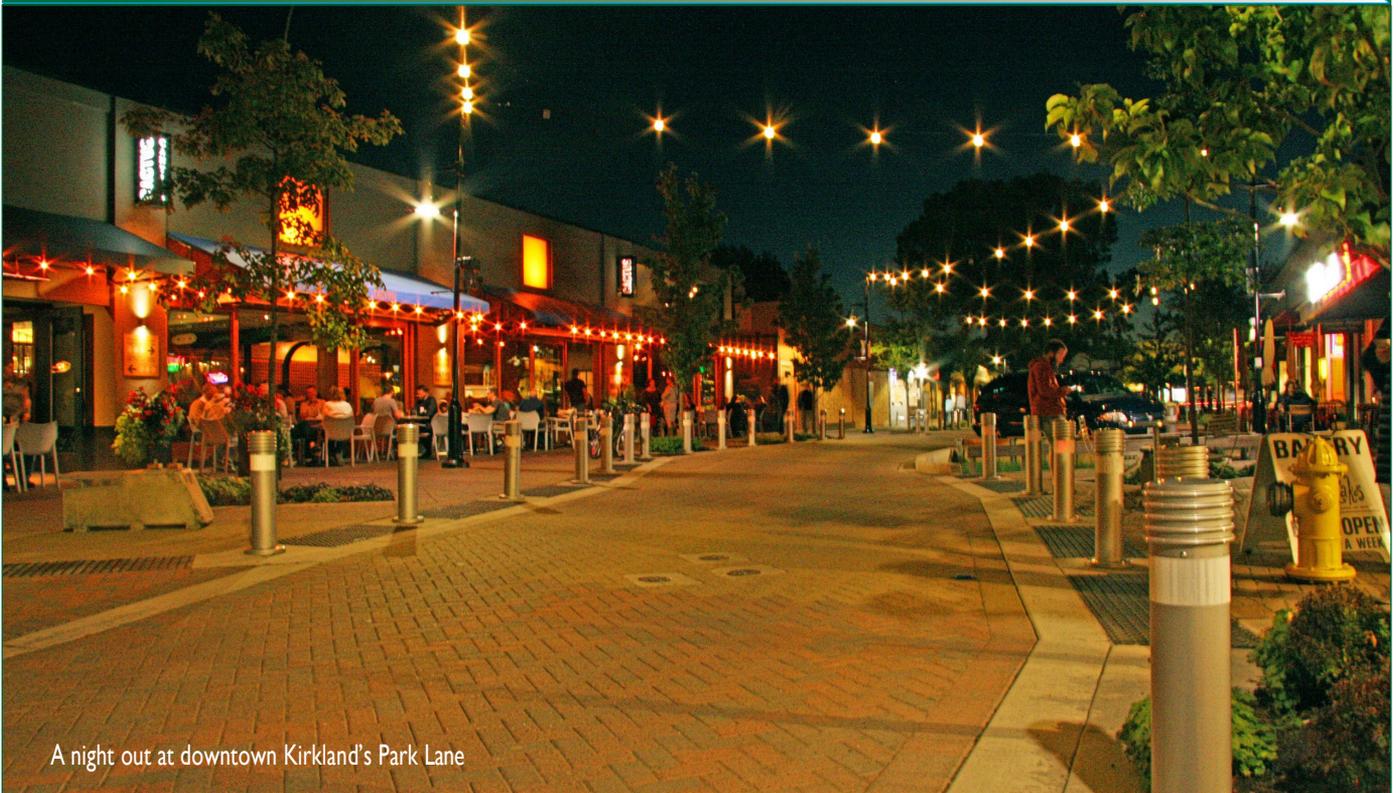
  
Anastasiya Warhol, City Clerk

Publication Date:  
01/26/2026

The City of Kirkland

# Design Guidelines

## For Pedestrian-Oriented Business Districts



A night out at downtown Kirkland's Park Lane



Adopted by the City Council pursuant to  
Kirkland Municipal Code Section 3.30.040.

Dated August 3, 2004

Updated: March 15 2022, O-4785

Attest:

Penny Sweet,  
Mayor

Adam Weinstein  
Director,  
Planning & Building  
Department

# Table of Contents

<b>Introduction</b>	<b>1</b>
<b>Pedestrian-Oriented Elements</b>	<b>8</b>
Introduction	
Sidewalk Width + Movement Zone	
Sidewalk Width - Curb Zone	
Sidewalk Width - The Storefront Activity Zone	
Pedestrian Coverings	
“Pedestrian-Friendly” Building Fronts	
Special Consideration for Neighborhood Business Districts	
Upper-Story Activities Overlooking Street	
Lighting from Buildings	
Pedestrian-Oriented Plazas	
Pedestrian Connections	
Blank Walls	
<b>Public Improvements and Site Features</b>	<b>15</b>
Introduction	
Pathway Width	
Pedestrian Paths and Amenities	
Street Trees	
Public Improvements and Site Features	
Entry Gateway Features	
Public Art	
<b>Parking Lot Location and Design</b>	<b>21</b>
Introduction	
Parking Location and Entrances	
Circulation Within Parking Lots	
Parking Lot Landscaping	
Parking Garages	
<b>Scale</b>	<b>25</b>
Introduction	
Fenestration Patterns	
Architectural Elements: Decks, Bay Windows, Arcades, Porches	
Building Modulation: Vertical	
Special Consideration for Neighborhood Business Districts	
Building Modulation: Horizontal	
Building Massing in Central Business District 1	
Special Considerations for Neighborhood Business Districts	

# Table of Contents continued

## building Material, Color, and detail

---

31

Introduction  
Ornament and Applied Art  
Color  
Street Corners  
Signs

## natural Features

---

34

Introduction  
Visual Quality of Landscapes  
Protection and Enhancement of Wooded Slopes  
Height Measurement on Hillsides  
Views of Water  
Culverted Creeks

The Illustrations throughout this document are provided by *MAKERS*.



# Introduction

## Purpose of Design Guidelines

This document sets forth a series of Design Guidelines, adopted by Section 3.30 of the Kirkland Municipal Code, that will be used by the City in the design modification review process and the alternative compliance process. For Board Design Review (BDR), the Design Review Board will use these guidelines in association with the Design Regulations of the Kirkland Zoning Code. To the extent that the standards of the Design Guidelines or Design Regulations address the same issue but are not entirely consistent or contain different levels of specificity, the Design Review Board will determine which standard results in superior design.

For Administrative Design Review (ADR), the Planning Official will use these guidelines when necessary to interpret the Design Regulations. They are also intended to assist project developers and their architects by providing graphic examples of the intent of the City's guidelines and regulations.

Most of the concepts presented in the Design Guidelines are applicable to any pedestrian-oriented business district.\* “Special Considerations” have been added, such as for Downtown Kirkland, to illustrate how unique characteristics of that pedestrian-oriented business district relate to the Guideline.

The Design Guidelines do not set a particular style of architecture or design theme. Rather, they will establish a greater sense of quality, unity, and conformance with Kirkland's physical assets and civic role.

The Design Guidelines will work with improvements to streets and parks and the development of new public facilities to create a dynamic setting for civic activities and private development. It is important to note that these Guidelines are not intended to slow or restrict development, but rather to add consistency and predictability to the permit review process.

### Use of the Guidelines for Design Modifications and Alternative Compliance

KZC Chapter 142 permits design modifications to specific zoning regulations. The Planning Official will use these guidelines to administer the design modification authority of KZC 142.

The Alternative Compliance process (outlined in KZC Chapter 142) is an optional path that allows applicants to propose substantial design modifications in place of the design regulations outlined in Chapter 92, or other zone-specific sections of the KZC. The Alternative Compliance Proposals will be reviewed by the Design Review Board (DRB) and will be reviewed for consistency with the objectives of the design guidelines.

# Kirkland Design Guidelines

The drawing below illustrates many of the design Guidelines described in this appendix

- 1 Pedestrian plazas and places for vendors encouraged through several regulations.
- 2 Buildings on corner lots may be required to incorporate an architectural or pedestrian-oriented feature at the corner. Many options are possible including plazas, artwork, turrets, curved corners, step backs, setbacks, etc.

### Special architectural requirements placed on use of concrete block and metal siding.

- 3 “Architectural scale” requirements direct large buildings to fit more comfortably with neighboring development. This example employs building setbacks, decks, curved surfaces, and recessed entries to reduce appearance of building mass.

- 4 Parking garages on pedestrian-oriented streets or through-block sidewalks may incorporate pedestrian-oriented uses or pedestrian-oriented space into front facades.

### Street trees required along certain streets.

- 5 Human scale features such as balconies or decks, bay windows, covered entries, gable or hipped rooflines, multiple paned windows, or pedestrian-oriented space may be required.
- 6 More flexible method of measuring building height on slopes.
- 7 New policies regarding tree protection and enhancement of wooded slopes. Standards for size, quantity, quality, and maintenance of landscape plant materials are set by the Zoning Code.

- 8 Standards for size, quantity, quality, and maintenance of landscape plant materials are set by the Zoning Code.
- 9 Standards are set for pathway width, pavement, lighting, and site features on required major pathways and public properties.

- 10 A building cornerstone or plaque may be required.
- 11 Covering up existing masonry or details with synthetic materials is restricted.

- 12 Ground story facades of buildings on pedestrian-oriented streets or adjacent to parks may be required to feature display windows, artwork, or pedestrian-oriented space.

- 13 Pedestrian weather protection required on pedestrian-oriented streets.

- 14 Architectural detail elements such as decorative or special windows, doors, railings, grillwork, lighting, trellises, pavements, materials, or artwork to add visual interest may be required.

### Size of parking lots abutting pedestrian-oriented streets may be restricted.

- 15 Quantity and locations of driveways are regulated.
- 16 Visible service areas and loading docks must be screened.

- 17 Provision for pedestrian circulation is required in large parking lots.

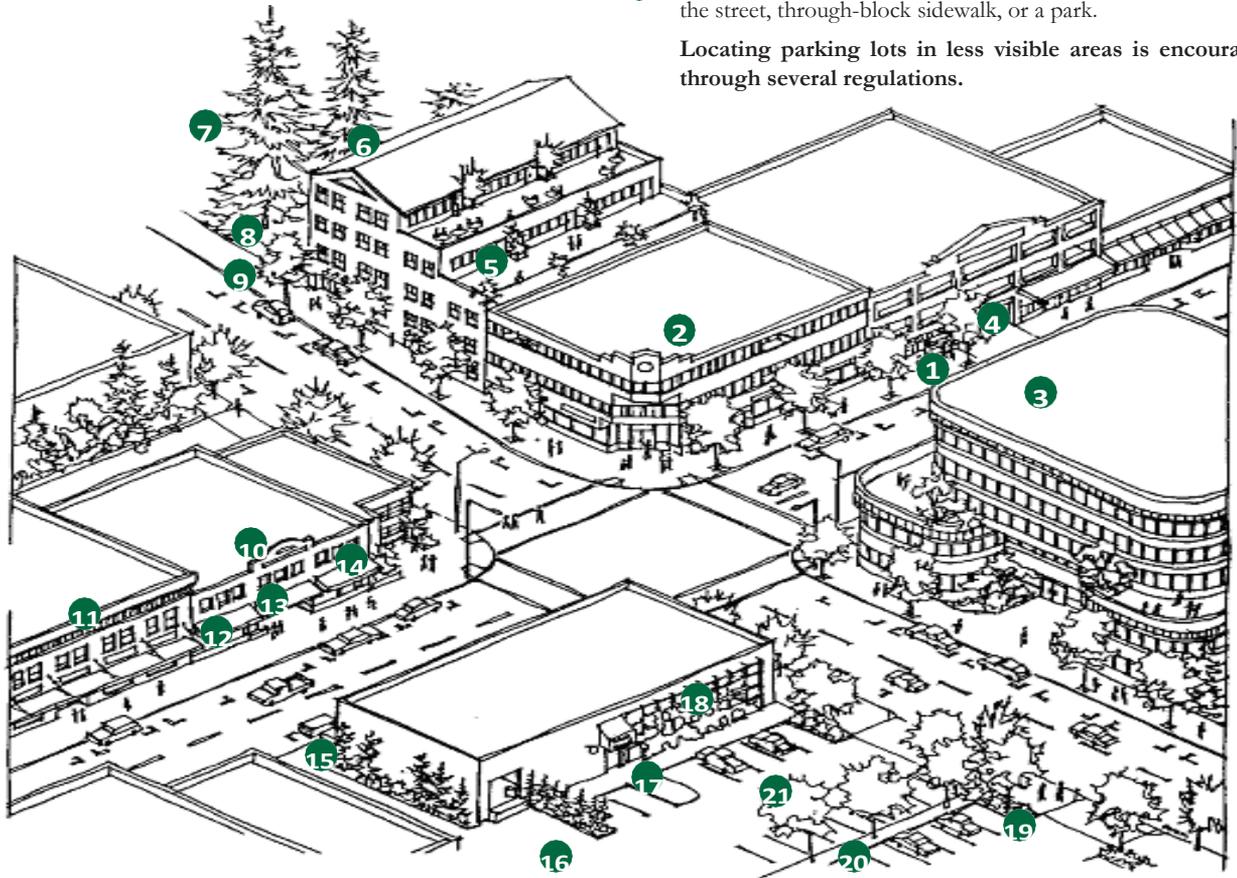
- 18 Blank walls near streets or adjacent to through-block sidewalks must be treated with landscaping, artwork, or other treatment.

- 19 Screening of parking lots near streets is required.

- 20 Standards for curbs, signing, lighting, and equipment are set for parking lots.

- 21 Internal landscaping is required on large parking lots visible from the street, through-block sidewalk, or a park.

### Locating parking lots in less visible areas is encouraged through several regulations.



## Purpose of the Design Guidelines for Downtown Kirkland

In 1989 the Kirkland City Council adopted Kirkland's Downtown Plan which set a vision for the downtown's future and outlined policies and public actions to make that vision a reality. One of the recommended actions is the adoption of a set of Downtown Design Guidelines to be used in reviewing all new development and major renovations in the downtown area. The goal of the Design Guidelines as stated in the plan is to

*... balance the desired diversity of project architecture with the equally desired overall coherence of the downtown's visual and historic character. This is to be achieved by injecting into each projects' creative design process a recognition and respect of design guidelines and methods which incorporate new development into downtown's overall pattern.*

In addition, the guidelines are intended to further the following urban design goals stated in the plan:

- ◆ Promote a sense of community identity by emphasizing Kirkland's natural assets, maintaining its human scale, and encouraging activities that make downtown the cultural, civic, and commercial heart of the community.
- ◆ Maintain a high-quality environment by ensuring that new construction and site development meet high standards.
- ◆ Orient to the pedestrian by providing weather protection, amenities, human scale elements, and activities that attract people to downtown.
- ◆ Increase a sense of continuity and order by coordinating site orientation, building scale, and streetscape elements of new development to better fit with neighboring buildings.
- ◆ Incorporate parks and natural features by establishing an integrated network of trails, parks, and open spaces and maintaining existing trees and incorporating landscaping into new development.
- ◆ Allow for diversity and growth through flexible guidelines that are adaptable to a variety of conditions and do not restrict new development.

## Purpose of the Design Guidelines for PLA5C

Planned Area 5C is part of the Moss Bay Neighborhood and is designated for high density residential and office uses. It is located just east of the Central Business District (CBD) and shares many of the CBD's

characteristics, although retail uses are not allowed.

The adjacent steep hillside to the north of PLA5C is part of the 85<sup>th</sup> Street right of way and it limits potential view obstruction from the five to six story buildings which can be developed in PLA5C.

The following guidelines, which encourage wide sidewalks, do not apply to PLA5C since there are no "pedestrian oriented streets" or "major pedestrian sidewalks" designated in the Zoning Code for this area.

- ◆ Sidewalk Width: Movement Zone
- ◆ Sidewalk Width: Storefront Activity Zone

An additional guideline that does not apply is "Height Measurement on Hillsides."

## Purpose of the Design Guidelines for Juanita Business District

The Juanita Business District Plan was adopted in 1990 by the City Council. It states that "the underlying goal of redevelopment in the business district is to create a neighborhood-scale, pedestrian district which takes advantage of the amenities offered by Juanita Bay."

As part of the Juanita Business District Plan, Design Regulations and Design Guidelines were established for new development and major renovations in the Business District (JBD). These guidelines and regulations are intended to further the following urban design features stated in the plan:

- ◆ Pedestrian pathways from the surrounding residential areas to and through the business district and on to Juanita Beach Park should be acquired and improved.
- ◆ New development will provide a pedestrian connection between Juanita Bay Park and Juanita Beach Park.
- ◆ View corridors to the lake should be explored through new development in the business district.
- ◆ Entry features, such as signs or sculpture, should be established in the locations shown in the Juanita Business District Plan.
- ◆ Coordinated streetscape improvements should be used throughout the business district, including street trees, street furniture, and other amenities, like flowers, banners, and signs.

## Purpose of the Design Guidelines for the Market Street Corridor, including the Market Street Historic District



The City Council adopted the Market Street Corridor Plan in December of 2006 as part of the Market and Norkirk Neighborhood planning process. The new plan



was created for commercial and multifamily properties adjoining Market Street extending from the Central Business District at the south end to 19th Avenue at the north end. The plan includes a vision for the corridor of an attractive, economically healthy area that accommodates neighborhood oriented businesses, office uses and multifamily housing in a way that complements and protects the adjacent residential neighborhoods.

The historic 1890's buildings at the intersection of Market Street and 7th Avenue create a unique sense of place that represents the original town center of Kirkland. The plan establishes an historic district in this area that will reflect the City's past through both its old and new buildings and its streetscape. New development and renovation within this historic district should reflect the scale and design features of the existing historic resources in the district.

As part of the Market Street Corridor Plan, Design Regulations and Guidelines are established for new development and major renovations in the Market Street Corridor (MSC). These guidelines and regulations are intended to further the following design objectives that are stated in the plan:

- ◆ Encourage preservation of structures and locations that reflect Kirkland's heritage.
- ◆ Support a mix of higher intensity uses along the Market Street Corridor while minimizing impacts on adjacent residential neighborhoods.
- ◆ Maintain and enhance the character of the historic intersection at 7th Avenue and Market Street.
- ◆ Provide streetscape, gateway and public art improvements that contribute to a sense of identity and enhanced visual quality.
- ◆ Provide transitions between low density residential uses within the neighborhoods and the commercial and multifamily residential uses along Market Street.

Except for the MSC2 zone, the following guidelines, which suggest wider sidewalks, do not apply since there are no "pedestrian oriented streets" or "major pedestrian sidewalks" designated in the Zoning Code for the Market Street Corridor.

- ◆ Sidewalk Width: Movement Zone
- ◆ Sidewalk Width: Storefront Activity Zone

Additional guidelines that do not apply to the Market Street Corridor include:

- ◆ Protection and Enhancement of Wooded Slopes

- ◆ Height Measurement on Hillsides
- ◆ Culverted Creeks

## Purpose of the Design Guidelines for North Rose Hill Business District

The North Rose Hill Business District goals and policies were adopted in 2003 as part of the North Rose Hill Neighborhood Plan. Development in the North Rose Hill Business District (NRHBD) is to complement the Totem Lake Business District and encourage increased residential capacity to help meet housing needs. Commercial uses are to be limited to those that are compatible with the residential focus of the NRHBD.

As part of the NRH plan, design regulations and guidelines were established for new development and major renovations in the Business District (NRHBD). These guidelines and regulations are intended to further the following urban design goals and policies stated in the plan:

- ◆ Ensure that public improvements and private development contribute to neighborhood quality and identity in the Business District through:
  - *Establishment of building and site design standards.*
  - *Utilization of the design review process.*
  - *Location and sharing of parking lots .*
  - *Utilization of high quality materials, public art, bicycle and pedestrian amenities, directional signs on all arterials, and other measures for public buildings and public infrastructure, such as streets and parks.*
- ◆ Provide transitions between commercial and residential uses in the neighborhood.
- ◆ Provide streetscape improvements that contribute to a sense of neighborhood identity and enhanced visual quality.

Since the focus of the NRHBD is on increasing residential capacity while accommodating supportive commercial uses, rather than developing into a destination retail business district, the following guidelines do not apply to this business district.

- ◆ Sidewalk Width – Movement Zone
- ◆ Sidewalk Width – Curb Zone
- ◆ Sidewalk Width – The Storefront Activity Zone
- ◆ Pedestrian Coverings
- ◆ Pedestrian-Friendly Building Fronts
- ◆ Upper-Story Activities Overlooking the Street

In addition, the following do not apply:

- ◆ Protection and Enhancement of Wooded Slopes
- ◆ Height Measurement on Hillsides

- ◆ Views of Water
- ◆ Culverted Creeks

## Purpose of the Design Guidelines for the Totem Lake Business District Core

The Kirkland City Council adopted a new neighborhood plan for Totem Lake in early 2002. The vision set forth in the Plan for the Totem Lake Business District Core is of a dense, compact community, with a mix of business, commercial and residential uses and a high level of transit and pedestrian activity.

The Plan establishes key overall design principles for the Business District Core, as well as specific design objectives for the Totem Lake Mall (TL 2), Evergreen Hospital campus (TL 3), and the mixed-use area west of the campus (TL 1). Design objectives promoted in the plan for the Business District Core include:

- ◆ Accommodate high density, transit-oriented development, consistent with the district's position in an Urban Center.
- ◆ Ensure that public and private development contribute to a lively and inviting character in the Business District Core.
- ◆ Reinforce the character of the Business District Core through public investments
- ◆ Produce buildings that exhibit high quality design, incorporate pedestrian features and amenities and display elements of both continuity and individuality
- ◆ Provide public spaces that are focal points for the community
- ◆ Provide visual and functional connections between adjacent developments through landscaping, public spaces and pedestrian connections.

Design considerations specific to the three subareas within the district include:

### Mixed-Use Area (TL 1)

- ◆ Break up the mass of larger buildings through techniques such as towers over podiums, to create a varied building footprint and the perception of a smaller overall building mass.
- ◆ Incorporate features that create distinctive roof forms, to contribute to a skyline that is visually interesting throughout the district.

- ◆ Ensure appropriate transitions from lower density uses north of the Business District Core through providing residentially scaled façades and centered building masses in development along NE 132nd Street.

### Retail Center (TL 2)

The Totem Lake Business District Plan direction for the TL2 area is to support its growth as a vibrant, intensive retail center for the Kirkland community and surrounding region. These guidelines are intended to promote the vision of this area as a "village-like" community gathering place, with high-quality urban and architectural design in redevelopment. To provide for flexibility and increased development potential, while ensuring coordinated development and design integrity over time, redevelopment should occur within the context of an overall site development or Master Plan for the entire property.

### Evergreen Hospital Medical Center Campus (TL 3)

The Totem Lake Business District Plan acknowledges the important role the hospital plays in the Kirkland community, and supports growth on the campus to strengthen this role. Design objectives stated in the Plan for the Evergreen Hospital campus are consistent with those expressed in the Master Plan approved for the site:

- ◆ Taller buildings should be located toward the center of the site and designed to minimize shadowing and transition impacts on residential areas.
- ◆ Public access to usable green spaces on the campus can help to offset the impacts of taller buildings on the site.
- ◆ Ensure campus edges are compatible with neighboring uses.
- ◆ Enhance and improve pedestrian access with the campus and to surrounding uses, particularly the transit center and to TL 2.

The approved Master Plan for the hospital campus includes additional, unique design guidelines that apply to institutional development in a campus environment:

- ◆ *Respond to Physical Environment:* New buildings should be attractive as well as functional additions to the campus.
- ◆ *Enhance the Skyline:* The upper portion of buildings should be designed to promote visual interest and variety on the skyline, except where building function dictates uninterrupted vertical mass.

- ◆ Avoid blank facades in buildings located on the perimeter of the campus.
- ◆ Use materials and forms that reinforce the visual coherence of the campus.
- ◆ Provide inviting and useable open space.
- ◆ Enhance the campus with landscaping.
- ◆ Guidelines for the transit center to be located on the hospital campus should be developed and incorporated with guidelines for the rest of the campus.

The following guidelines do not apply to the Totem Lake Business District Core:

- ◆ Height Measurement on Hillsides
- ◆ Views of Water

## Purpose of the Design Guidelines for Finn Hill Neighborhood Center (FHNC)

The Finn Hill Neighborhood Plan was adopted in early 2018 by the City Council. The Neighborhood Plan sets the vision for the Finn Hill Neighborhood Center north of NE 141st ST along Juanita Drive as a mixed use, neighborhood scale commercial and residential village to strengthen the neighborhood identity.

The design guidelines are intended to further the following design objectives described in the Plan for the FHNC and summarized below:

- ◆ Building and site design is attractive, pedestrian oriented and compatible in scale and character with the surrounding neighborhood.
- ◆ Pedestrian paths connect between uses on a site and adjacent properties.
- ◆ Parking lots or parking garages are oriented to the back or side of buildings or treated with landscaping or design features.
- ◆ Streetscape improvements are attractive to identify Finn Hill as unique to other commercial districts and multi-modal in design.
- ◆ Public gathering spaces contain seating and landscaping.
- ◆ Bicycle and pedestrian amenities are provided including directional signage.
- ◆ Green building and sustainable site techniques are utilized.
- ◆ Art, signs and landscaping are used to add character to the commercial area.

The following guidelines do not apply to this district:

- ◆ Protection and enhancement of wooded slopes
- ◆ Height measurement on Hillsides
- ◆ Culverted Creeks
- ◆ Open Space at Street Level

## Purpose of the Design Guidelines for the Houghton/Everest Neighborhood Center (HENC)

The plan for the HENC was adopted in 2017. The primary goal of the plan is to promote a strong and vibrant pedestrian oriented neighborhood center with a mix of commercial and residential land uses that primarily serve the adjacent neighborhoods.

In addition, the HENC contains an important interface with the Cross Kirkland Corridor (CKC). Successfully integrating site and building design, as well as public access, with this important transportation and open space amenity will mutually benefit the neighborhood center and the CKC. Thoughtful design of the interface will attract nonmotorized customers and residents to the neighborhood center and create an attractive and safe space for pedestrians and bicyclists using the CKC.

The Guidelines are intended to further the following design objectives that are stated in the Comprehensive Plan.

- ◆ Coordinate development on both sides of the NE 68th Street Corridor in the Everest and Central Houghton neighborhoods.
- ◆ Promote a pedestrian-oriented development concept through standards for a coordinated master plan for the center.
- ◆ Reduce ingress and egress conflicts within and around the center through creation of a circulation system for all users including vehicles, bicycles and pedestrians.
- ◆ Design buildings with careful attention given to modulation, upper story step backs, and use of materials to reduce the appearance of bulk and mass.
- ◆ Coordinate street improvements.
- ◆ Provide transitions between commercial and low density residential areas.
- ◆ Discourage southbound through traffic on 106th Avenue NE.



- ◆ Enhance the gateway at the corner of NE 68th Street and 108th Avenue NE.
- ◆ Provide gathering spaces and relaxation areas within the center.

The following guidelines do not apply to the Neighborhood Center:

- ◆ Protection and Enhancement of Wooded Slopes
- ◆ Height Measurement on Hillsides
- ◆ Culverted Creeks

## Purpose of the Design Guidelines for Neighborhood Business Districts

The Comprehensive Plan establishes a hierarchy of commercial districts, with regional goods and services at the upper end and neighborhoods goods and services at the lower end.

Kirkland's Neighborhood Business Districts (BN, BNA, and MSC2) are important in providing neighborhood goods and services. Given the more localized draw for residents to meet their everyday needs, an emphasis on convenient and attractive pedestrian connections and vehicular access is important.

In addition, because these districts are surrounded by the residential land uses they serve, the design character and context of new development is critical to ensure that it integrates into the neighborhood.

The design guidelines are intended to further the following design objectives that are stated in the Plan:

- ◆ Establish development standards that promote attractive commercial areas and reflect the distinctive role of each area.
- ◆ Encourage and develop places and events throughout the community where people can gather and interact.
- ◆ Moss Bay neighborhood: Ensure that building design is compatible with the neighborhood in size, scale, and character.
- ◆ South Rose Hill neighborhood: Residential scale and design are critical to integrate these uses into the residential area.

The following guidelines do not apply to these districts:

- ◆ Protection and Enhancement of Wooded Slopes
- ◆ Height Measurement on Hillsides
- ◆ Culverted Creeks

## Purpose of the Design Guidelines for the North Juanita Neighborhood Center

The Juanita Neighborhood Plan was updated and adopted in 2024 by the City Council. The Neighborhood Plan supports the redevelopment of three aggregated parcels at the southern border of the North Juanita Neighborhood Center to serve as catalyst for redevelopment in the area and encourage increased residential capacity to help meet housing needs. Commercial uses are primarily neighborhood-serving and complement an active, mixed use destination for shopping, living and community gathering.

Design guidelines serve to accomplish the following design objectives detailed in the Neighborhood Plan:

- ◆ Mixed-use buildings are oriented towards the adjoining rights-of-way.
- ◆ Pedestrian-oriented design elements are incorporated into the development such as plazas and publicly accessible open space to nurture community gathering opportunities. These spaces include amenities such as public art, interactive features, and native landscaping.
- ◆ Development supports access to nearby transit stops and greenways through integrated pathways, bike parking, and pedestrian and rolling facilities.
- ◆ A gateway feature should be provided at the corner of NE 132nd St. and 100th Ave NE.

## Purpose of the Design Guidelines for the Bridle Trails Neighborhood Center (BCX Zone)

The Bridle Trails Neighborhood Plan was adopted in late 2018 by the City Council. The Neighborhood Plan encourages redevelopment of the Bridle Trails Neighborhood Center into a lively pedestrian-oriented, transit-supportive, mixed-use residential and commercial neighborhood center.

The design guidelines are intended to further the following design objectives described in the Plan for neighborhood center and summarized below:

- ◆ Careful attention to architectural scale, massing and upper story step backs, pedestrian orientation and connections, compatibility with surrounding residential uses and commercial uses across NE 70th Street, building modulation, and use of materials to reduce the appearance of bulk and mass.
- ◆ Buildings are oriented to adjoining rights-of-way

and internal pedestrian pathways.

- ◆ Green building standards and sustainable site standards are included in development.
- ◆ Taller building forms are located away from adjoining residential properties.
- ◆ Pedestrian-oriented design elements are incorporated into the development such as plazas to create public gathering spaces with public art, water features, and landscaping.
- ◆ Driveways are consolidated to minimize impacts on surrounding streets, and adjacent residential uses, and to foster a pedestrian-oriented site design.
- ◆ Parking and transportation impacts are minimized to create a pedestrian-oriented neighborhood center.
- ◆ A gateway feature should be provided at the corner of NE 70th Place and 132nd Avenue NE.

## Purpose of the Design Guidelines for the Rose Hill Business District (RH) 8 Zone

The Rose Hill Neighborhood Plan envisions this area east of 128th Avenue NE as an area with less intensive office, neighborhood retail, and neighborhood services uses. The area features a mix of smaller scale uses oriented towards both the regional and local population. The style of development should be more residential in character including conversion of single family homes into commercial businesses. Nearly all buildings should feature pitched roofs and porches or smaller covered entries. Over time, many smaller sites should be consolidated to maximize development opportunity and share vehicular access and parking. The design guidelines provide a number of street frontage options for businesses. In the future, the resulting development will be a mix of storefronts directly on the street, storefronts with small landscaped setbacks, businesses maintaining parking in front, and multi-story buildings with parking underneath.



# Pedestrian-Oriented Elements

## Introduction

Successful pedestrian-oriented business districts, as opposed to “commercial strips,” depend upon making pedestrian circulation more convenient and attractive than vehicular circulation, because the retail strategy for such districts is to encourage the customer to visit often and for more than one purpose at a time. The desired shopping pattern is for the customer to park in a convenient location and walk to several different businesses or attractions. The guidelines in this section focus on creating a high-quality pedestrian environment, especially along *pedestrian-oriented streets*. *Pedestrian-oriented streets* are specific streets defined for each business district.

This section also deals with building elements that detract from pedestrian qualities. One such detraction is a large expanse of blank wall, which, when adjacent or near to neighboring properties or overlooking public areas, can be intrusive and create undesirable conditions for pedestrians and neighbors. Therefore, the guidelines direct new development to treat blank walls with landscaping, building modulation, or other elements to reduce the impact of blank walls on neighboring and public properties.

The guidelines dealing with the spatial and functional integration of sidewalk areas and building elements address several issues:

- ◆ Width of sidewalk to accommodate pedestrian flow, building entrances, and other sidewalk activities.
- ◆ Pedestrian weather protection.
- ◆ “Pedestrian-friendly” building fronts.
- ◆ Other building facade elements that improve pedestrian conditions along the sidewalk.
- ◆ Mitigation of blank walls and screening of service areas.



*On the following pages are described urban design guidelines relating to pedestrian circulation and amenities. The guidelines outline the general issues and present design information, concepts, and solutions to address the issues. The guidelines serve as a conceptual foundation and support the regulations included in the Kirkland Zoning Code.*

## Sidewalk Width: Movement Zone

### Intent

Maintain a continuous and safe streetscape with a pedestrian-friendly character.

### Issue Discussion

Pedestrian movement is a primary function of sidewalks. The sidewalk has three overlapping parts with different functions: the curb zone, the movement zone, and the storefront or activity zone.

A well-sized and uncluttered movement zone allows pedestrians to move at a comfortable pace. People can window-shop comfortably and enjoy a relaxed atmosphere without bumping into street signs, garbage cans, or other people.

### Discussion

An adult person measures approximately 2’ across the shoulders, but a pedestrian carrying grocery bags, pushing a baby carriage or bicycle, or walking a dog measures 3’ across. A window-shopper will require a minimum of 2’-6” to 3’ wide space to avoid being pushed or having their view obstructed.

The movement zone should be at least 10’ to 12’ wide so that two couples can comfortably pass one another. This same space also will allow one person to pass a couple while another person passes from the opposite direction. In business districts add 3’ to the storefront activity zone for window-shopping.

The width of the sidewalk movement zone should consider the function of sidewalks, the level of pedestrian

traffic, and the general age groups of the pedestrians (children and the elderly slow traffic on sidewalks that are too narrow).



## Guideline

A sidewalk should support a variety and concentration of activity yet avoid overcrowding and congestion. The average sidewalk width should be between 10' and 18'. New buildings on pedestrian-oriented streets should be set back a sufficient distance to provide at least 10' of sidewalk. If outdoor dining, seating, vending, or displays are desired, an additional setback is necessary.

### Special Consideration for Downtown Kirkland

Most of the business core of Kirkland is already developed with fairly narrow sidewalks. New development should provide sidewalks at the recommended width. Providing wider sidewalks throughout downtown is a long-term endeavor.

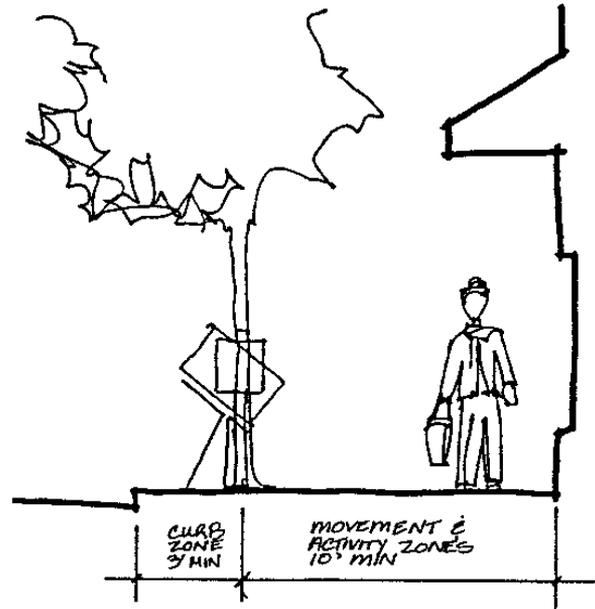
### Special Consideration for Juanita Business District

A concentrated, organized, retail-oriented core with a unified pedestrian circulation network is a goal of the Juanita Business District. The pedestrian system will also serve to connect the perimeter of the district to the core.

### Special Consideration for the Totem Lake Business District Core

New development in TL2 should provide sidewalks at the recommended width, to contribute to the pedestrian-orientation of new development. Public gathering places, such as pedestrian-oriented plazas linked to the sidewalk, should be encouraged.

## Sidewalk Width – Curb Zone



### Issue-Intent

To provide a buffer between vehicular traffic and pedestrians. The curb zone contains parking meters, garbage cans, newspaper stands, street signs, light poles, mail boxes, phone booths, bus stops, and trees. The curb zone is also a buffer between vehicular traffic and pedestrians.

### Discussion

The curb zone may be integrated into the sidewalk design in a number of ways.

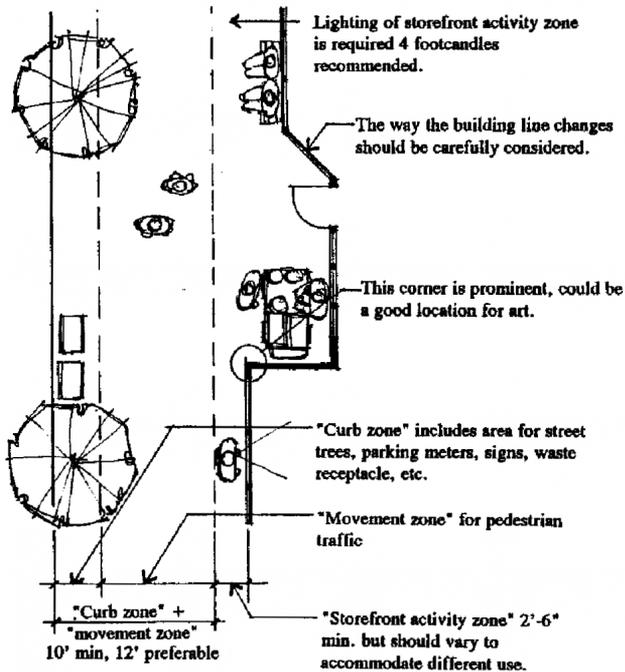
- ◆ *A curb zone with parallel parking.* Getting in and out of parked cars requires 2'-6"; so the curb zone width should be between 4'-6" and 5'-6".
- ◆ *A curb zone without parallel parking.* Space is not needed to park cars; the curb zone width should be between 3' and 4'.
- ◆ *A curb zone with street furniture clustered in sidewalk bulbs along the street; parking is allotted in the pockets between the bulbs.* Clusters of street elements – benches, newspaper stands, covered bus stops – require a sidewalk width of about 8' to 12'.

The curb zone may be visually separated from the movement zone by changes in color or surface material. Street furniture and other elements may be grouped and unified by color and shape to give the street a less cluttered appearance.

The design of the curb zone and street elements provides an opportunity for Kirkland to develop a visual identity that differs from street to street yet is still characteristic

of Kirkland.





## Guidelines

Street elements—trees, parking meters, signs—should be organized in the curb zone to reduce congestion. During busy periods, pedestrians may use the curb zone for walking.

Where pedestrian traffic is the heaviest, sidewalk bulbs can be constructed to accommodate bike racks, waste receptacles, and newspaper racks. Corner bulbs also increase pedestrian visibility.

## Sidewalk Width – The Storefront Activity Zone

### Intent

To provide amenities that enrich the pedestrian environment and increase pedestrian activity.

### Issue Discussion

The storefront activity zone is the most important area for improving pedestrian amenities because it offers protection, provides space for sidewalk activities, and is a transition from the public space of the sidewalk to the private space of the building.

### Discussion

At least 10' of the sidewalk must be kept for pedestrian movement. In addition, there must be room for other activities that add life and interest to the street. Window shopping requires a minimum of 2'-6". Other activities require:

- ◆ Bench for sitting: 4' min.

- ◆ Vendor: 4' min. (6' preferable)
- ◆ Outdoor dining: 6' min. (one table)
- ◆ Outdoor displays: 4' min. (6' preferable)

The activity desired in the storefront activity zone can vary from property to property. This may result in a more animated sidewalk environment with protected alcoves and niches.

## Guideline

New buildings should be set back a sufficient distance from the front property line a minimum of 10' to allow enough room for pedestrian movement. Wider setbacks should be considered to accommodate other sidewalk uses that would benefit their businesses and the pedestrian environment. Lighting and special paving of the storefront activity zone are also beneficial.

## Pedestrian Coverings

### Issue Intent

~~Pedestrian coverings such as awnings and canopies offer shelter, provide spatial enclosure, and add design interest to a retail streetscape.~~

- To provide shelter for pedestrians.
- To provide spatial enclosure and add design interest on pedestrian oriented streets with retail and commercial uses.

### Discussion

The design of awnings and canopies should be coordinated with a number of factors:

*The width of a canopy or awning depends on its function. A 3' to 4' canopy will provide rain cover for window-shopping. A 5' or greater canopy will provide cover for a street sale, and a 7' to 8' canopy will provide room for a window shopper and a passing couple.*

*The width of the sidewalk should be considered when sizing the awning. Water spilling down the edges of awnings is unpleasant; thus the awning should be either extended or shortened if there is not room for two people to pass one another either under the awning or outside the awning.*

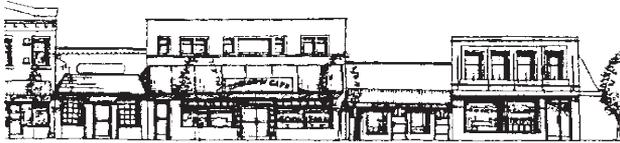
*The architecture of the building determines the appropriate placement and style of the canopy or awning. A canopy should be continuous in shape, design, and placement throughout a building.*

*The overall style of a street should guide the choice of type, color, and size of coverings. The quality of light emanating from awnings or canopies should be controlled. The back-lit*

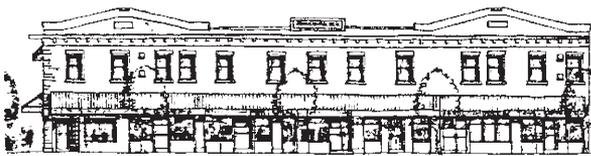
plastic awning typical of fast food chains is inappropriate on pedestrian streetscapes.

*The crown of trees* can be a canopy in its own right by defining space and providing shelter. Canopies and awnings should be appropriately dimensioned to allow for tree growth.

*The street type.* A rich variety of canopies and awnings is particularly desirable on pedestrian-oriented streets and less important on automobile-oriented streets.



**Nonuniform Awnings and Facades  
(Recommended for Pedestrian Oriented Streets)**



## Guideline

Awnings or canopies should be required on facades facing pedestrian-oriented sidewalks. A variety of styles and colors should be encouraged on pedestrian-oriented streets, and a more continuous, uniform style encouraged for large developments on entry arterial streets.

## “Pedestrian-Friendly” Building Fronts

### Intent

- To enhance the pedestrian environment.
- To create safe and active sidewalks and pathways.

### Issue

~~Building setbacks were originally developed to promote “pedestrian-friendly” building fronts by providing light, air, and safety. But dull building facades and building setbacks that are either too wide or too narrow can destroy a pedestrian streetscape. A successful pedestrian business district must provide interesting, pedestrian-friendly building facades and sidewalk activities.~~

### Discussion

Building fronts should have pedestrian-friendly features transparent or decorative windows, public entrances, murals, bulletin boards, display windows, seating, or street vendors that cover at least 75 percent of the ground-level storefront surface between 2’ and 6’ above the sidewalk.

Sitting areas for restaurant and merchandise displays should allow at least a 10’ wide pavement strip for walking. Planters can define the sitting area and regulate pedestrian flow.

Blank walls severely detract from a pedestrian streetscape. To mitigate the negative effects of blank walls:

- ◆ Recess the wall with niches that invite people to stop, sit, and lean.
- ◆ Allow street vendors.
- ◆ Install trellises with climbing vines or plant materials.
- ◆ Provide a planting bed with plant material that screens at least 50 percent of the surface.
- ◆ Provide artwork on the surface.

## Guideline

All building fronts should have pedestrian-friendly features as listed above.

## Special Consideration for Downtown Kirkland - Glazing

Building frontages along pedestrian-oriented streets in the Central Business District should be configured to have a 15’ story height to ensure suitability for diverse retail tenants and enhance the pedestrian experience. Where these taller retail stories are required, special attention to storefront detailing is necessary to provide a visual connection between pedestrian and retail activity.

## Guideline

Storefronts along pedestrian-oriented streets should be highly transparent with windows of clear vision glass beginning no higher than 2’ above grade to at least 10’ above grade. Windows should extend across, at a minimum, 75% of the façade length. Continuous window walls should be avoided by providing architectural building treatments, mullions, building modulation, entry doors, and/or columns at appropriate intervals.

## Special Consideration for Rose Hill Business District (RH) 8

Incorporate transparent windows and doors and weather protection along all non-residential facades adjacent to a sidewalk or internal pathway. Weather protection features could include awnings, canopies, marquees or other permitted treatments. Alternative treatments may be considered if they meet the objectives. For example, reduced transparency and weather protection levels may



be considered if an alternative to configuration provides other amenities above and beyond what is required by KZC Chapter 92 and the Design Guidelines and if building details or architectural treatments provide interest at close range and won't "deaden" the pedestrian environment or create a potential safety problem.

***~~Special Consideration For Non-Retail Lobbies In Central Business District 1A & 1B~~***

~~Non-retail uses are generally not allowed along street frontage within Central Business District 1. However, in order to provide pedestrian access to office, hotel, or residential uses located off of the street frontage or above the retail, some allowance for lobbies is necessary.~~

## Guideline

~~Lobbies for residential, hotel, and office uses may be allowed within the required retail storefront space provided that the street frontage of the lobby is limited relative to the property's overall retail frontage and that the storefront design of the lobby provides continuity to the retail character of the site and the overall street.~~

### *Special Consideration for the Totem Lake Business District Core*

Since pedestrians move slowly along the sidewalk, the street level of buildings must be interesting and varied. Since the potential exists for large tenants to locate within TL 2, efforts should be made to minimize the impacts of these uses along pedestrian-oriented streets and concourses. Along 120<sup>th</sup> Avenue NE, buildings should be designed to add vitality along the sidewalk, by providing multiple entrance points to shops, continuous weather protection, outdoor dining, transparency of windows and interactive window displays, entertainment and diverse architectural elements. Ground floor development in TL 2 should be set close to the sidewalk along pedestrian streets and concourses to orient to the pedestrian and provide an appropriately-scaled environment.

### *Special Consideration for Neighborhood Business Districts, Finn Hill Neighborhood Center (FHNC) and Houghton/Everest Neighborhood Center (HENC), Bridle Trails Neighborhood Center (BCX Zone), North Juanita Neighborhood Center (BCX 1 Zone)*

#### Issue Discussion

To create a focal point for the community and engage pedestrians, buildings are encouraged to be oriented to pedestrian-oriented streets in these zones. However, commercial space that is above or below the grade of the sidewalk can compromise the desired pedestrian orientation.

## Guideline

Commercial space should generally be at grade with the adjoining sidewalk. Where this is not feasible, the building should be setback from the sidewalk far enough to allow a comfortable grade transition with generous pedestrian-oriented open space.

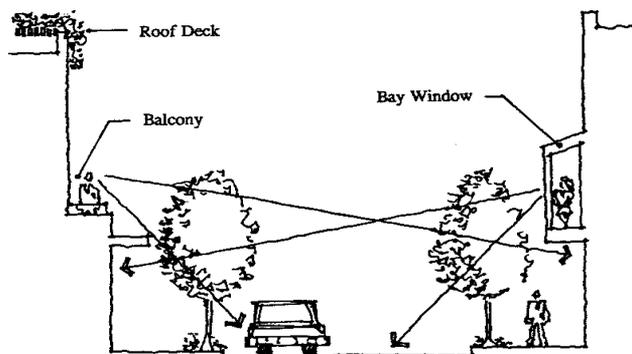
### *Special Consideration for Bridle Trails Neighborhood Center (BCX Zone)*

~~Continuous commercial building fronts should be provided along adjoining arterials and through-block pathways. Consideration should be made to maximize the usage of through-block pathways with commercial and other public activations. Any non-commercial building fronts should be located in areas where less successful commercial activity may occur. Special attention should be made in locating commercial building fronts near intersecting through-block pathways and where transit services are located.~~

## Upper-Story Activities Overlooking the Street

Issue Intent: To enhance street presence and safety through active upper-story design.

Upper-story architectural features such as balconies, roof decks, and bay windows improve the relation between the upper-story living and working units and the street. Upper-story activity provides additional security at night – people overlooking a street tend to “patrol” it – and give the street a more human, people-oriented quality.



## Discussion

Upper-story activity provides additional security at night – people overlooking a street tend to “patrol” it – and give the street a more human, people-oriented quality.

All buildings should have either an individual balcony or bay window for each dwelling unit or a collective roof deck that overlooks the street or both. This is especially important on the second and third floors where it is easier to establish connection with people on the street level.

Retail stores, offices, and studios liven second stories, particularly at night when second story activities are silhouetted.

Balconies should have direct access from an interior room and be at least 6' in depth so that two or three people can sit at a small table and have enough room to stretch their legs.

Plantings are encouraged on balconies and roof decks in order to bring more greenery into the City. Window seating at bay windows enables people to sit by a window and overlook the street.

## Guideline

All buildings on pedestrian-oriented streets should be encouraged to have upper-story activities overlooking the street, as well as balconies and roof decks with direct access from living spaces. Planting trellises and architectural elements are encouraged in conjunction with decks and bay windows. Upper-story commercial activities are also encouraged.

## Lighting from Buildings

### Issue Intent

- To enhance safety by providing light levels sufficient to adequately illuminate pedestrian areas and building facades.
- To create inviting pedestrian areas using a variety of illumination techniques.

~~To provide adequate lighting without creating excessive glare or light levels. Overpowering and uniform illumination creates glare and destroys the quality of night light. Well-placed lights will form individual pools of light and maintain sufficient lighting levels for security and safety purposes.~~

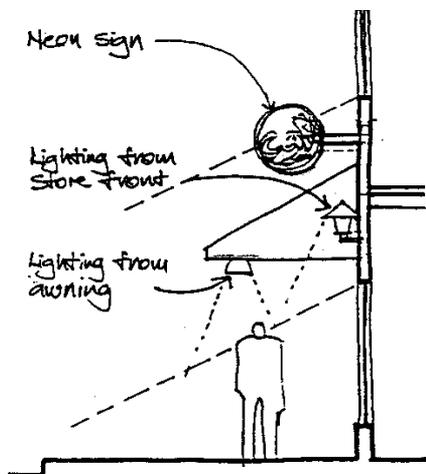
### Discussion

All building entries should be lighted to protect occupants and provide an inviting area.

Building facades, awnings, and signs should not be lighted with overpowering and uniform lights. They should be lighted with low-level building-mounted lights and placed apart to form pools of light. Lighting from storefronts, canopies, or awnings is a very attractive and effective way to light sidewalks.

Recommended Minimum Light Level:

- ◆ Primary pedestrian walkway: 2 foot candle
- ◆ Secondary pedestrian walkway: 2 foot candle
- ◆ Parking lot: 1 foot candle



## Guideline

All building entries should be well lit. Building facades in pedestrian areas should provide lighting to walkways and sidewalks through building-mounted lights, canopy- or awning-mounted lights, and display window lights. Encourage variety in the use of light fixtures to give visual variety from one building facade to the next. Back-lit or internally-lit translucent awnings should be prohibited.

## Pedestrian-Oriented Plazas

### Issue Intent

- To enrich the pedestrian environment and create gathering spaces for the community.
- To configure buildings and uses to encourage pedestrian activity and pedestrian focal points.

~~Too often we see well-designed, but empty, plazas. There is no clear formula for designing a plaza, but a poorly designed plaza will not attract people.~~

### Discussion

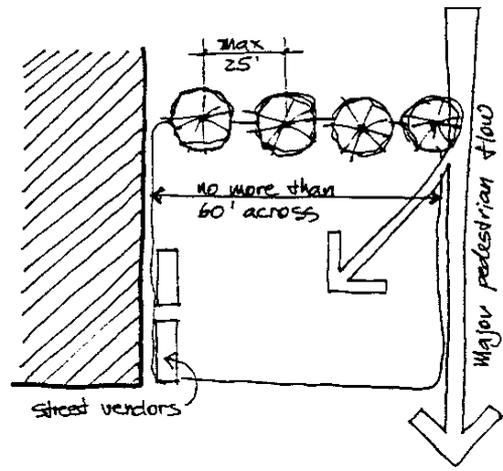
Plazas should be centrally located on major avenues, close to bus stops, or where there are strong pedestrian flows on neighboring sidewalks.

Plazas should be no more than 60' across and no more than 3' above or below the sidewalk. They must be handicapped accessible.

Plazas should have plenty of benches, steps, and ledges for seating. At least one linear foot of seating per 30 square feet of plaza area should be provided; seating should have a minimum depth of 16".

Locate the plaza in a sunny spot and encourage public art and other amenities. At least 50 percent of the total frontage of building walls facing a plaza should be occupied by retail uses, street vendors, or other pedestrian-oriented uses.

Provide plenty of planting beds for ground cover or shrubs. One tree should be required for every 200 square feet at a maximum spacing of 25' apart. Special precaution must be taken to prevent trees from blocking the sun.



## Guideline

Successful pedestrian-oriented plazas are generally located in sunny areas along a well-traveled pedestrian route. Plazas must provide plenty of sitting areas and amenities and give people a sense of enclosure and safety.

### *Special Consideration for Bridle Trails Neighborhood Center (BCX Zone)*

Plazas should be located facing pedestrian-friendly building fronts, near intersecting through-block pathways and where transit services are located. Plaza should be a focal point of public gather and seek opportunities to provide space for performances, passive and children's recreation.

### *Special Considerations for the Totem Lake Business District Core*

Public spaces, such as landscaped and/or furnished plazas and courtyards should be incorporated into the development, and be visible and accessible from either a public sidewalk or pedestrian connection. Primary pedestrian access points to retail development in TL 2 along 120<sup>th</sup> Avenue NE may be especially effective locations for public plazas.

Open spaces are especially important in TL 1, where the built environment may be dense. Well designed open spaces in front of and between buildings, visually linked with the open spaces of adjacent developments, will help to provide relief for the pedestrian.

## Pedestrian Connections

- **Issue Intent** The ability to walk directly into a commercial center from the public sidewalk, the Cross Kirkland Corridor and Eastside Rail Corridor, or a bus stop, is essential to both pedestrian and vehicular safety.

## Discussion

Well defined, direct pedestrian connections from the building to the public sidewalk are not always available in commercial centers. The connection between the internal pedestrian system on the site and the public sidewalk is often interrupted by landscaping or an automobile driveway.

Properly located landscaping can be used along with special paving to help define pedestrian links through the site.



## Guideline

Commercial developments should have well defined, safe pedestrian walkways that minimize distances from the public sidewalk, the Cross Kirkland Corridor and Eastside Rail Corridor, and transit facilities to the internal pedestrian system and building entrances.

## Blank Walls

- **Issue Intent** To enrich the pedestrian environment and create gathering spaces for the community.
- To configure buildings and uses to encourage pedestrian activity and pedestrian focal points.

~~Blank walls create imposing and dull visual barriers. On the other hand, blank walls are ready “canvases” for art, murals, and landscaping.~~

## Discussion

*Blank walls on street fronts.* Blank walls on retail frontage deaden the surrounding space and break the retail continuity of the block. Blank walls should be avoided on street front elevations. The adverse impact of a blank wall on the pedestrian streetscape can be mitigated through art, landscaping, street vendors, signs, kiosks, bus stops, or seating. Design guidelines in New York, San Francisco, and Bellevue recommend that ground floor retail with pedestrian-oriented displays be the primary uses in commercial districts. This approach is meant to restore and maintain vitality on the street via continuous rows of retail establishments.

*Blank walls perpendicular to street fronts.* In some cases fire walls require the intrusion of a flat, unadorned surface. These conditions merit landscaping or artistic treatment. Examples of such treatment include installing trellises for vines and plant material, providing landscaped planting beds that screen at least 50 percent of the wall, incorporating decorative tile or masonry, or providing artwork (mural, sculpture, relief) on the wall.



## Guideline

Blank walls should be avoided near sidewalks, parks, the Cross Kirkland Corridor and Eastside Rail Corridor, and pedestrian areas. Where unavoidable, blank walls should be treated with landscaping, art, or other architectural treatments.



## Introduction

Site features and pedestrian amenities such as lighting, benches, paving, waste receptacles, and other site elements are an important aspect of a pedestrian-oriented business district's character. If these features are design-coordinated and high quality, they can help to unify and upgrade the district's visual character. Development of a master plan for public spaces can provide a coordinated approach to their installation throughout the district.

The guidelines in this section apply primarily to elements associated with street right-of-ways, public parks, and required *major pedestrian pathways*. Although the standards do not apply to private property, except where a *major pedestrian pathway* is required, property owners are encouraged to utilize the standards in private development where they are appropriate. However, there may be cases where different site features, such as light fixtures and benches, should be selected to complement the architectural design of the individual site.

## Pathway Width

### Issue Intent

To provide wide sidewalks and pathways that accommodate pedestrian movement and provides a pleasing pedestrian experience.

Pathways must be sufficiently wide to handle projected pedestrian traffic. A pathway that is too narrow will have maintenance problems at its edges. A pathway that is too wide is unnecessarily costly and a poor use of space.

### Discussion

A pedestrian path of 10' to 12' can accommodate groups of persons walking four abreast or two couples passing each other.

A path near a major park feature or special facility like a transit center should be at least 12' wide. An 8' path will accommodate pedestrian traffic of less than 1,000 persons per hour.

### Empirical Comparison:

- ◆ Green Lake path = 8'
- ◆ Burke-Gilman Path = 8'
- ◆ Typical sidewalk = 8' to 14'

### Guideline

Design all major pedestrian pathways to be at least 8' wide. Other pathways with less activity can be 6' wide.



### Special Considerations for Juanita Business District

Through-site connections from street to street are a desirable pedestrian amenity in Land Use Area JBD-1.

The goal of these pedestrian connections will be to knit the individual developments into a more cohesive whole, providing convenient pedestrian mobility throughout even if the parcels are developed individually.

An appropriate seat width ranges from 6” to 24”.

### ***Special Consideration for North Rose Hill Business District***

Buildings in the NRHBD will be setback at least ten feet from the sidewalk. Landscaping and entry features will be located within this setback yard. Therefore, the sidewalk can be somewhat narrower than on a pedestrian oriented street.

### ***Special Consideration for Houghton/Everest Neighborhood Center***

Through block pedestrian connections and connections to the Cross Kirkland Corridor are important features that will help to provide pedestrian access throughout the center.

### ***Special Considerations for the Totem Lake Business District Core***

Through-site connections from street to street, between the upper and lower portions of TL 2, and within TL 2 are needed to provide convenient pedestrian mobility, and to contribute to the village-like character desired for TL 2. Pedestrian connections to surrounding related uses, such as the hospital campus and transit center should also be provided.

Within TL 1, buildings should be set back at least ten feet from the sidewalk. Landscaping and entry features should be located within this setback yard, allowing the sidewalk to be somewhat narrower than on a pedestrian oriented street.

### ***Special Consideration for Bridle Trails Neighborhood Center (BCX Zone)***

The through-block pathway connecting NE 70th Place to shops and services within the neighborhood center should be designed with amenities and help transition pedestrian traffic from lower grade at the street to the more level grade within the neighborhood center. Design of this pathway should provide places for people to pause and gather and provide a planted buffer from vehicular traffic.

## **Pedestrian Paths and Amenities**

### **Issues Intent**

~~Pedestrians require more detailed visual stimuli than do people in fast moving vehicles. Pedestrian paths should be safe, enjoyable, and interesting.~~

To ensure pedestrian paths are safe, enjoyable, and engaging.

### **Discussion**

Street furniture such as benches, planters, fountains, and sculptures enhance the visual experience and reduce apparent walking lengths. Planters, curbs, rails, and other raised surfaces can also be used for seating. Any height between 12” to 20” will do with 16” to 18” being the best.



Unit paving such as stones, bricks, or tiles should be installed on small plazas and areas of special interest. Asphalt can be used on minor routes to reduce cost and maintenance.

For safety reasons, lighting should be planned along all pedestrian paths. Lighting can originate either from street lights or from building-mounted lights. Street trees and shrubs should be planted along all pedestrian walkways and used to screen parking lots. For safety and appearance purposes, trees and shrubs should be pruned regularly.

### ***Special Consideration for Bridle Trails Neighborhood Center (BCX Zone)***

Streets and pathways should enhance the pedestrian experience and find opportunities to provide passive seating areas, cafe seating, green space, small scale performance area, public art, and children's recreation and small public gathering space. Sidewalks along 130th Avenue NE should enhance the City's Greenways connection.

## **Street Trees**

### **Intent**

- To enhance the pedestrian environment.
- To use trees that provide seasonal interest.
- To use trees that will not obscure views of businesses from the street.

### **Issues**

Streets are the conduits of life in a community. The repetition of trees bordering streets can unify a community's landscape. Trees add color, texture, and form to an otherwise harsh and discordant urban environment.

A strong street tree planting scheme can establish community identity and provide a respite from the weather and the built environment. Large, deciduous trees planted in rows on each side of the street can bring visual continuity to Kirkland – particularly on major entry arterials. Smaller trees should be planted in confined areas.

Street trees will not obscure businesses from the street if the appropriate trees are selected and maintained. Branches can frame ground floor businesses, allowing bus and truck movement while enhancing the pedestrian environment.

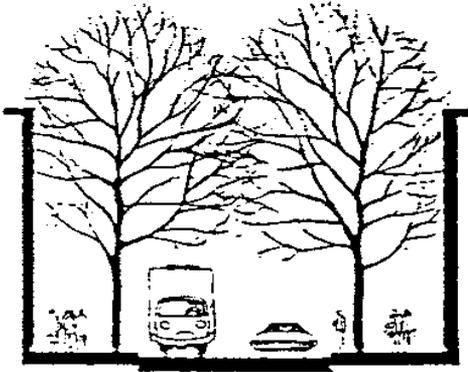
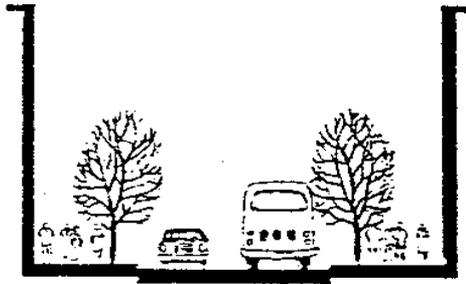
Trees should be of adequate size to create an immediate impact and have a good chance of survival.

Species with invasive root systems or that are prone to disease, intolerant of pollution, or short-lived should be avoided.

### **Guideline**

**The City should prepare a comprehensive street tree planting plan recommending species and generalized locations.**





### Special Considerations for Downtown Kirkland

A strong street tree planting scheme is especially important in downtown because of the variety of scale and architecture encouraged in private development. Major entries into Kirkland, especially along Central Way, Kirkland Avenue, Lake Street, and Market Street, should be unified by a strong street tree program.

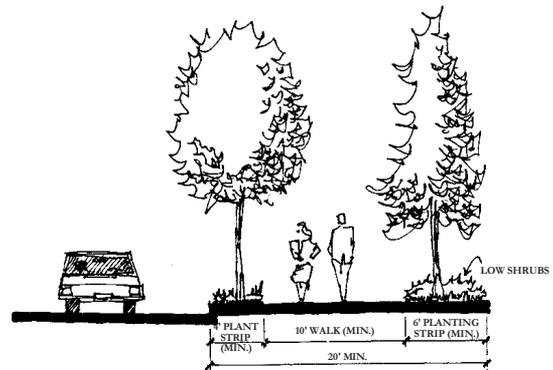
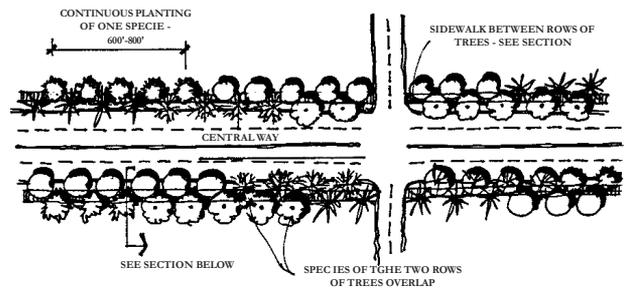
#### Some preliminary ideas for a street tree planting plan are:

*Central Way:* Two rows of trees on each side could be planted (one row near the curb and one row in the required setback on the perimeter of parking lots as in Parkplace). The two rows could feature uniform plantings of species approximately 600' to 800' long. The species could change so that different combinations of species occur along Central Way. This would provide a continuous boulevard effect and incorporate the existing trees.

*Lake Street and other pedestrian-oriented streets with narrow sidewalks:* Flowering pear trees might be a good option since they have tight narrow shapes, attractive flowers, and dark green foliage. Photinia standards might be another option since they are small and have bright red evergreen foliage.

### Special Considerations for Juanita Business District

Street trees in the business district should be upgraded with varieties that will not block views of businesses or the lake.



*Proposal for a distinctive, double-row tree planting of street trees on Central Way.*

#### Some preliminary ideas for a street tree planting plan are:

**98th Avenue NE:** Limb up existing maples and add flowering pear trees (flowers and good fall color) along the curb.

**Juanita Drive:** Choose street trees that will screen large buildings but still allow views to the lake (flowering pears for example).

**97th Avenue NE/120th Place NE:** Plant trees to screen parking lots and service entrances. Possibilities are zelkova (elm-like with good fall color) or flowering pears.

### Special Considerations for the Market Street Corridor

A consistent street tree plan should be used to add character to the Corridor. The landscape strip on the east side of Market Street adds interest and provides a more secure pedestrian environment. Additional street trees should be considered on the west side of Market Street in order to provide a similar environment.

## ***Special considerations for North Rose Hill Business District***

Feature a diverse planting of street trees that take into account width of landscape strip, location of overhead utility lines, and maintenance requirements.

Some preliminary ideas for a street tree planting plan are:

**NE 116<sup>th</sup> Street:** Add street trees that will buffer the pedestrian corridor from traffic while providing some visual access to adjacent businesses. (*Quercus rubra* (red oak), *Tilia cordata* ‘Greenspire’ (littleleaf linden), *Zelkova serrata* ‘Village Green’ for example).

**124<sup>th</sup> Avenue NE:** Choose street trees that will buffer the pedestrian but still allow some visual access to adjoining businesses (*Carpinus japonicus* (Japanese hornbeam), *Cercidiphyllum japonicum* (Katsura), *Fraxinus pennsylvanica* ‘Summit’ (Summit ash) for example).

**Slater Avenue NE:** Add trees with flowers and good fall colors as a transition to the residential portion of the neighborhood (*Malus* sp. (flowering crab), *Styrax japonicus* (Japanese snowbell), *Crataegus phaenopyrum* (Washington hawthorn), *Prunus padus* ‘Summer Glow’ (bird cherry- red leaves) for example).

## ***Special Considerations for the Totem Lake Business District Core***

Street trees within this area should be selected to achieve the varying objectives of the district. Some preliminary ideas for a street tree planting plan are:

**Totem Lake Boulevard:** South of NE 128<sup>th</sup> Street, trees should be planted that balance the goals of creating a “green corridor” along the boulevard, providing a safe and inviting pedestrian experience and enabling visibility of the site’s businesses to the freeway traveler. Smaller trees planted at frequent intervals anchored by larger, “boulevard” trees at primary site entrances would achieve these objectives. As an alternative or additional component, groupings of trees planted behind a meandering sidewalk may also be effective.

**North of NE 128<sup>th</sup> Street to NE 132<sup>nd</sup> Street,** plantings should be unified with those used along Totem Lake Boulevard to the south.

**120<sup>th</sup> Avenue NE:** South of NE 128<sup>th</sup> Street, choose street trees that will emphasize the pedestrian connection between the upper and lower mall, such as the use of larger trees at crossings and major points of entry. Choose spacing and varieties to create a plaza-like character to encourage pedestrian activity. Trees in planters and colorful flower beds will soften the area for pedestrians but allow visual access to adjoining businesses.

The tree planting plan used along NE 128<sup>th</sup> Street between Totem Lake Boulevard and 120<sup>th</sup> Avenue NE should be continued to the segment of 120<sup>th</sup> Avenue NE between NE 128<sup>th</sup> Street and NE 132<sup>nd</sup> Street, to provide a consistent identity throughout the district.

**NE 132<sup>nd</sup> Street:** Create a strong streetscape element, inviting to the pedestrian, with street trees proportionate to adjacent land uses.

## **Public Improvements and Site Features**

### **Intent**

To promote a cohesive city image through consistent, high-quality public improvements and site features.

### **Discussion**

The quality and character of public improvements and site features such as street and park lights, benches, planters, waste receptacles, pavement materials, and public signs are critical components of a city’s image. Standards for public improvements and site features, along with a master plan for public spaces, will assist in the development of a coordinated streetscape that will unify the variety of private development. Successful standards help assure high quality, low maintenance site features, and simplify the purchase and replacement of features for parks and public works departments.

Since public improvement standards have long-term implications for the community, relevant City departments must be involved in their development to make sure all concerns are met. Standards should permit some flexibility and address technical issues such as cost, availability, handicapped accessibility, and durability.



### **Guideline**

**The Planning and Building Department, along with other City departments, should develop a set of public improvement and site feature standards for use in pedestrian-oriented business districts. The standards can be the same or unique for each district. A master plan for public spaces within a district should be adopted to coordinate placement of the features and otherwise carry out the Comprehensive Plan.**

The City of Kirkland should work with interested groups to design a public sign system for gateways, pathways, information kiosks, etc., with a signature color palette and identifying logo.

## Special Considerations for the Market Street Corridor

An historic style of street light should be used to reflect the nature of the 1890's buildings in the historic district at 7th Avenue and Market Street. These lights may also be used along other stretches of the corridor, particularly in the area between the Historic District and the Central Business District.

## Special Consideration for Houghton/Everest Neighborhood Center

Pedestrian lighting should be provided along school walk routes and all pedestrian oriented streets in the center.

## Entry Gateway Features

### Intent

- To enhance the character and identity of the neighborhoods in the city.
- To provide a welcoming statement for visitors entering the city.

### Discussion

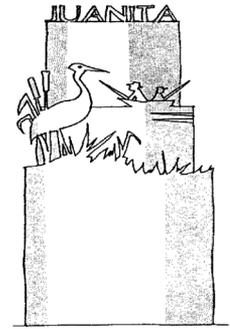
The Comprehensive Plan calls for gateway features at the key entry points into neighborhoods and business districts. Entry points differ in topography, available space, and surrounding visual character; nevertheless, gateway features should be reinforced by a unified design theme. Gateway features can be different in size or configuration, yet still incorporate similar materials, landscaping, graphics, and design elements.

### Discussion

The gateway features should frame and enhance views. Large sign bridges or flashing graphics would dominate the view and are inappropriate. Consistent elements that could be incorporated at all entry points might include:

- ◆ Distinctive landscaping such as floral displays or blue-green colored evergreen foliage.
- ◆ Multicolored masonry, perhaps forming a screen or wall on which an entry sign is placed.
- ◆ A distinctive light such as a column of glass block or cluster of globes.
- ◆ A unifying device such as the district's logo. In Downtown Kirkland, for example, a triangular sail logo could be a metal weather vane or an actual fabric sail on a steel armature.
- ◆ A repetitive element such as a series of closely spaced sails or lights.

- ◆ A trellis incorporating landscaping. A trellis or arbor is adaptable to space constraints.
- ◆ Similar artwork such as a different animal or bird sculpture at each entry.



## Guideline

Construct entry gateway features at locations noted in the Comprehensive Plan. Gateways may be constructed in conjunction with commercial development. Emphasis should be placed on framing the view into the district.

## Special Consideration for Downtown Kirkland

The transit center is another “gateway” experience. The center should be a focal feature that provides comfort and amenities for transit users. Some form of shelter with a strong architectural identity should be pursued.

## Special Consideration for Juanita Business District

The entry features should be “identity-giving elements” that reflect the business district and Juanita Bay. If successful they can become an identifying symbol or logo for the district and an attraction in themselves.

## Special Consideration for North Rose Hill Business District

Use public art and private efforts to establish gateway features that strengthen the character and identity of the neighborhood. Use landscaping, signs, structures or other features that identify the neighborhood.

At the southwest corner of NE 116<sup>th</sup> Street and 124<sup>th</sup> Avenue NE a neighborhood gateway feature such as open space or plaza with signage should be integrated with a pedestrian connection linking Slater and NE 116<sup>th</sup> Street. In the alternative, a corner land mark consisting of a combination of open space and architectural building design features should be provided to identify the business district.

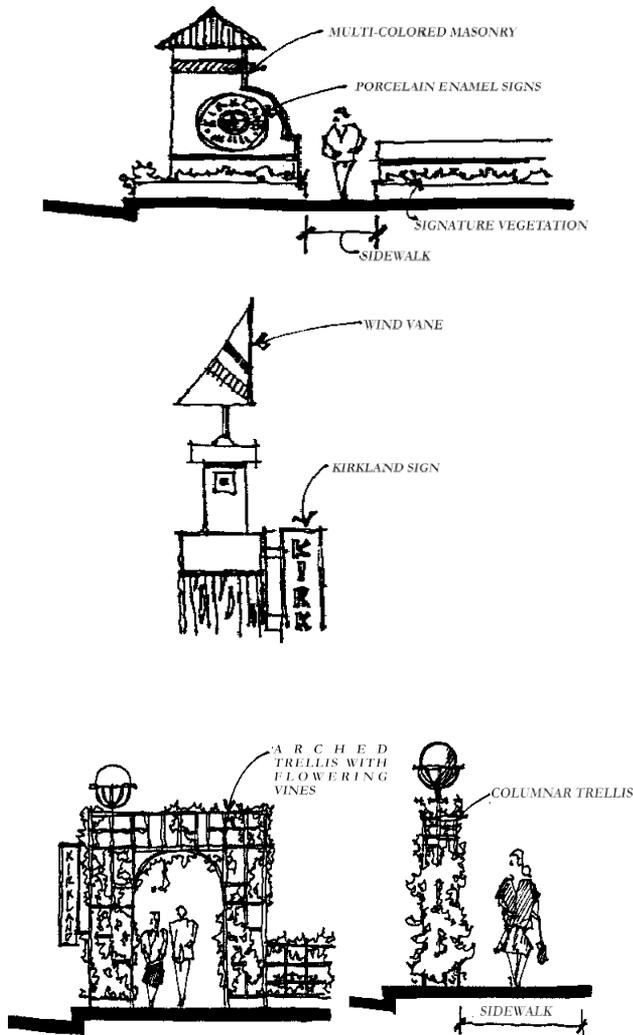
## Special Considerations for Rose Hill Business District (RH) 8

Incorporate entry gateway features in new development on fabric sail on a steel armature. NE 85<sup>th</sup> Street at 132<sup>nd</sup> Ave NE. Gateway features should some or all of the following:

- ◆ Distinctive landscaping including an assortment of varieties of roses.
- ◆ Artwork (e.g. vertical sculpture incorporating historical information about Rose Hill).
- ◆ A gateway sign with the City logo.
- ◆ Multicolored masonry forming a base for an entry sign.
- ◆ Decorative lighting elements.

## Special Considerations for the Totem Lake Business District Core

The Transit Center on the hospital campus should be a “landmark” feature for both the Totem Lake Business District Core and the hospital campus, providing a focal point for residents, employees and visitors. A combination of signs and symbols linking the transit center to the pedestrian connection along NE 128<sup>th</sup> Street, the flyer stop and the Park and Ride should be provided. Design of the transit center should be compatible with campus development yet be clearly identifiable as a facility serving the general public.



A prominent entry to the district exists at the intersection of NE 128<sup>th</sup> Street and Totem Lake Boulevard, where vehicles and pedestrians arrive from the crossing over I-405. Entry features provided in this area should contribute to the identity associated with the Business District Core.

Public art and private efforts can be used to establish gateway features to strengthen the character and identity of the Business District Core and the neighborhood. At the northern entry to the Business District Core at 120<sup>th</sup> Avenue NE and NE 132<sup>nd</sup> Street, a neighborhood entry sign or other identifying neighborhood feature should be provided. Another important entry point identified in the neighborhood plan is along Totem Lake Boulevard, just east of 120<sup>th</sup> Avenue NE. A feature providing a sense of entry into the Business District Core at this location would be appropriate.

## Public Art

### Intent

To reflect community identity and enhance civic spaces

### Issue Discussion

Art begins with the perceptions and expressive talents of individual artists. “Public art” applies that expression to the public realm either by its location in a public setting or by its emphasis on subjects relevant to the larger community. Public art contributes to the unique character, history, and sense of place of a community.

### Discussion

Public art is more than merely urban decoration; it can play an integral role in civic revitalization. Public art can make us more aware of our surroundings; reinforce the design character of our streets, parks, and buildings; commemorate special events; and serve as a catalyst for public activity and civic pride. At its best, art opens our eyes to new perceptions and helps us understand who we are and what is special about our community.

Public art is generally most effective when it is integrated with larger civic improvement efforts. Opportunities for art can be identified earlier and funding can be used more effectively. For example, emblems, lighting, pavement decorations, and decorative pedestrian furniture can be incorporated as part of a street improvement project at little cost to the total project such as in Seattle’s Third Avenue transit corridor, Port Angeles’s Maritime Flags, and Portland’s Transit Mall.

The involvement of an artist in the design of a park, fountain, street lighting, or signs can add a special quality that has more impact than if the artwork and the functional element were decorated separately. The famous art nouveau detailing on Paris’s metro stations is a good example.

### Guideline

**Kirkland should continue its tradition of encouraging public**

art pieces.





## Parking Lot Location and Design

### Introduction

In pedestrian-oriented business districts, improperly located and poorly designed parking lots can destroy the ambiance and qualities that attract people to the district in the first place. This section contains guidelines to direct development of parking facilities. The number of required stalls is specified in the Kirkland Zoning Code. The guidelines in this section deal with:

- ◆ Parking lot location – Parking in front of buildings is discouraged, and combined lots that serve more than one business or use are encouraged.
- ◆ Parking lot entrances – The number of entries is addressed.
- ◆ Parking lot circulation and pedestrian access – Clear internal vehicular and pedestrian circulation is required, especially in large parking lots.
- ◆ Parking garages – Parking garages provide convenient, less intrusive parking. Yet, garages can themselves be intrusive since they are often large monolithic structures with little refinement, interest, or activity. The guidelines for parking garages are intended to make them fit into the scale and character of pedestrian-oriented districts.

- ◆ Parking Lot Landscaping – Parking lot landscaping should be more extensive if the lot has to be in a location that is visible from a street or public park than if the lot is located at the rear of the site hidden away from streets and neighboring properties. This provision is made to encourage parking lot development in less visible locations.

On the following pages, urban design guidelines are presented that outline design information, concepts, and solutions associated with parking lot development. They serve as a conceptual basis for the regulations in the Zoning Code.

### Parking Locations and Entrances

#### Issue/Intent

- To minimize the visual and functional impact of parking facilities on the fronting street, pedestrian environment, and neighboring properties.
- To enhance pedestrian and vehicular safety.
- To maintain traffic flow on streets.
- To promote shared parking.

#### Discussion

The ingress and egress of vehicles in parking lots disrupts pedestrian movement and through traffic – especially near intersections. Moreover, busy streets are a safety hazard. Parking lots that are accessed by a single curb cut reduce potential conflict and use land more efficiently. Also, combining the parking lots of individual stores into a large parking network makes it easier for patrons to find convenient parking stalls.

Parking lots should be encouraged in rear or side yards. The parking lot at Wendy's restaurant on Central Way is an example of this configuration.

The City of Seattle limits parking lot access on pedestrian-oriented streets such as Broadway on Capitol Hill.



## Guideline

**Minimize the number of driveways by restricting curb cuts and by encouraging property and business owners to combine parking lot entrances and coordinate parking areas. Encourage side and rear yard parking areas by restricting parking in front yards. Require extensive screening where there is front yard parking.**

### ***Special Consideration for Downtown Kirkland***

Parking lot location and design is critical on busy entry streets such as Market Street, Central Way, Lake Street, Kirkland Avenue, and in the congested core area where pedestrian activities are emphasized. The *Downtown Plan* calls for limiting the number of vehicle curb cuts.

### ***Special Consideration for Juanita Business District and North Rose Hill Business District***

Shared accesses and reciprocal vehicular easements should be established in order to reduce the number of curb cuts. The Juanita Business District Plan also encourages shared parking/service areas in Land Use Area JBD-1. This is particularly critical in TL 2, where buildings should front on 120<sup>th</sup> Avenue NE to foster the desired pedestrian-oriented environment.

### ***Special Consideration for the Totem Lake Business District Core***

Throughout the Totem Lake Business District Core, parking areas located between the street and the building should be discouraged. This is particularly critical in TL 2, where buildings should front on 120th Avenue NE to foster the desired pedestrian-oriented environment.

### ***Special Considerations for Houghton/Everest Neighborhood Center***

Consolidate driveways within the neighborhood center, especially existing driveways that are currently closely spaced. Restrict or mitigate surface parking between buildings and the Cross Kirkland Corridor.

### ***Special Considerations for Rose Hill Business District (RH) 8***

Encourage development to locate and orient buildings towards the street with parking to the side or the rear. At a minimum this should include:

- Non-residential facades located directly adjacent to the sidewalk or buildings featuring a modest landscaped front yard area or plaza area between the sidewalk and the facade.
- Primary building entries and windows facing the

street.

- Landscaping trimmed to maintain visibility between the sidewalk and the building.

Office and residential developments are encouraged to locate and orient buildings towards an interior open space or courtyard, where space allows. In this scenario, primary building entries may orient towards the open space provided there is direct visibility into the open space from the sidewalk. Windows should be provided on the street facade. Buildings may be located towards the rear of the property provided they meet landscaping, parking, pathway, and facade standards along the front.



## Circulation Within Parking Lots

### ~~Issue~~Intent

- To ensure pedestrian safety and provide clear and connected vehicular circulation routes.

- 

~~Large parking lots can be confusing unless vehicle and pedestrian circulation patterns are well organized and marked. Parking lots should be combined to reduce driveways and improve circulation.~~

### Discussion

Vehicle Circulation. Parking lots should have few dead-end parking lanes and provide drive-through configurations. The APA *Aesthetics of Parking* publication recommends channelized queuing space at the entrances and exits to parking lots to prevent cars from waiting in the street.

Pedestrian Circulation. Good pedestrian circulation is critical. A clear path from the sidewalk to the building entrance should be required for all sites, even through parking lots in front yards. For sites with large parking lots, clear pedestrian circulation routes within the lot from stalls to the building entrances should be provided. In addition, a raised concrete pavement should also be provided in front of the entrance as a loading or waiting area so the entrance will not be blocked by parked vehicles. Finally, pedestrian access between parking lots on adjacent properties should be provided.

### Guideline

**Parking lot design should be clear and well organized. Space should be provided for pedestrians to walk safely in all parking lots.**

### *Special Consideration for Downtown Kirkland*

Because land is limited in Downtown Kirkland, efficient and compact parking lot configurations are a top priority. Parking lots in the periphery of the core area that accommodate about 100 vehicles (approximately 3/4 to 1 acre) should be articulated with landscaped berms.

## Parking Lot Landscaping

### ~~Issue~~Intent

- To minimize the visual and spatial impacts of parking lots ~~Parking lots are typically unsightly, require vast quantities of space, break the links between buildings, and destroy the continuity of streetfronts. If possible,~~

~~parking lots should be located at the rear of buildings. When this is not possible, landscaping can be used to break up and screen parking lots.~~

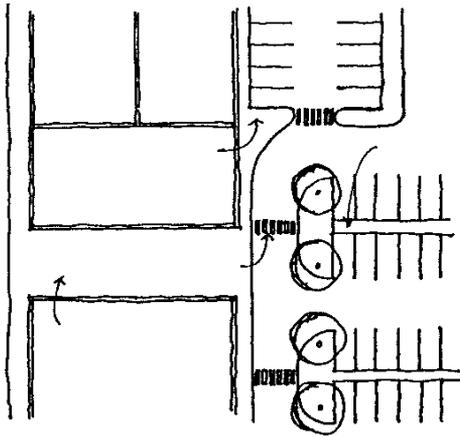


## Discussion

Parking lots can be concealed by a structural screen wall or through the use of plant materials. Plant materials can create dense, hedge-like screens, separating lots from adjacent uses or public rights-of-way. Perimeter plantings must provide an adequate screen. A screen wall constructed in a similar style as adjacent development may be used in lieu of perimeter landscaping.

Trees along the edges of and within parking lots can effectively soften an otherwise barren and hostile space. Interior plantings can be consolidated to provide islands of greenery or be planted at regular intervals. Use of drought-tolerant plants can improve the likelihood that the landscaping will survive and look good.

Landscaping guidelines should be flexible and allow creative screening methods (e.g., clustering trees, berming, mixing structures, and trees). Less landscaping should be required if the lot is hidden from view.



## Guideline

**Parking lots must be integrated with the fabric of the community by creatively using landscaping to reduce their visual impact.**

### *Special Considerations for the Market Street Corridor*

Screening and landscaping should be required where parking is adjacent to single family residential uses in order to reduce impacts on the adjoining homes.

### *Special Consideration for Juanita Business District, North Rose Hill Business District and the Totem Lake Business District Core*

Screening and landscaping should be required where parking is adjacent to sidewalks in order to improve visual qualities and reduce clutter.

Within TL 2, the provision of landscaping to soften the impacts of cars and pavement is important. Clusters of trees rather than single trees may be more effective in certain portions of the mall's parking areas. Visibility of the mall from the freeway should be considered when evaluating the locations and types of landscaping to be used.

## Parking Garages

### Issue Intent

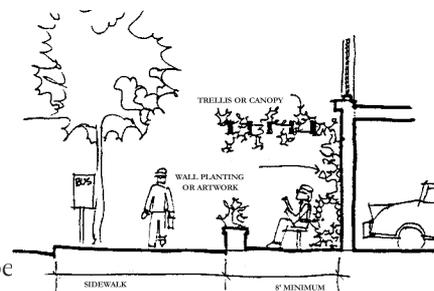
To mitigate the visual impacts of parking structures.

~~Parking garages are some of the most unattractive buildings built during the past several decades. Most new parking structures are designed with little or no attention to screening or treatment of the facades.~~

### Discussion

There are several ways to mitigate the visual impacts of parking garages in the urban environment. A garage in a pedestrian area can contain a pedestrian-oriented retail use in the ground floor area of the garage adjacent to the street. Cafes, newsstands, or other small shops can fit well within the typical parking garage, requiring the space equivalent to only one 20' bay of parking.

Also, parking garages can be set back to provide space for a small landscaped plaza with a seating area. Moreover, the wall of the garage behind the plaza can be used as a canvas for landscaping or artwork. Also, the plaza could be covered with a glass canopy or trellis. The plaza should face south to receive sunlight. A plaza of this type is ideal for bus stops or street vendors.



In non-pe round the perimeter of parking garages can help screen their bulk. Strict standards for minimum landscaping around garages should be developed.

## Guideline

The intrusive qualities of parking garages must be mitigated. In pedestrian areas, ground-level retail uses or appropriate pedestrian spaces should be required. Also, extensive landscaping should be required near residential areas and in highvisibility locations. On hillsides and near residential areas the stepping back or terracing of upper stories should be considered to reduce scale.

### *Special Consideration for Downtown Kirkland*

Garages built on Downtown Kirkland's perimeter slopes, near residential areas, or near the waterfront can fit less obtrusively into the landscape when terraced. Treatment of the facade of the parking structure can be just as effective in mitigating the visual impacts of parking garages as pedestrian-oriented businesses, plazas, or landscaped setbacks at the ground level.

### *Special Consideration for the Totem Lake Business District Core*

The development densities planned for the Totem Lake Business District Core may result in the need for large parking structures to support them. Careful design of the structures will be important to retain a visually attractive environment.

The location of parking structures along pedestrian-oriented streets or pedestrian pathways should be discouraged. Where parking structures cannot be located underground and must be provided on the ground floor, an intervening use is desirable to retain the visual interest along the street. If parking areas are located in a separate structure from the primary use, the structure must be set back from the street, and screened with substantial landscaping.

Within TL 2, if it is not possible or practical to locate parking structures behind a building or underground, structured parking should be developed, oriented and screened to complement adjacent buildings, reduce automobile/pedestrian conflicts, and support the pedestrian environment. Artwork, display windows, trellises and/or dense vegetation are examples of screening devices that may be successful in balancing the scale of the structure with the pedestrian environment.





# Scale

## Introduction

When architects talk about a building’s “scale,” they generally mean the perceived size of the building relative to an individual person or its surroundings. The term “human scale” is used to indicate a building’s size relative to a person, but the actual size of a building or room is often not as important as its perceived size. Architects use a variety of design techniques to give a space or structure the desired effect; whether it be to make a room either more intimate or spacious, or a building either more or less imposing. Frank Lloyd Wright, for example, used wide overhangs and horizontal rooflines to make his prairie-style houses appear lower and longer, better fitting into the flat, midwestern landscape. Unless the objective is to produce a grandiose or imposing building, architects generally try to give a building a “good human scale,” meaning that the building is of a size and proportion that feels comfortable. For most commercial buildings, the objective is to attract customers and visitors by designing comfortable, inviting buildings.

Generally, people feel more comfortable in a space where they can clearly understand the size of the building by visual clues or proportions. For example, because we know from experience the size of typical doors, windows, railings, etc., using traditionally-sized elements such as these provides a sense of a building’s size. Greek temples that feature columns, but not conventional doors, windows, or other elements, do not give a sense of human scale (although the Greeks subtly modified the properties and siting of their temples to achieve the desired scale). The guidelines in this section describe a variety of techniques to give a comfortable human scale by providing building elements that help individuals relate to the building.

“Architectural scale” means the size of a building relative to the buildings or elements around it. When the buildings in a neighborhood are about the same size and proportion, we say they are “in scale.” It is important that buildings have generally the same architectural scale so that a few buildings do not overpower the others. The exception to this rule is an important civic or cultural building that has a prominent role in the community. For example, nobody accuses a beautiful cathedral in a medieval European town of being “out of scale.” Because the Comprehensive Plan encourages a variety of different uses and building heights, such as in Downtown Kirkland, the buildings’ sizes will vary widely. To achieve a more harmonious relationship between the buildings and a more consistent character, design techniques should be used to break the volume of large buildings down into smaller units. Several guidelines in this section are directed toward achieving a consistent scale within districts.

The following guidelines illustrate some design techniques to give buildings a “sense of scale.” The regulations in the Zoning Code related to scale require that project architects address the issues of human and architectural scale while providing a wide range of options to do so.

## Fenestration Patterns

### Intent

To promote human scale and visual interest through well-proportioned, varied window patterns appropriate to their context.

Issue—DiscussionThe size, location, and number of windows in an urban setting creates a sense of interest that relies on a subtle mixture of correct ratios, proportions, and patterns. Excess window glazing on a storefront provides little visual contrast; blank walls are dull and monotonous. The correct window-to-wall ratio and a mix of fenestration patterns can create an enjoyable and cohesive urban character on both pedestrian- and automobile-oriented streets.

Many local contemporary buildings have “ribbon windows” (continuous horizontal bands of glass) or “window walls” (glass over the entire surface). Although effective in many settings, these window types do little to indicate the scale of the building and do not necessarily complement the architecture of small-scaled buildings. Breaking large expanses or strips of glass with mullions or other devices can help to give the building a more identifiable scale.

## Discussion

According to an old architectural cliché, windows are a building's eyes. We look to windows for visual clues as to the size and function of the building. If the window areas are divided into units that we associate with small-scale commercial buildings, then we will be better able to judge the building's size relative to our own bodies. Breaking window areas into units of about 35 square feet or less with each window unit separated by a visible mullion or other element at least 6 inches wide would accomplish this goal. Another successful approach is multiple-paned windows with visible mullions separating several smaller panes of glass. But on the ground floor where transparency is vital to pedestrian qualities, this device may be counterproductive.

Patterns of fenestration should vary depending on whether the street is pedestrian- or automobile-oriented. A window pattern that is interesting from a car may be monotonous to a slow-moving pedestrian; likewise, a window pattern that is interesting to a pedestrian may seem chaotic from a fast-moving car. Thus, pedestrian-oriented fenestration should allow for more complex arrangements and irregularity while automobile-oriented fenestration should have more gradual changes in pattern and larger and more simple window types.

An optimum design goal would allow for varied treatment of window detailing with unifying features such as 18" to 24" sills, vertical modulation in structure, varied setbacks in elevation, and more highly ornamented upper-story windows. Excessive use of ribbon windows throughout a building does not engage the eye and should be avoided.



## Guideline

**Varied window treatments should be encouraged. Ground floor uses should have large windows that showcase storefront displays to increase pedestrian interest. Architectural detailing at all window jambs, sills, and heads should be emphasized.**

## Special Considerations for the Market Street Corridor

Window treatment in the historic district should reflect the trim detailing, size, proportions, location and number of windows in the existing historic buildings in the district.

## Special Consideration for Downtown Kirkland

Breaking larger window areas into smaller units to achieve a more intimate scale is most important in Design Districts 1, 2, 4, 8, and the southwest portion of 3 where new buildings should fit with older structures that have traditional-styled windows.

## Architectural Elements: Decks, Bay Windows, Arcades, Porches Intent

To enhance building character and human scale.

## Issue-Discussion

Special elements in a building facade create a distinct character in an urban context. A bay window suggests housing, while an arcade suggests a public walkway with retail frontage. Each element must be designed for an appropriate urban setting and for public or private use. A building should incorporate special features that enhance its character and surroundings. Such features give a building a better defined "human scale."

## Discussion

Requirements for specific architectural features should be avoided and variety encouraged. Building designs should incorporate one or more of the following architectural elements: arcade, balcony, bay window, roof deck, trellis, landscaping, awning, cornice, frieze, art concept, or courtyard. Insistence on design control should take a back seat to encouraging the use of such elements.

## Guideline

**Architectural building elements such as arcades, balconies, bay windows, roof decks, trellises, landscaping, awnings, cornices, friezes, art concepts, and courtyards should be encouraged.**

## ***Special Consideration for Downtown Kirkland***

Pedestrian features should be differentiated from vehicular features; thus fenestration detailing, cornices, friezes, and smaller art concepts should be concentrated in Design Districts 1 and 2, while landscaping and larger architectural features should be concentrated in Design Districts 3, 5, 7, and 8.

## ***Special Consideration for the Totem Lake Business District Core***

Balconies provide private open space, and help to minimize the vertical mass of structures. Residential building facades visible from streets and public spaces should provide balconies of a sufficient depth to appear integrated with the building and not “tacked on”.

## ***Special Considerations for Rose Hill Business District (RH) 8***

Site and orient multi-story buildings to minimize impacts to adjacent single family residents. For example, if a multi-story building is located near a single family property, provide landscaping elements and/or minimize windows and openings to protect the privacy of adjacent homes. Another consideration is to increase upper level building setbacks.

## **Building Modulation – Vertical Intent**

- To encourage an architectural scale of development that is compatible with the vision for the district.
- To add visual interest to buildings.

### **Issue Discussion**

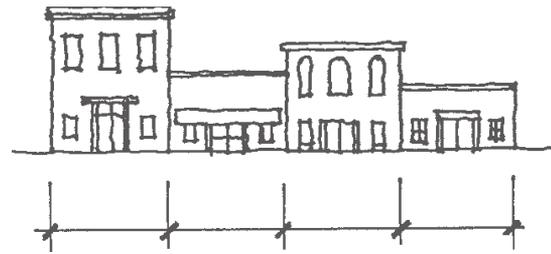
Vertical building modulation is the vertical articulation or division of an imposing building facade through architectural features, setbacks, or varying rooflines. Vertical modulation adds variety and visual relief to long stretches of development on the streetscape. By altering an elevation vertically, a large building will appear to be more of an aggregation of smaller buildings. Vertical modulation is well-suited for residential development and sites with steep topography.

### **Discussion**

Urban design guidelines should address vertical modulation in order to eliminate monotonous facades. Vertical modulation may take the form of balcony setbacks, varied rooflines, bay windows, protruding structures, or vertical circulation elements – the technique used must be integral

to the architecture.

*Vertical modulation in urban setting*



Vertical modulation is important primarily in neighborhoods where topography demands a stepping down of structures. The vertical modulation of a large development project in a residential area can make the project appear to be more in scale with the existing neighborhood. Long facades can be vertically modulated to better conform to the layout and development pattern of single-family houses. The vertical modulation of buildings on steep slopes also provides terraced development rather than one single building block, thereby better reflecting the existing terrain.

## Guideline

**Vertical building modulation should be used to add variety and to make large buildings appear to be an aggregation of smaller buildings.**

*This building uses both horizontal and vertical modulation to add interest and reduce its visual bulk.*

### ***Special Considerations for the Totem Lake Business District Core***

Since greater heights are allowed in TL 1 than elsewhere in the city, the impacts of increased height are a concern. Impacts associated with taller buildings are generally ones of reduced open space and privacy, shadowing and loss of light.

Massing of development in slimmer but taller towers rather than in shorter, wider buildings presents an opportunity to create open space between existing buildings, particularly when buildings step back from property lines and neighboring structures. For new buildings to fit in to the existing setting, a balance between higher and lower structures should be maintained.

To preserve openness between structures, separation between towers, both on a development site and between adjacent properties, should be provided. The specific separation should be determined based on height, relation and orientation to other tall structures, configuration of building mass and solar access to public spaces.



Taller buildings or “towers” in TL 1 should have relatively compact floor plates. The use of towers above a two-three story podium creates a varied building footprint and the perception of a smaller overall building mass. When the building’s mass is instead concentrated in lower buildings with larger floor plates, greater emphasis should be placed on open space and plazas to provide relief at the pedestrian level.

Design treatments used in the upper portion of a building can promote visual interest and variety in the Totem Lake Business District Core skyline. Treatments that sculpt the facades of a building, provide for variety in materials, texture, pattern or color, or provide a specific architectural rooftop element can contribute to the creation of a varied skyline.

### ***Special Considerations for Neighborhood Business Districts, Finn Hill Neighborhood Center (FHNC) and the Houghton/Everest Neighborhood Center, Bridle Trails Neighborhood Center (BCX Zone)***

#### **Issue Discussion**

Because these districts are typically integrated into residential areas, the design should reflect the scale of the neighborhood by avoiding long façades without visual relief.

#### **Guideline**

**Façades over 120 feet in length should incorporate vertical definition including substantial modulation of the exterior wall carried through all floors above the ground floor combined with changes in color and material.**

### ***Special Considerations for Rose Hill Business District (RH) 8***

Because of the proximity to low density areas, a combination of vertical modulation techniques to reduce the architectural scale of buildings is desirable to provide visual relief for the surrounding residential neighborhood. Modulation is encouraged at 30 foot intervals. Alternatives will be considered provided they meet the intent of the guidelines.

## **Building Modulation – Horizontal Intent**

- To encourage an architectural scale of development that is compatible with the vision for the district.
- To add visual interest to buildings.

#### **Issue Discussion**

Horizontal building modulation is the horizontal articulation or division of larger building façades. The lower portion of a multi-story building should incorporate pedestrian-scale elements and a strong base. The top of the building should

## **Discussion**

A lively urban character uses a variety of architectural forms and materials that together create an integrated pattern of development with recurring architectural features. Horizontal awnings, balconies, and roof features should be incorporated into new development provided that their appearance varies through the use of color, materials, size, and location.



*Horizontal modulation elements: canopy, brick banding, and window details.*

incorporate distinctive roof treatments. Elevations that are modulated with horizontal elements appear less massive than those with sheer, flat surfaces. Horizontal modulation is well suited to downtown areas and automobile-oriented streetscapes where the development of tall building masses is more likely.

## Guideline

Horizontal building modulation may be used to reduce the perceived mass of a building and to provide continuity at the ground level of large building complexes. Building design should incorporate strong pedestrian-oriented elements at the ground level and distinctive roof treatments.

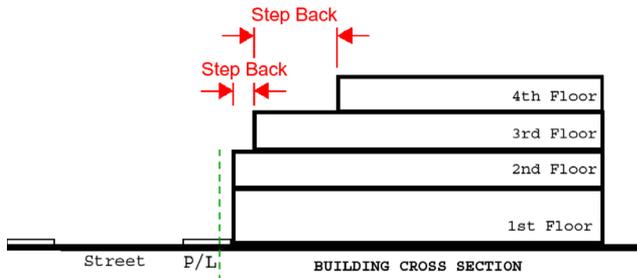
### *Special Consideration for Downtown Kirkland*

Large-scale developments, particularly east of the core area, should stress continuity in streetscape on the lower two floors. Setback facades and varied forms should be used above the second stories.

### *Special Consideration for Building Massing in Central Business District 1 (CBD 1A & 1B) and the Houghton/ Everest Neighborhood Center - Upper Story Step Backs, Bridle Trails Neighborhood Center (BCX Zone)*

#### Issue Discussion

Taller buildings can negatively affect human scale at the street level and should be mitigated. Upper story step backs provide a way to reduce building massing for larger structures. An upper story building step back is the horizontal distance between a building façade and the building façade of the floor below.



By reducing mass at upper stories, visual focus is oriented towards the building base and the pedestrian experience. In addition, greater solar access may be provided at the street level due to the wider angle which results from the recessed upper stories.



*Marina Heights*

Upper story step backs are appropriate in areas where taller buildings are allowed and imposing building facades at the sidewalk are intended to be avoided.

## Discussion

Design guidelines should address upper story step backs to improve the pedestrian experience and maintain human scale. When viewed from across the street, upper story step backs generally reduce perceived building massing and provide additional sunlight at the ground level. When viewed from the sidewalk immediately adjacent to the building, upper story step backs reduce the view of the upper stories and help maintain pedestrian scale by preventing large buildings from looming over the sidewalk.

Since the benefits of upper story step backs are primarily experienced from the public realm in front of buildings, the step backs should be located within a zone along the front property line.

Overly regimented building forms along front facades should be avoided to prevent undesirable building design. The arrangement of building step backs should create varied and attractive buildings consistent with the principles discussed in previous sections.

Upper story step backs also allow for additional eyes on the street in the form of decks and/or balconies. Upper story activities help improve the relationship of the building to the streetscape. Landscaping should also be incorporated at the upper stories to help soften building forms.

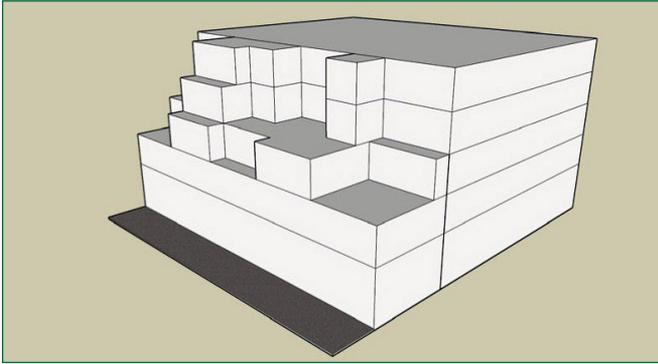
In order to quantify upper story step backs, measurement should be taken from the property line. Setback is the term used to describe the distance of a structure from the property line. By measuring from the pre-existing property line, setbacks provide for consistency in measurement and will account for projects where additional right-of-way is proposed or required along the property frontage for wider sidewalks and/or additional public open space.

The required upper story setback should be allowed to be reduced if an equal amount of beneficial public open space is provided at the street level. A certain amount of building cantilevering over sidewalks may also be allowed if the pedestrian environment is not adversely affected.

The Kirkland Zoning Code establishes the requirements for upper story setbacks and provisions for allowing reductions to the required upper story setbacks in exchange for open space at the street level. The following guidelines are intended to provide the Design Review Board the tools to create varied and attractive buildings.

## Guidelines - Upper Story Setbacks

- ◆ **Buildings above the second story (or third story where applicable in the Downtown Plan) should utilize upper story step backs to create receding building forms as building height increases, allow for additional solar access, and maintain human scale at the street level.**
- ◆ **The final arrangement of building mass should be placed in context with existing and/or planned improvements, solar access, important street corners, and orientation with the public realm.**
- ◆ **A rigid stair step or “wedding cake” approach to upper story step backs is not appropriate.**
- ◆ **Decks and/or balconies should be designed so that they do not significantly increase the apparent mass of the building within the required upper story setback area.**



*Varied step back approach*

- ◆ In addition to applying setbacks to upper stories, building facades should be well modulated to avoid blank walls and provide architectural interest.
- ◆ Along pedestrian oriented streets, upper story building facades should be stepped back to provide enough space for decks, balconies and other activities overlooking the street.
- ◆ Landscaping on upper story terraces should be included where appropriate to soften building forms and provide visual interest.
- ◆ Continuous two or three story street walls should be avoided by incorporating vertical and horizontal modulations into the building form.
- ◆ Limited areas of vertical three, four, or five story walls can be used to create vertical punctuation at key facades. Special attention to maintain an activated streetscape is important in these areas.
- ◆ For properties on Park Lane which front multiple streets and upper story setbacks are proposed to be averaged, concentration of upper story building mass along Park Lane should be avoided.

### ~~Guideline – Open Space at Street Level~~

~~Reductions to required upper story setbacks may be appropriate where an equal amount of beneficial public open space is created at the street level consistent with the following principles:~~

- ◆ ~~Public open space should be open to the sky except where overhead weather protection is provided (e.g. canopies and awnings).~~
- ◆ ~~The space should appear and function as public space rather than private space.~~
- ◆ ~~A combination of lighting, paving, landscaping and seating should be utilized to enhance the pedestrian experience within the public open space.~~
- ◆ ~~Public open space should be activated with adjacent shops, outdoor dining, art, water features, and/or landscaping while still allowing enough room for pedestrian flow.~~

- ◆ ~~Where substantial open space “trade-offs” are proposed, site context should be the primary factor in the placement of the public open space (e.g. important corners, solar access).~~

### ~~Guideline for CBD 1A & 1B only – Building Cantilevering Over Sidewalks~~

~~Buildings may be allowed to cantilever over sidewalks if a sidewalk dedication and/or easement is required consistent with following guidelines:~~

- ◆ ~~The total length of cantilevered portions of a building should be no more than 1/3rd of the entire length of the building façade. The cantilevered portions of a building should be spread out and not consolidated in a single area on the building façade.~~
- ◆ ~~Unobstructed pedestrian flow should be maintained through the subject property to adjoining sidewalks.~~
- ◆ ~~Space under the building cantilever should appear and function as part of the public realm.~~
- ◆ ~~The sense of enclosure is minimized.~~

### *Special Considerations for Neighborhood Business Districts and FHNC*

#### Issue Discussion

Where buildings are close to the street in these neighborhood areas, vertical building massing can negatively affect human scale at the street level. Upper story step backs provide a way to reduce building massing. An upper story building step back is the horizontal distance between a building façade and the building façade of the floor below.

#### Guideline

Above the ground floor, buildings should utilize upper story step backs to create receding building forms as building height increases. Rather than a rigid stair step approach, varied step back depths and heights should be used to create well modulated façades and usable decks and balconies overlooking the street.

#### Issue

Within the South Rose Hill Neighborhood Plan, additional mitigation of scale impacts is called for.

#### Guideline

*Building height, bulk, modulation, and roofline design should reflect the scale and character of adjoining single- family development. Special Considerations for Rose Hill Business District*  
8

Encourage buildings to utilize architectural styles common to neighboring residential areas. This includes hipped or gabled roofs, front porches or covered entries, and fenestration patterns that relate to adjacent single family homes.

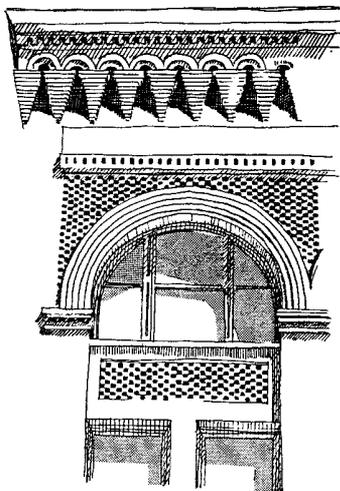
# Building Material Color and Detail

## Introduction

Many historic cities and towns owe much of their charm to a limited palette of building materials. One thinks of how the white clapboard houses of a New England village or the tile-roofed structures of an Italian hill town provide a more unified, consistent visual character. Today, there is a wide spectrum of building materials available, and modern towns such as Kirkland feature a variety of materials and colors. Architects have demonstrated that materials often considered unattractive, such as cinderblocks or metal siding, can be successfully used in attractive, high-quality buildings.

When buildings are seen from a distance, the most noticeable qualities are the overall form and color. If we take the typical building in Kirkland to be 100' wide and 35' tall, then we must be at least 200' away from the building for it to fit within our cone of vision so that we can perceive its overall shape. At that distance, windows, doors, and other major features are clearly visible.

However, as we approach the building and get within 60' to 80' from the building (approximately the distance across a typical downtown street), we notice not so much the building's overall form as its individual elements. When we get still closer, the most important aspects of a building are its design details, texture of materials, quality of its finishes, and small, decorative elements. In a pedestrian-oriented business district, it is essential that buildings and their contents be attractive up close.



Therefore, these design guidelines are intended to allow a variety of materials and colors, but direct the use of certain materials so that their application does not significantly detract from design consistency or quality. Most of the regulations in the Zoning Code deal with the application of specific materials such as metal siding and cinderblocks so that their potentially negative characteristics are minimized. In addition, the guidelines include guidelines and regulations that require all buildings to incorporate design details and small-scale elements into their facades.

## Ornament and Applied Art

### Issue Intent

~~Ornament and applied art To add quality, visual interest, and a sense of human scale to the built environment and It is necessary to understand the place and appropriateness of ornament in order to maintain a cohesive and integrated urban setting.~~

### Discussion

Ornament and applied art can be used to emphasize the edges and transition between public and private space, and between walls to ground, roof to sky, and architectural features to adjacent elements. Ornament may consist of raised surfaces, painted surfaces, ornamental or textured banding, changing of materials, or lighting. Therefore, buildings should incorporate art features that emphasize architectural elements and connections. Ornament should also maintain a cohesive relationship to its setting, emphasizing its connection to the surrounding space.

### Guideline

**Ornament and applied art should be integrated with the structures and the site environment and not haphazardly applied. Significant architectural features should not be hidden, nor should the urban context be overshadowed. Emphasis should be placed on highlighting building features such as doors, windows, eaves, and on materials such as wood siding and ornamental masonry. Ornament may take the form of traditional or contemporary elements. Original artwork or hand-crafted details should be considered in special areas.**

### *Special Considerations for the Market Street Corridor*

Emphasis on building features such as doors, windows, cornice treatment, bricks and ornamental masonry should be taken into consideration when designing new or remodeled buildings in the historic district. These features should be in keeping with the building materials, colors and details of the existing historic buildings.

## Color

### Issue Intent

To strengthen community identity and emphasize a cohesive and context-sensitive color scheme.

- ~~Color bolsters a sense of place and community identity (e.g., white New England villages, adobe-colored New Mexico towns, limestone Cotswold villages). Kirkland should consider emphasizing the existing color scheme and developing a unified design identity.~~

### Discussion

A variety of colors should be used in Kirkland. By no means should design be limited by overly-restrictive guidelines dictating color use. Based on Kirkland's existing color scheme, the following general guidelines can prevent garish, incongruous colors from being inappropriately applied or juxtaposed to more subdued earth tones and colors.

- ◆ Where appropriate, use the natural colors of materials such as brick, stone, tile, and stained wood (painted wood is acceptable).
- ◆ Use only high-quality coatings for concrete.
- ◆ Emphasize earth tones or subdued colors such as barn red and blue-gray for building walls and large surfaces.
- ◆ Reserve bright colors for trim or accents.
- ◆ Emphasize dark, saturated colors for awnings, and avoid garish and light colors that show dirt.
- ◆ Avoid highly-tinted or mirrored glass (except stained-glass windows).
- ◆ Consider the color of neighboring buildings when selecting colors for new buildings.

### Guideline

**Color schemes should adhere to the guidelines enumerated above. The use of a range of colors compatible within a coordinated color scheme should be encouraged.**

### *Special Considerations for Bridle Trails Neighborhood Center (BCX Zone)*

Special attention to the use of colors and materials should be used on a building's upper stories to reduce the appearance of taller buildings.

## Street Corners

### Issue Intent

- To enhance the appearance of highly visible locations.

- To enhance the character and identity of the design districts.
- To enhance pedestrian access and safety.

~~Street corners provide special opportunities for visual punctuation and an enhanced pedestrian environment. Buildings on corner sites should incorporate architectural design elements that create visual interest for the pedestrian and provide a sense of human proportion and scale.~~



## Discussion

Corners are crossroads and provide places of heightened pedestrian activity. Rob Krier notes that: “The corner of a building is one of the most important zones and is mainly concerned with the mediation of two facades.” Corners may be accentuated by towers and corner building entrances.

## Guideline

**Buildings should be designed to architecturally enhance building corners.**

### *Special Consideration for Downtown Kirkland*

Special attention should be paid to both the design and detailing of new buildings on corner sites in the pedestrian oriented design districts. Existing buildings could incorporate some of these elements (human-scale and visual punctuation) through the use of such elements as awnings and well-designed signs at the corner.

Downtown Kirkland has several “T” intersections, and the building located at the terminus of the street view corridor presents a high-visibility opportunity for special architectural treatment.

The corner of Central Way and Third Street marks a prominent gateway to the core area as well as the Downtown Transit Center and deserves special design emphasis.

### *Special Consideration for Houghton/Everest Neighborhood Center*

The corner of NE 68th Street and 108th Avenue NE provides a gateway to the Neighborhood Center. Buildings at this corner should be designed to enhance this gateway with elements such as building setbacks and step backs, architectural features, public open space, view preservation and art (see also Design Guidelines for Entry Gateway Features). Building frontages should encourage street level pedestrian activity.

### *Special Considerations for Rose Hill Business District (RH) 8*

Encourage design treatments that emphasize street corners through the use of building location and design, plaza spaces, landscaping, distinctive architectural features, and/or signage. Incorporate storefronts directly at the 128th street corners to reinforce the desired pedestrian-oriented character of the district. Encourage special landscaping elements on all street corners including a variety of plant types and

textures that add seasonal interest. Encourage all buildings located at or near street corners to incorporate special architectural elements that add visual interest and provide a sense of human proportion and scale. This could include a raised roofline, turret, corner balconies, bay windows, special awning or canopy design, and/or distinctive use of building materials.



# Signs

## Intent

To encourage the use of creative, well-crafted signs that will contribute to the character of the district while providing adequate identification for buildings and tenants.

## Issues Discussion

Kirkland's Zoning Code regulates signs throughout the city in order to create a high-quality urban environment. Automobile-oriented signs typically found on commercial strips can be overpowering and obtrusive. Pedestrian signs are smaller and closer to viewers; thus, creative, well-crafted signs are more cost effective than large signs mounted high on poles.

Signs should be an integral part of a building's facade. The location, architectural style, and mounting of signs should conform with a building's architecture and not cover up or conflict with its prominent architectural features. A sign's design and mounting should be appropriate for the setting.

## Discussion

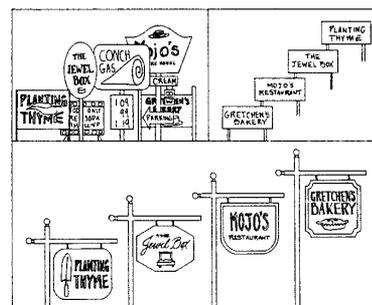
Pedestrian-oriented signs are most effective when located within 15' of the ground plane. Three-inch-high letters can be read at 120' and 6" letters read at 300'. Large lettering is not necessary. The signs should be aligned to people on sidewalks and not automobile drivers. "Blade" signs or single signs hanging below canopies or small signs located on canopies or awnings are effective.

Signs with quality graphics and a high level of craftsmanship are important in attracting customers. Sculpted signs and signs that incorporate artwork add interest. Signs with front lighting and down lighting (but not internal lighting) are recommended. Neon signs are appropriate when integrated with the building's architecture.

Generic, internally-lit "can" signs that are meant to be set anywhere are not appropriate. Ground-mounted signs should feature a substantial base and be integrated with the landscaping and other site features. Mounting supports should reflect the materials and design character of the building or site elements or both.

*Too much variety*      *Too much uniformity*

*Though unified by common design elements, signs can still express the individual character of businesses.*



reflect the historic nature of the buildings in the area.

## Guidelines

- ◆ All signs should be building-mounted or below 12' in height if ground mounted. Maximum height is measured from the top of the sign to the ground plane.
- ◆ No off-premises commercial signs, except public directional signs, should be permitted. No billboards should be permitted.
- ◆ Signs for individual parking stalls should be discouraged. If necessary, they should not be higher than necessary to be seen above bumpers. Parking lot signs should be limited to one sign per entrance and should not extend more than 12' above the ground.
- ◆ Neon signs, sculptural signs, and signs incorporating artwork are encouraged.
- ◆ Signs that are integrated with a building's architecture are encouraged.
- ◆ Shingle signs and blade signs hung from canopies or from building facades are encouraged.
- ◆ Traditional signs such as barber poles are encouraged.

### *Special Considerations for Downtown Kirkland*

- ◆ The Downtown Plan's mandate for high-quality development should also be reflected in sign design.
- ◆ No internally lit plastic-faced or can signs should be permitted.
- ◆ All signs in the downtown should be pedestrian-oriented. Master-planned sites such as Parkplace may also include signs oriented to automobile traffic for the whole complex.

### *Special Considerations for the Totem Lake Business District Core*

- ◆ Signs within the TL2 should be coordinated through a sign package for the entire property.

### *Special Considerations for the Market Street Corridor*

Electrical signs are not allowed along the Market Street Corridor. Signs within the historic district should



# Natural Features

## Introduction

### General

An important aspect of a pedestrian-oriented business district is its physical setting. Natural features of a place are key to residents' and visitors' perception. This section lays out guidelines which serve to merge the design of structures and places with the natural environment. It discusses concepts behind new landscaping as well as the maintenance and protection of existing natural features.

### *Special Considerations for Downtown Kirkland*

A primary goal stated in the Downtown Plan's Vision Statement is to "clarify Downtown's natural physical setting." Besides its excellent waterfront, Downtown Kirkland's most important natural feature is its bowl-shaped topography which provides views down from the heights and views from the downtown of the wooded hillsides surrounding the district. The valley topography also helps to define the downtown's edges and facilitates the transition from largely commercial activities in the valley floor to the mostly residential areas in the uplands. Although Peter Kirk Park is a man-made open space, it too provides a naturalizing function.

### *Special Considerations for Juanita Business District*

The underlying goal of redevelopment in the business district is to create a neighborhood-scale, pedestrian district which takes advantage of the amenities offered by Juanita Bay.

### *Special Considerations for the Totem Lake Business District Core*

An important goal in the Totem Lake Business District Plan is to establish a green corridor extending in an east/west direction across the neighborhood. Portions of the green corridor follow Totem Lake Boulevard, along the western boundary of TL 2. Properties abutting the designated greenbelt should be landscaped with materials that complement the natural areas of the green corridor where possible.

## Visual Quality of Landscapes

### Issue Intent

- To enhance the visual quality of the urban environment.
- To incorporate greenery into the urban environment.

~~The relationship between landscaping and architecture is symbiotic; plant materials add to a building's richness, while the building points to the architectural qualities of the landscaping.~~

### Discussion

Foliage can soften the hard edges and improve the visual quality of the urban environment. Landscaping treatment in the urban environment can be categorized as a *pedestrian/ auto, pedestrian, or building landscape*.



**The Pedestrian/Auto Landscape** applies to where the pedestrian and auto are in close proximity. Raised planting strips can be used to protect the pedestrian from high-speed and high-volume traffic. Street trees help create a hospitable environment for both the pedestrian and the driver by reducing scale, providing shade and seasonal variety, and mitigating noise impacts.

**The Pedestrian Landscape** offers variety at the ground level through the use of shrubs, ground cover, and trees. Pedestrian circulation, complete with entry and resting points, should be emphasized. If used effectively, plant materials can give the pedestrian visual cues for moving through the urban environment. Plant materials that provide variety in texture, color, fragrance, and shape are especially desirable.

**The Building Landscape.** Landscaping around urban buildings – particularly buildings with blank walls – can reduce scale and add diversity through pattern, color, and form.

Examples of how landscaping is used to soften and enhance



the visual quality of the urban environment include:

- ◆ Dense screening of parking lots.
- ◆ Tall cylindrical trees to mark an entry.

- ◆ Continuous street tree plantings to protect pedestrians.
- ◆ Several clusters of dense trees along long building facades.
- ◆ Cluster plantings at focal points.
- ◆ Parking with trees and shrubs planted internally as well as on the perimeter.

## Guidelines

The placement and amount of landscaping for new and existing development should be mandated through design standards. Special consideration should be given to the purpose and context of the proposed landscaping. The pedestrian/auto landscape requires strong plantings of a structural nature to act as buffers or screens.

The pedestrian landscape should emphasize the subtle characteristics of the plant materials. The building landscape should use landscaping that complements the building's favorable qualities and screens its faults.

Along the Cross Kirkland Corridor and Eastside Rail Corridor, landscape design should screen where necessary, but generally soften the edge between the public and private space to integrate and complement corridor functions.

### *Special Consideration for North Rose Hill Business District*

A dense landscape buffer should be utilized to provide a transition separating commercial uses from adjoining single family or multi-family residential uses.

### *Special Consideration for the Totem Lake Business District Core*

Within TL 1, special landscaping elements such as gateways, arches, fountains and sculptures should be incorporated, in order to create a lively streetscape and provide visual interest along the street edge. Where possible, existing mature landscaping should be retained and incorporated into new development to soften the impact of increased site coverage and preserve the green character of the area.

### *Special Consideration for Bridle Trails Neighborhood Center (BCX Zone)*

A combination of both street and private trees with associated landscaping should be used to help mitigate the urban edges of the neighborhood center adjacent to residential neighborhoods.

## Protection and Enhancement of Wooded Slopes

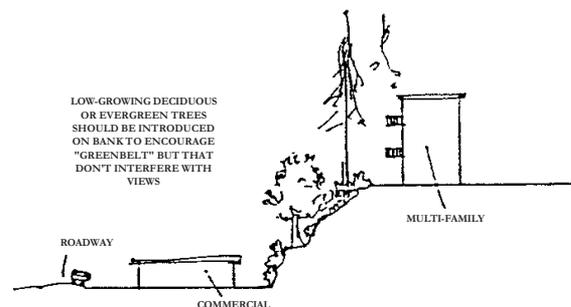
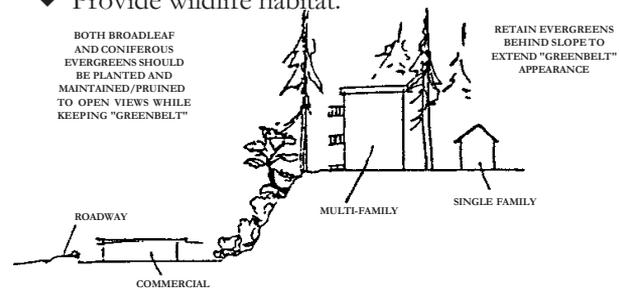
### Issue-Intent

To use topography for natural screening that maintains views. Topography provides opportunities for natural screening that maintains views.

### Discussion

New plantings on wooded slopes should be selected for their slender, open growth pattern. Limbing-up and thinning-out branches should also be allowed to maintain views while keeping the character of the wooded hillsides. Weed species should be removed and replaced with appropriate native species. Wooded slopes can:

- ◆ Reduce visual impacts of the urban environment.
- ◆ Separate uses by providing a transition zone.
- ◆ Mitigate urban noise and air pollution for upland uses.
- ◆ Provide wildlife habitat.



### Guidelines

Vegetation on slopes should be preserved and maintained as a buffer using native vegetation wherever possible.

New multifamily and single-family residential developments on slopes should be required to retain about 30 percent of the site in wooded open space and inventoried significant trees. Tree removal or enhancement can be determined by the use and site design.

Property owners of lowlands should be sensitive to upland uses and enhance hillsides to maintain existing views. Deciduous trees should be restricted to small varieties; coniferous evergreens should be thinned-out or limbed-up to allow for views from adjoining properties.

In developments above view slopes, coniferous evergreens should be incorporated into the site back from the slope to give continuity with the wooded slope. The back sides of commercial lots at the base of hillsides should be planted to screen upland properties from unsightly views of rooftops.

### **Special Consideration for Downtown Kirkland**

Using and enhancing existing wooded slopes is especially important to Kirkland's natural setting. The hillsides surrounding Downtown Kirkland can provide a "ring of green." As vegetation ascends the slope it provides a "greenbelt" effect. The proper maintenance or enhancement of such slopes need not disrupt view corridors of upland properties.

### **Special Consideration for Juanita Business District**

The views of wooded hillsides surrounding the Juanita Business District are a local asset that can be used to upgrade the area's visual impact.

## **Height Measurement on Hillsides**

### **Issue**

Maintaining views and enhancing natural land forms is important to the design character of Kirkland. The scale relationships of built forms to their terrain should minimize visual barriers to views and lessen the impact on surrounding neighborhoods. In order to promote responsible design, building height restrictions should permit a development envelope that conforms to the terrain. Terracing, the stepping down of horizontal elements, is an effective way to develop hillsides and maintain views.



*nnTerraced buildings reflect the hillside topography ringing Kirkland's Downtown.*

## **Discussion**

The visual character of a landscape should be reflected in the buildings. Buildings that do not conform to steep inclines detract from the natural features of the site and should be avoided. In contrast, buildings that use the terrain as an opportunity for variation in the built form easily fit into their setting without disruption. Terracing a building to roughly parallel the slope of a site will create a building envelope that follows the contour of its property. Terraced roof decks, modulated roofs, and sloped roofs can carry out this objective.

## **Guideline**

**The top of the building should roughly follow the slope of the existing terrain.**

## **Views of Water**

### **Intent**

- To strengthen Kirkland's sense of place by preserving and enhancing views of Lake Washington.

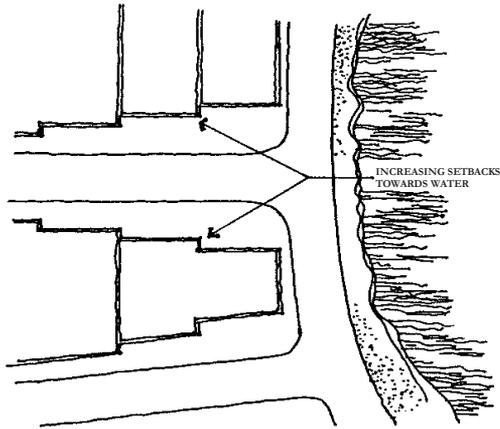
~~Issue~~ Discussion Views of Lake Washington give Kirkland its sense of place within the regional context. The waterfront remains an exceptional resource that should be better linked to nearby districts. A water view is a recurring reminder of the direction, function, and origin of Kirkland.

## **Discussion**

Views may be considered in three ways. The *distant panorama* may be seen from one-quarter to more than one mile away. Development has eliminated most of Kirkland's panoramic views; remaining views should be protected. *View corridors* are places where an avenue between buildings creates a slotted visual path allowing a glimpse of the water beyond. *Proximity views* are those adjacent to and within one block away from the waterfront; they extend the waterfront's character. Each type of view is critical to Kirkland's urban design character.

View corridors and panoramic views from higher ground can be protected by height restrictions and limitations on rooftop clutter. Existing structures in some areas block views of the Lake. With renovation of existing structures, opening up of views should be encouraged. New development should respect the existing view corridors.

Proximity views require much larger fields of vision, therefore, development should remain a comfortable distance from the shore and be set back along view corridors. This will allow views of the water to widen from increasingly closer distances and will eliminate an abrupt change between development and shoreline.



### Guideline

Existing views should be maintained. This can be accomplished by widening setbacks as development approaches the water. Buildings should step down hillsides. Buildings and rooftop appurtenances should be placed perpendicular to the water in order to safeguard views.

### *Special Consideration for Juanita Business District*

View corridors to the Lake should be explored through new development in the business district. Existing residential views and view opportunities through Juanita Beach Park and down public streets should be preserved.

### *Special Consideration for Houghton/Everest Neighborhood Center*

Buildings, landscaping and streetscape features along the NE 68th Street corridor should be designed to preserve existing views from the public right-of-way. Public spaces should be oriented to take advantage of views when possible.

## Culverted Creeks

### Intent

To restore natural character by reopening stream beds when feasible.

Issue Discussion Often stream beds fall victim to progress and their stream banks are reduced to a drain pipe. One way to further the objective of clarifying the natural physical setting is to reopen stream beds wherever possible.

### Guideline

Opportunities should be sought to restore portions of culverted creeks to their natural state.

### *Special Consideration for Downtown Kirkland*

A former stream bed, now enclosed in culverts, flows through the center of downtown from 6th Street, through Peter Kirk Park, just south of Central Way and into Marina Park. A restored stream bed could be incorporated in the parks and other public sites, and possibly on private property.

### *Special Considerations for the Totem Lake Business District Core*

One channel of the Totem Lake tributary extends along I-405, west of Totem Lake Boulevard in a culvert to Totem Lake. If it is feasible, restoration of this stream bed could be incorporated into the green corridor design developed for this segment of Totem Lake Boulevard. Another tributary of Juanita Creek runs across the northwest section of the Business District Core, with portions in a culvert and other portions remaining in an open stream bed. Redevelopment of these properties could include restoration of the culverted portions of the stream as an amenity provided on site.

# City of Kirkland Design Guidelines NE 85th Street Station Subarea Plan

May 31, 2022



THIS PAGE INTENTIONALLY LEFT BLANK.

# Table of Contents

## **Policy Overview**

Introduction.....	5
Design Vision.....	6
Comprehensive Plan Design Direction.....	7

## **Overview of Design Districts**

Green Innovation District.....	10
Forbes Lake District.....	12
Rose Hill Gateway District.....	13
Downtown Gateway District.....	14

## **Design Guidelines**

All Districts .....	16
Site Planning .....	16
Building Design.....	25
Green Innovation District.....	30
Forbes Lake District.....	32
Rosehill Gateway District .....	36
Downtown Gateway District.....	37

# 1.0

**Policy Overview**

# 1 | Introduction

## **Purpose of the Design Guidelines**

This document sets forth a series of Design Guidelines, adopted by Section 3.30 of the Kirkland Municipal Code, that will be used by the City in the design review modification process and the alternative compliance review process for projects in the NE 85th Street Station Area.

The Design Guidelines are intended to be used in conjunction with the Form-Based Code requirements established in Chapter 57 of Kirkland Zoning Code. In cases where the Zoning Code establishes flexible standards to encourage creative building design and attractive public spaces, the Design Review Board Planning Official will use these guidelines to administer that flexibility through the design departures and minor variation modification authority of KZC 142.

~~The Design Guidelines do not set a particular style of architecture or design theme. Rather,~~

~~they will establish a greater sense of quality, unity, and conformance with Kirkland's physical assets and civic role. The Design Guidelines will work with improvements to streets and parks and the development of new public facilities to create a dynamic setting for civic activities and private development~~

## **Use of the Guidelines for Alternative Compliance**

The Alternative Compliance process (outlined in KZC Chapter 142) is an optional path that allows applicants to propose substantial design modifications in place of the design regulations outlined in Chapter 92, or other zone-specific sections of the KZC. The Alternative Compliance Proposals will be reviewed by the Design Review Board (DRB) and will be reviewed for consistency with the objectives of the design guidelines.





### 3 | Comprehensive Plan Design Direction

The urban design framework establishes a set of overarching goals, policies and strategies to shape future public and private development and investments in the district. The Station Area Design District is divided into four urban design subdistricts. While the design of public and private development will be guided in a manner that creates a cohesive identity for the Station Area, each subdistrict will evolve into its own unique neighborhood character and identity, described in more detail below.

#### **Goal - Focus growth in inclusive housing and jobs near transit.**

There is a mutually supportive relationship between transit ridership and the amount of housing, jobs, and services near transit. The Station Area Plan designates the areas closest to the future BRT Stride station as priority locations for increased development. Not only are these areas prime opportunities to broaden the mix of jobs and housing choices within the station area, this strategy focuses growth in a more sustainable, compact form. In addition, the areas closest to the future station on the east side of I-405 are reserved for taller office development. This serves a dual role of focusing growth in the City where residents and employees have the best access to high-capacity transit for the station and using larger office buildings as a buffer to protect residences from the noise and air pollution that come from high volume roadways like I-405.

#### **Policy - Establish a strong public realm network and transit-oriented community that puts people first.**

The vision for the station area includes a robust, vibrant public realm with a mix of active ground floor uses, generous sidewalks, and improved tree canopy. The urban design framework identifies key streets where a combination of public and private investments will create focal points and destinations for the district, the city, and the region. These include enhancing NE 85th

Street to a more urban street that becomes a place for people to engage, supporting retail-focused streets like 120th Ave NE near Forbes Lake, and neighborhood hubs like the 7th Ave corridor in Norkirk. Each of these focal points brings together recommendations around mobility, public realm, land use, sustainability, and building massing.

#### **Policy - Connect across barriers with a multi-modal transportation network.**

As a station area plan, it's particularly important to create a network of mobility options that connect transit users between the station and key services and destinations. Green midblock connections help break down large blocks into walkable distances. New and enhanced sidewalks and bikeways provide safe and comfortable walking and biking connections throughout the district. Finally, increased transit service, including the Stride BRT station and future King County Metro's K-line BRT, flexible parking policies, and strategic roadway capacity improvements provide a multi-faceted approach to mitigate congestion and accommodate travel needs and parking demand. This holistic approach to mobility is integrated into all aspects of the urban design framework.

#### **Policy - Leverage existing natural systems and resources, enhance ecosystem performance, and increase resilience.**

Like all of Kirkland, the station area is a rich natural environment with important ecological assets and opportunities to improve the sustainability and resilience of the district. Updated policies encourage stormwater management through on-site green infrastructure like bioswales in streetscapes and within larger developments. Street types in the form-based code will lead to increased tree canopy in the public realm, and ecological assets like Forbes Lake become the focus of a new boardwalk network and "trailhead" that's integrated into the streetscape at 120th Ave NE and NE 90th St.

**Policy - Ensure appropriate development scale with transitions to adjacent neighborhoods and design regulations.**

While planning for growth in the station area, supporting transitions in scale to adjacent neighborhoods is a key focus of the urban design framework. The form-based code regulates elements of massing and form to step down from larger commercial office blocks to mid-rise neighborhood mixed use development, and eventually to smaller “missing middle” infill. Special rules for transitions, landscaping requirements, and other policies further specify how new development should respond to the existing context. Additional design guidelines and the City’s Design Review process will ensure that building massing and details reflect a pedestrian-oriented district.



# 2.0

## Overview of Design Districts

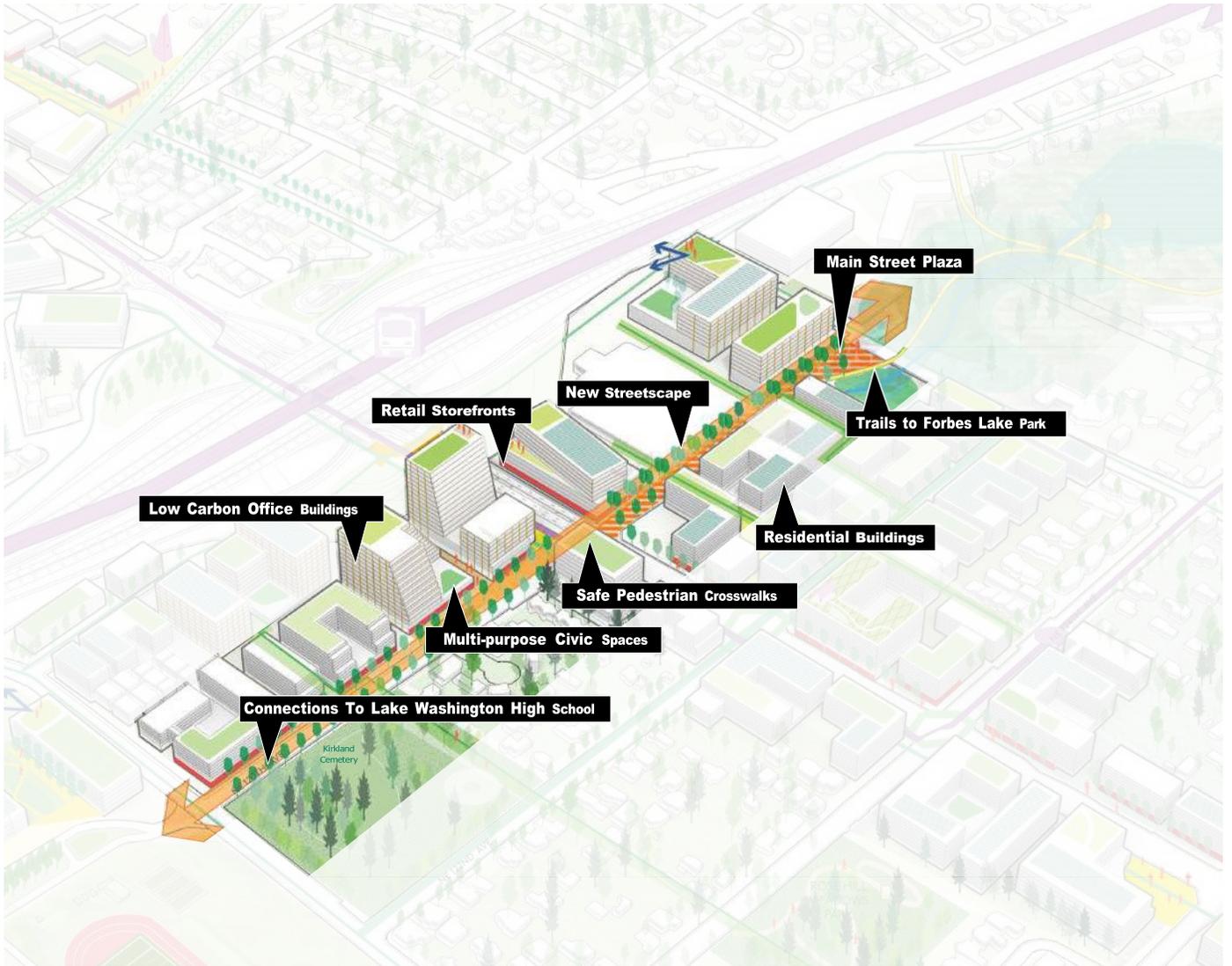
## 4 | Green Innovation District

This vibrant, mixed use district is a model of innovation and place for community, students, and the workforce to connect. It transitions from shops and office uses to mixed use and residential buildings, to civic uses. Active transportation choices, connections to green space, and a walkable 120th Ave NE offer a healthy lifestyle.

The larger sites within this subarea should coordinate the phasing and organization of major redevelopment around new internal vehicular and pedestrian circulation systems with buildings orienting toward the internal circulation networks and adjacent streets. Large sites along NE 85th Street can provide a welcoming pedestrian and visual entrance to the District from the Stride BRT Station. These large sites

can provide their own pedestrian-oriented focal points that include a plaza area surrounded by shops or wide sidewalk areas along an interior access street.

Because this area allows some of the most intensive development in the Station Area, thoughtful design transitions from north to south and west to east are important to ensure that new development integrates into the surrounding land use context.



## 5 | Forbes Lake District

A walkable mixed-use district with opportunities for shops and office uses as well as mid-rise residential uses, organized around a green main street corridor with retail and active uses combined with small open spaces on 120th that connects to Forbes Lake. Biophilic design and visible water, energy, and biodiversity strategies tell the story of this place.

Similar to the Green Innovation District, large site developments in this District create opportunities to arrange development around complete internal circulation networks that provide a framework for building placement and arrangement of pedestrian open spaces and amenities. Development on NE 85th Street should also provide a welcoming pedestrian and visual entrance to the District.



## 6 | Rose Hill Gateway District

A corridor-based gateway with a mix of active ground floors and mid-rise residential along NE 85th Street that focuses on creating a strong sense of arrival from Redmond with streetscape design, public art, and urban design features.

The District creates an environment where pedestrians and bicyclists can move comfortably along NE 85th Street. Corner treatments at each intersection, including an interaction of open spaces and building architecture, provide opportunities to create gathering places for the neighborhood and a distinct identity for the District.



## 7 | Downtown Gateway District

A gateway district to Downtown Kirkland via 6th St that emphasizes mid-rise residential and office uses along 6th and important bicycle and pedestrian connections along green pathways to and from the Stride BRT Station and the Cross Kirkland Corridor.



# 3.0

## Design Guidelines

The Guidelines in Section 8 apply to all districts. Sections 9 – 11 identify Guidelines that are district-specific and respond to key locations defined in the City's Comprehensive Plan as requiring special attention.

## 8 | All Districts

**Overall Intent:** Create a pedestrian-oriented mixed use transit district.

### Site Planning

#### 1. Streetscape

**Intent:** Maintain a continuous and safe streetscape with a pedestrian-friendly character.

- In conjunction with the required street types, streets should contribute to the physical safety and comfort of pedestrians .
- Use design elements such as separate storefronts, pedestrian-oriented signs, exterior light fixtures, awnings, and overhangs to add interest and give a human dimension to street-level building facades.
- In general, buildings with active ground floor uses should be set as close as possible to sidewalk to establish active, lively uses. Maintain a continuous street wall, limiting gaps to those necessary to accommodate vehicular and pedestrian access.
- Encourage recessed main building and/or shop entrances consistent with a traditional “main street” design that is inviting and promotes street-scape continuity.
- The corners of buildings located at street intersections should recess to promote visibility and allow for a collection of people.
- Allow larger buildings to recess from the sidewalk edge to allow for entry forecourts, provided street continuity is not interrupted along the majority of the block.



## 2 Public Spaces: Plazas, Courtyards, Terraces, and Gardens

**Intent:** Provide a friendly pedestrian environment by creating a variety of usable and interesting public and semi-public open spaces.

- Position plazas in visible locations on major internal circulation routes, close to bus stops, or where there are strong pedestrian flows on neighboring sidewalks. For large sites, development should be configured to create a focal plaza or plazas. Plazas should establish a relationship with the adjacent sidewalk or internal pathway to enhance visibility and accessibility.
- Incorporate plenty of benches, steps, and ledges for seating. A combination of permanent and moveable seating is encouraged. Seating areas should be provided with views of amenities, landscaping elements, or people watching. Seating should also encourage use by individuals, small groups, and civic gathering where appropriate
- Provide storefronts, street vendors, or other pedestrian-oriented uses, to the extent possible, around the perimeter of the plaza
- Provide landscaping elements that add color and seasonal interest. This can include trees, planting beds, potted plants, trellises, and hanging plants
- Incorporate pedestrian amenities, including:
  - pedestrian scaled lighting
  - special paving, such as integral colored/stained concrete, stone, brick, or unit pavers
  - specialty pedestrian scale bollards or other types of accent lighting
  - public art and/or water features
- Consider the solar orientation and wind patterns in the design of the open space and choice of landscaping to maximize outdoor comfort.
- Make plazas and courtyards comfortable for multiple types of human activity and social interaction – standing, sitting, talking, eating, etc.
- Create a sense of enclosure and space definition within outdoor spaces through a combination of building and landscape elements. Oversized spaces that lack definition are discouraged.
- Usable ground level, rooftop, and/or terrace open space should be provided for multifamily residential uses. Open space should be large enough to provide functional leisure or recreational activity and provide for a range of activities and age groups, including children's play areas.



### 3. Pedestrian Connections and Wayfinding

**Intent:** Create a network of safe, attractive, and identifiable linkages for pedestrians and bicyclists.

- Provide clearly defined pedestrian connections at locations specified in the Zoning Code and Citywide Transportation Connection Map. Connections that are publicly accessible should be designed to be clearly recognizable as part of the pedestrian and bicycle network.
- Ensure that pathways are well illuminated. Pathways should provide added safety with abutting active uses and visibility from upper story uses wherever feasible.
- Wayfinding signage should be incorporated at key locations and intersections of pathways to help orient users to public and private destinations within the Station Area and City.
- Connections should be designed inclusively and be accessible to all. Where grade transitions are necessary, provide graceful physical and visual transitions through the use of landscaping, terraced planters, overlooking balconies, wide and inviting stairways, and other pedestrian connections. Stairs connecting to bikeways should include runnels to allow pedestrians to change grade with their bikes.

### 4. Lighting

**Intent:** Ensure that lighting contributes to the character of the Station Area, provides personal safety, and does not disturb adjacent developments and residences.

- Use City-approved fixtures for street lighting along City streets.
- Lighting elements throughout the Station Area and on adjoining rights-of-way should be coordinated, including public open spaces, accent lighting, and streets.
- Lighting should include non-glaring design, such as cut-off fixtures that avoid light spilling over onto other properties.
- Flood lighting of entire building facades should not be allowed.
- Lighting on upper levels should be sensitive to adjoining residences.



## 5. Screening of Trash and Service Areas

**Intent:** To screen trash and service areas from public view.

- All service, loading, and trash collection areas should be screened by a combination of planting and architectural treatment similar to the design of the adjacent building.
- Avoid locating service, loading, and trash collection facilities in pedestrian-oriented areas, including midblock connections.



## 6. Signs

Intent: Create signs that are creative, engaging, and effective for a variety of user groups, respond to a variety of spaces, and reflect the desired character for design districts.

Large site developments should create Master Sign Plans that are in keeping with the following design objectives:

- Signs should be complementary and integrated with the unique character of the specific areas or buildings where they are located.
  - Signs should be high quality and consistent with the contemporary urban character of comparable developments in Kirkland.
  - The design of buildings should identify locations, sizes, and general design for future signs.
- Where Master Sign Plans are required in KZC 57, the Master Sign Plan should include a hierarchy of elements based on function, such as:
    - site signs for entries, wayfinding, District identity
    - building signs for addressing and landmarking
    - tenant signs to encourage expressive individualization



### 7. Landscaping

Intent: To enhance the visual quality of the urban environment and provide multi-benefit landscaping that provides beauty and high performance ecosystem functions.

- Project landscaping should incorporate the Green Factor requirements of KZC Chapter 57 into the overall project design. These sustainable landscape elements should be designed and implemented to provide a high level of ecosystem function in terms of urban heat island mitigation, biofiltration, reduced irrigation, and support for pollinators and other ecosystem benefits identified in the Chapter.

- In addition to ecosystem function, the Green Factor requirements should be integrated into building design elements like vertical and horizontal modulation to enhance design objectives. Large tree species planted with generous soil volumes can work with horizontal building modulations to enhance the effectiveness of façade breaks. Similarly, green roofs and terrace plantings can increase the effectiveness and visibility of vertical building modulations by softening the mass of upper stories and creating an interesting skyline.
- Encourage landscape elements such as rain gardens and Silva Cell installations that provide screens its faults while not blocking views of the business or signage.

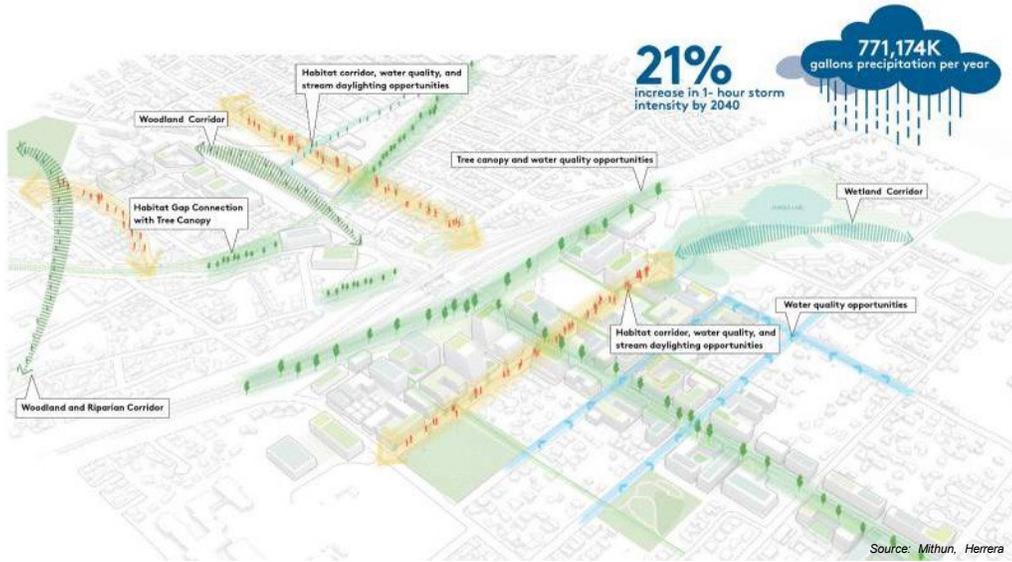
**INCENTIVE AMENITY**

- On-Site Public Open Space - Plaza
- On-Site Public Open Space - Pocket Park

**GREEN FACTOR**

- Small Trees
- Landscaped areas with soil depth of 24" or more
- Medium Shrubs or perennials
- Landscaped areas planted with native or drought tolerant plants
- Planting that provides food, forage and refuge for a diversity of species
- Innovation. Contributes to district sustainability goals including habitat connectivity, tree canopy, or stormwater goals beyond the site boundary.
- Bioretention facilities and/or soil cells





**1. Building Design Orientation to the Street**

**Intent:** Ensure that buildings contribute to the liveliness of the Station Area’s public spaces, and overall community character.

- The following design treatments should apply to areas where retail and active use frontages occur:
- Streets and public spaces should be enlivened by storefronts, windows, merchandise, and other activity. Along appropriate street types, buildings should be designed with frequent entrances to encourage multi-tenant occupancy and walk-in traffic.
- Entrances: Principal building entry should be visible from internal or external streets and public space. Entries should be marked by large entry doors and/or canopy/portico/overhang.

- Transparency: To help provide a visual connection between activities, ground floor facades should provide high levels of transparency.
- Weather Protection : Where required, pedestrians weather protection should:
  - be constructed of durable materials
  - vary in design and respond to architecture of the building
  - have continuity, minimizing gaps



## 2 Massing/Articulation

**Intent:** Create a variety of form and massing through articulation and use of materials to maintain a pedestrian scale.

- Break down the scale and massing of larger buildings into smaller and varied volumes. This should occur through a combination of vertical and horizontal modulation and/or articulation at appropriate intervals. Modulation should be paired with changes in building materials and colors to strengthen these massing moves. Avoid excessive changes in modulation, material choice, or color that distracts from the larger architectural concept.
- Design departures and minor variations from Maximum Façade Widths specified in the Zoning Code are appropriate where different massing strategies are used to provide similar or superior visual relief at the ground level to create a comfortable pedestrian scale and appropriate modulation is expressed in upper levels of the façade.
- All building faces should be responsive to the context of the surrounding environment and neighboring buildings. Utilize elements from neighboring buildings to establish a datum that can inform changes in material, modulation, articulation, or other changes in mass or façade.
- Design all sides of the building with care.
- Buildings should distinguish a strong “base” using articulation and materials that connect it to the ground plane. The base should include regulating lines and rhythms to create a pedestrian-scaled environment appropriate for its street frontage. Design departures and minor variations from Maximum Street Level Façade Widths may be appropriate where alternative design solutions result in an improved building base design solution.
- Provide clear pattern of building openings. Windows, balconies, and bays should unify a building’s street wall and add considerably to a façade’s three-dimensional quality.
- Ribbon windows and extensive use of mirrored glass are discouraged.



- Employ major architectural expressions into the facade, roof form, massing, and orientation, such as tower forms, oversized windows, and entrances to demarcate gateways and intersections. Strong corner massing can function as a visual anchor at key locations within the District.
- Building modulation and articulation should be employed to break up long facades and create a visual interest unique to each building. The type of modulation should be determined by the overall design concept for each building, using dimensions from window sizes, column spacing, rain screen paneling, etc. to determine a distinct design solution.
- Facades that are stepped back should be distinguished by a change in elements such as window design, railings, trellises, details, materials, and/or color so that the result is a richly organized combination of features that face the street
- Roof Silhouettes: Express roofs in varied ways. Consider potential views of roof tops from adjacent buildings. Avoid monotonous design.
- Locate and/or screen rooftop equipment so that it is not visible from public spaces. Integrate rooftop screening into building's form.
- Sustainability features such as solar panels, wind power, and fresh air ventilation shafts should be thoughtfully placed and integrated into the building design, but these features are to be encouraged and the function should not be diminished by these guidelines.
- Green building certification programs, such as Passive House, that strive for ultra-high energy efficiency may require efficient building envelope configurations that can challenge the desired vertical and horizontal building modulation measures prescribed in these Guidelines. The Design Review Board should work collaboratively with applicants consider the project's desired sustainability certification outcomes and ensure that these guidelines do not preclude certification.



### 3. Parking Garages

**Intent:** Mitigate the intrusive qualities of parking garages in pedestrian areas.

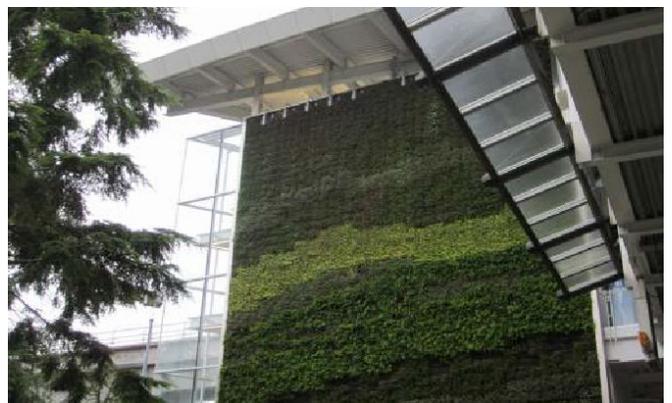
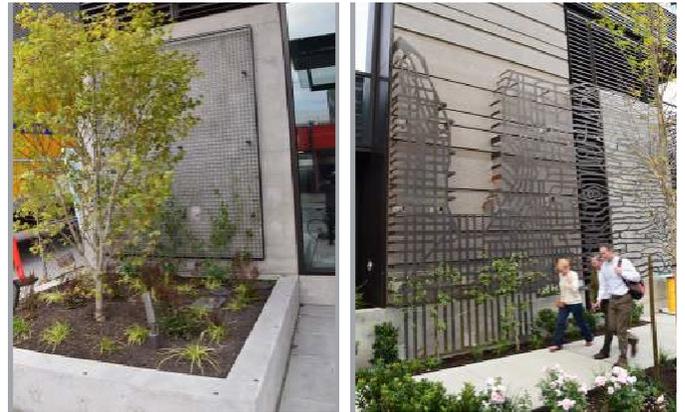
- Visible parking structures should generally be located away from public sidewalks and through block pathways. Where this cannot occur, design strategies such as intervening ground-level retail uses, dense landscaping, comfortable pedestrian spaces, and/or attractive facade treatments should be required.
- Design and site parking garage entries to complement, not subordinate the pedestrian entry. If possible, locate the parking entry away from the primary street, to either the side or rear of the building.
- Utilize similar architectural forms, materials, and/or details to integrate the garage with the development.



### 4. Blank Wall Treatments

**Intent:** Reduce the visual impact of blank walls by providing visual interest.

- Although blank walls are generally not encouraged along public streets and pedestrian spaces, there may be a few occasions in which they are necessary for functional purposes. Any blank walls longer than 20 feet should incorporate two or more of the following to provide visual interest:
  - vegetation, such as trees, shrubs, ground cover and or vines adjacent to the wall surface
  - artwork, such as bas-relief sculpture, murals, or trellis structures
  - seating area with special paving and planting
  - architectural detailing, reveals, contrasting materials, or other special visual interest



### 5. Encourage High-Quality Design

**Intent:** Ensure that all buildings in the Station Area are constructed as a quality addition to the Kirkland Community.

- Exterior architectural design and building materials should exhibit permanence and quality appropriate to Kirkland’s urban setting.



## 9 | Green Innovation District

**Intent:** Respond to the important location and significant development opportunities of this District by establishing critical connections and innovative design solutions that will catalyze development throughout the Station Area.

### Site Planning

- Site features, landscape elements, and architectural statements should welcome pedestrians and bicyclists to the District from the Stride BRT Station and create obvious connections for them to get to their destinations within this District or elsewhere in the community.
- Include land forming techniques such as berming and large, dense plantings along the freeway to reduce visual, air quality, and noise impacts to adjoining development and the neighborhood as a whole.
- Establish a series of landscaped open spaces arranged along the pedestrian network in the district to create comfortable pedestrian spaces among the larger building forms in the District.
- Design for an engaging pedestrian experience along the street level floor of buildings to create

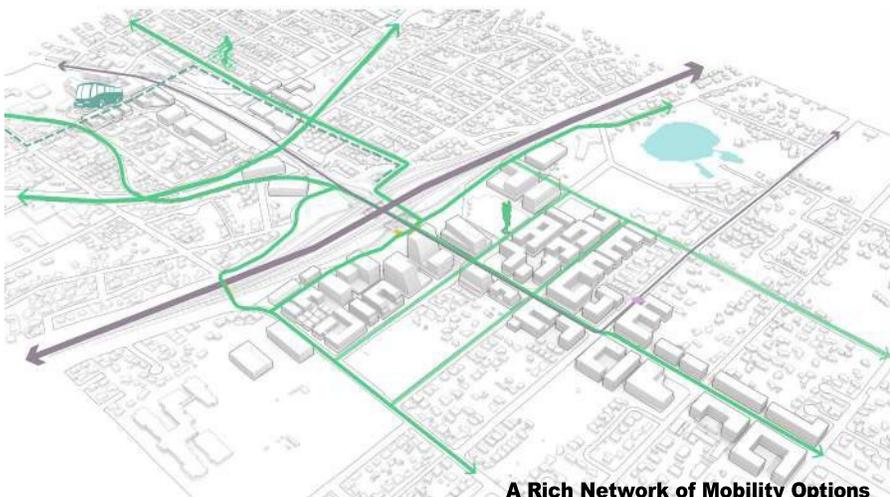
a seamless transition between the public realm (back of public sidewalk) and adjoining private development. This should take the form of variability in sidewalks widths, modulations of the building faces, and wider areas for pedestrian space and landscape areas.

- The corner at the NE 85th Street and 120th Avenue NE intersection should include a meaningful open space treatment to create a gathering space as well as a gateway to the District for the community. Corner building treatments should accentuate the space and help define the gateway.
- 120th Avenue NE is envisioned as a major pedestrian spine from NE 80th Street to NE 90th Street. Frontages should orient buildings, open spaces, driveways, and other site elements in such a way as to support the pedestrian activity intended for this street.
- Transitions from the Commercial Mixed Use District to the Neighborhood Mixed Use District should create opportunities for future shared open space and shared pathways to ensure long term cohesiveness for residents and employees of this District.



## Building Design

- The following design techniques should be incorporated into projects to minimize the dominance of large single occupant structures:
  - individual building footprints should be separated by open space, or include design strategies to create distinct buildings
  - multiple tenant spaces on the ground floor of structures abutting pedestrian or vehicular routes
  - stepping back of upper stories adjacent to areas with lower allowed heights
  - providing openness by limiting the floor area on upper stories, separating the individual buildings, and providing ample building modulation
- The following principles should ensure that buildings are distinct and respond to the unique location
  - within large, multi-building projects:
    - buildings should be designed to integrate with each other, while demonstrating architectural diversity. Buildings should be responsive to context of the surrounding environment and neighboring buildings
    - materials should be selected to integrate with each other and to help provide a richness of architectural diversity
    - windows should incorporate variation of patterning between buildings
  - In addition to complying with guidelines for parking garages, visible parking podiums should be integrated with the architecture of buildings above in terms of façade treatments and materials.



## 10 | Forbes Lake District

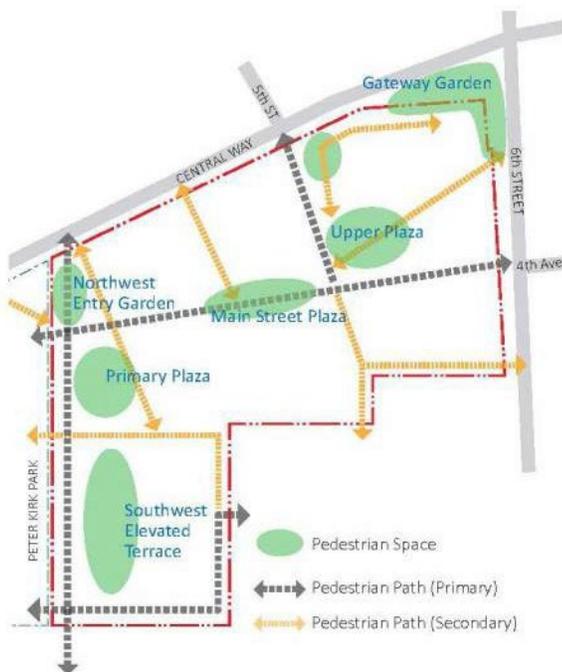
**Intent:** Establish 120th Ave NE as a pedestrian-oriented main street for the District and as a visual and ecological connection to Forbes Lake Park.

### Site Planning

- As with the Green Innovation District, this District should include site features, landscape elements, and architectural statements welcoming pedestrians and bicyclists to the District from the Stride BRT Station and create obvious connections for them to get to their destinations within this District or elsewhere in the community.
- Large development sites should be master-planned to provide coordinated development. The master plans should be pedestrian oriented and incorporate design standards such as:
  - buildings and retail storefronts oriented primarily to external frontages to activate the public realm. If there are internal pedestrian and vehicular routes within the site, orient buildings to engage with these internal routes through façade treatments, landscape design, and other elements to support a pedestrian-friendly environment. Internal routes should also connect

to surrounding streets with clearly identifiable building and pedestrian access points and entryways to adjacent streets and internal pedestrian pathways

- design techniques to prevent the dominance of large single occupant structures, such as use of smaller building footprints, multiple tenant spaces on each floor of a structure abutting a street, stepping back of upper stories along NE 85th Street and
- incorporate useable public spaces, plazas or pocket parks, and public amenities, such as art, sculptures, fountains, or benches
- use landscaping to emphasize entries into buildings, pedestrian areas, and pedestrian routes to enhance public spaces, parking areas, and to screen blank walls and service areas.
- placement of loading and service areas shall be located away from NE 85th Street and pedestrian areas
- The corners at the NE 85th Street/120th Avenue NE and at 120th Avenue NE/122nd Avenue intersections should include open space treatments to create



Conceptual Master Plan Example - Kirkland Urban

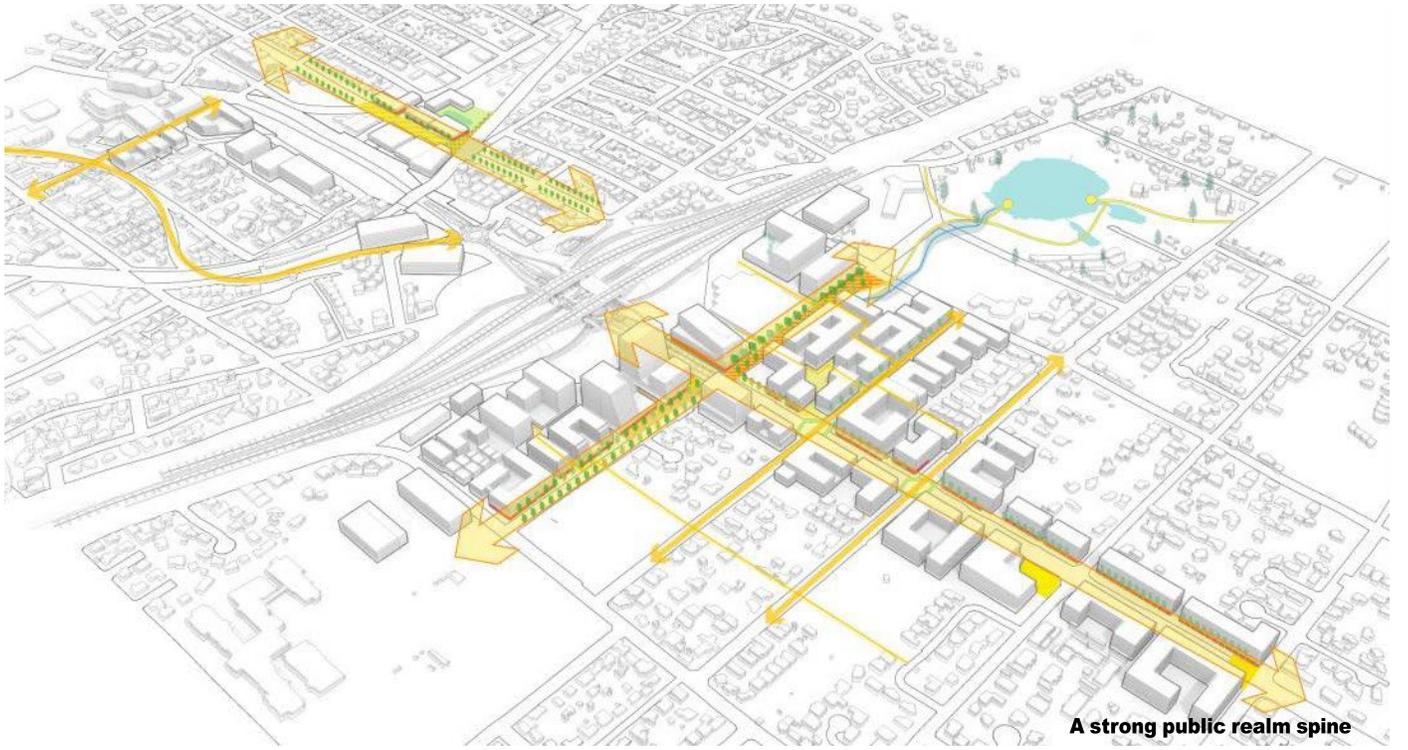


gathering spaces as well as gateways to the District for the community. Corner building treatments should accentuate the spaces and help define the gateway.

- Along 120th Avenue NE , buildings should present an active, transparent, continuous, and pedestrian oriented street edge. The street level floor of buildings should create a seamless transition between the public realm (back of public sidewalk) and adjoining private development. This should take the form of variability in sidewalks widths, modulations of the building faces, and wider areas for outdoor dining, pedestrian space, and landscape areas.
- Projects should include berming and large, dense plantings along the freeway to reduce visual, air quality, and noise impacts to adjoining development and the neighborhood as a whole.
- The north end of the District should include landscape and open space features that both transition to and leverage the opportunity created by Forbes Lake Park and future nonmotorized connections to the neighborhoods to the north.

### **Building Design**

- The design techniques prescribed for the Green Innovation District are also applicable in this District.



**A strong public realm spine**

## 11 | Rosehill Gateway District

Intent: Establish a gateway into the Station Area from point east while also creating a neighborhood hub defined by active streetscapes and pedestrian connections into adjoining neighborhoods.

### Site Planning

- The street corners along NE 85th Street within this District provide special opportunities for visual punctuation and an enhanced pedestrian environment. They should include the following considerations:
  - encourage design treatments that emphasize street corners through the use of building location and design, plaza spaces, landscaping, distinctive architectural features, and/or signage
  - incorporate storefronts directly at 124th, 126th, and 128th street corners to reinforce the desired pedestrian-oriented character of the District
  - Encourage special landscaping elements on all street corners in the District. Such landscaping elements should incorporate a variety of plant types and textures that add seasonal interest
  - encourage all buildings located at or near street corner to incorporate special architectural elements that add visual interest and provide a sense of human proportion and scale. This could include a raised roofline, turret, corner balconies, bay windows, special awning or canopy design, and/or distinctive use of building materials
- Minimize the number of curb cuts into a development, particularly off of NE 85th Street. To the extent possible, adjacent developments should share driveways.
- Develop an efficient internal vehicular access system that minimizes conflicts with pedestrians and NE 85th Street traffic flow.
- Configure development to provide interior vehicular connections to adjacent uses, where desirable. Where current connections to adjacent uses are not feasible, but desirable in the future, configure development to provide the opportunity for a future connection, should the adjacent site be redeveloped.
- Encourage the use of rose bushes in highly visible locations together with other plants to reinforce the identity of the Rose Hill neighborhood (low maintenance and drought tolerant varieties).



## 12 | Downtown Gateway District

**Intent:** Establish the urban design and nonmotorized transportation connectivity between Downtown Kirkland, the Stride BRT Station, and the Station Area Districts to the east of the freeway.

### Site Planning

- As with other quadrants of the Station Area that adjoin the Stride BRT Station, redevelopment to the southwest of the Station should include site features, landscape elements, and architectural statements welcoming pedestrians and bicyclists to the District from the Stride BRT Station and create obvious connections for them to get to their destinations within this District or elsewhere in the community.
- Redevelopment along the Cross Kirkland Corridor should provide a strong open space, pedestrian, and bicycle orientation toward the Corridor, including:
  - enhancement of designated public connections
  - avoiding blank facades and service areas
  - establishment of green open space opportunities

- The existing east-west pedestrian and bicycle network should be improved with multi use pathways. Where steep grades require stairs, bicycle runnels should be included to make it easier for those with bikes to make the transition.

### Building Design

- Buildings along NE 85th Street should explore opportunities to connect to sidewalk and bicycle improvements along the street to create an improved streetscape and access to the Stride BRT Station.



# The City of Kirkland

# Design Guidelines

## For Totem Lake Business District



The Bower Apartments



The Village at Totem Lake open space



Jade Residences



The Totem Lake Connector Bridge



Adopted by the Kirkland City Council pursuant to Kirkland Municipal Code Section 3.30.040, Ordinance 4678 September 1, 2020

Attest:



Penny Sweet,  
Mayor



Adam Weinstein,  
AICP, Director,  
Kirkland Department of



# Design Guidelines for Totem Lake Business District

## Table of Contents

<b>Introduction</b> .....	<b>1</b>
Urban Design Goals .....	1
Design Vision for Totem Lake Business District .....	1
Design Concept for the Totem Lake Business District .....	2
Vision for District TL 5 .....	3
Vision for District TL 6A .....	5
Vision for District TL 6B .....	5
Vision for TL 7 .....	7
Vision for TL 10A and TL 10B .....	7
Vision for TL 10D and TL 10E .....	8
Vision for Kingsgate Park and Ride TOD in PR 1.8 Zone .....	9
Vision for Large-Site Development .....	9
Vision for Landscaped Boulevard, the Totem Lake “Circulator” .....	10
<b>Design Guidelines</b> .....	<b>10</b>
<b>1. Entry Gateway Features</b> .....	<b>11</b>
<b>2. Street Trees</b> .....	<b>12</b>
<b>3. Street Corners</b> .....	<b>13</b>
<b>4. Pedestrian-Friendly Building Fronts</b> .....	<b>15</b>
<b>5. Building Location and Orientation</b> .....	<b>16</b>
<b>6. Sidewalk and Pathway Widths</b> .....	<b>17</b>
<b>7. Pedestrian Coverings</b> .....	<b>19</b>
<b>8. Blank Walls</b> .....	<b>20</b>
<b>9. Lighting</b> .....	<b>21</b>
<b>10. Pedestrian Amenities</b> .....	<b>22</b>
<b>11. Interior Pedestrian Connections</b> .....	<b>24</b>
<b>12. Pedestrian Plazas</b> .....	<b>26</b>
<b>13. Residential Open Space</b> .....	<b>27</b>
<b>14. Parking Lots and Vehicular Circulation</b> .....	<b>29</b>
<b>15. Parking Garages</b> .....	<b>30</b>
<b>16. Architectural Style</b> .....	<b>31</b>
<b>17. Architectural Scale</b> .....	<b>31</b>
<b>18. Human Scale</b> .....	<b>33</b>
<b>19. Building Details and Materials</b> .....	<b>34</b>
<b>20. Signs</b> .....	<b>35</b>
<b>21. Service Areas</b> .....	<b>36</b>
<b>22. Visual Quality of Landscapes</b> .....	<b>36</b>
<b>23. Territorial Views</b> .....	<b>37</b>

# Introduction

## Purpose of Design Guidelines

This document sets forth a series of Design Guidelines, adopted by Section 3.30 of the Kirkland Municipal Code that will be used by the City in the design modification process and the alternative compliance Design Board Review (DBR) process for development in the Totem Lake Business District. The Totem Lake Business District encompasses the Business District Core and the adjacent land within the district's boundaries. Design Guidelines governing development in the Business District Core are contained in the document titled, *Design Guidelines for Pedestrian-Oriented Business Districts*, Section 3.30.040 of the Municipal Code.

Other documents that should be referred to during design review are the Totem Lake Business District Plan goals and policies contained in the Comprehensive Plan, the Totem Lake (TL), Office Zones (PO and PR including PR 1.8 Transit Oriented Development Zone) Use Zone Charts found in the Kirkland Zoning Code, and the Totem Lake Urban Center Enhancement and Multimodal Transportation Network Plan, approved by the Kirkland City Council on May 15, 2018.

~~For Administrative Design Review (ADR), the Planning Official will use these guidelines when necessary to interpret the Design Regulations. They are also intended to assist project applicants and their architects by providing graphic examples of the intent of the City's guidelines and regulations.~~

~~The Design Guidelines do not set a particular style of architecture or design theme. They are intended to establish a greater sense of quality, unity, and conformance with Kirkland's physical assets and civic identity. These guidelines are not intended to slow or restrict development, but rather to add consistency and predictability to the permit review process.~~

## Use of the Guidelines for Design Modifications and Alternative Compliance

KZC Chapter 142 permits design modifications to specific zoning regulations. The Planning Official will use these guidelines to administer the design modification authority of KZC 142.

The Alternative Compliance process (outlined in KZC Chapter 142) is an optional path that allows applicants to propose substantial design modifications in place of the design regulations outlined in Chapter 92, or other zone-specific sections of the KZC. The Alternative Compliance Proposals will be reviewed by the Design Review Board (DRB) and will be reviewed for consistency with the objectives of the design guidelines.

## Urban Design Goals

Urban design goals and objectives for the desired future development of the area were adopted in 2015 as part of the Totem Lake Business District Plan. The Totem Lake Business District is comprised of distinct areas separated by major transportation corridors such as I-405, NE 124th Street. Urban design policies seek to establish visual and non-motorized connections between these areas, create effective transitions within and around the district, and provide a collective identity for Totem Lake.

### Totem Lake Business District Plan Urban Design Goals TL 21-TL-25

- **Goal TL-21:** Ensure that public and private development contributes to a coherent and attractive identity for the business district.
- **Goal TL-22:** Develop gateway features that strengthen the character and identity of the Business District.
- **Goal TL-23:** Develop a new landscaped boulevard, or "Circulator" that provides a green visual connection between the subareas of the business district through enhanced landscape and public amenities.

- **Goal TL-24:** Provide interconnected streetscape improvements throughout the business district that contribute to a sense of neighborhood identity and enhance visual quality.
- **Goal TL-25:** Provide effective transitions between the light industrial, commercial and higher density multifamily uses in the business district and single-family residential areas surrounding the district.

### **Design Vision for Totem Lake Business District**

The Totem Lake Business District will continue to evolve into an attractive urban center – as a dense, compact community, with a mix of business, commercial and residential uses and a high level of transit and pedestrian activity. Outside of the Business District Core, the Plan for the Totem Lake Business District envisions new connections between areas separated by built features such as I-405 and building design that promotes a sense of community identity and continuity throughout the district.

The rich mix of uses in Totem Lake is accompanied by enhanced mobility within the district. Efficient vehicular capacity occurs through infrastructure investment and an expanded bicycle and pedestrian network provides additional opportunities for pedestrian-oriented development and placemaking. Local transit connections, an extensive non-motorized network and a local boulevard system will all combine to complement and support the regional system.

The Plan envisions an attractive and economically strong district in Totem Lake. It acknowledges the challenges to the creation of a single community identity posed by the area's natural and built elements that split the district into four fairly distinct quadrants. Totem Lake is the City's only neighborhood bisected by Interstate-405. Nevertheless, the use of design measures that address important elements of design will move the district forward into a cohesive and coherent community. Key design issues to be addressed include human and architectural scale, breaking up of building mass, attention to building detail and appropriate building orientation. The identity and appeal of Totem Lake will be strengthened through the establishment of continuous and interconnected walking and biking networks; improvements in the public realm, including publicly accessible spaces along the frontage of new development and in public gathering spaces within the right of way; gateway enhancements; public art; and streetscapes with coordinated street lights, sidewalk design, landscaping and street furniture. Implementation of the Circulator street concept to connect subareas of the district, combined with the reduction of block sizes achieved through new streets oriented to local traffic and new through block connections, will improve circulation and simplify wayfinding for visitors.

Several areas within the district present unique opportunities for development. The Planning Concept Map (Figure 1) illustrates where these opportunities exist. Further discussion in this section presents the desired vision for each of these areas, as well as for the landscaped boulevard or "Circulator", that should tie the district together.

# Design Concept for the Totem Lake Business District

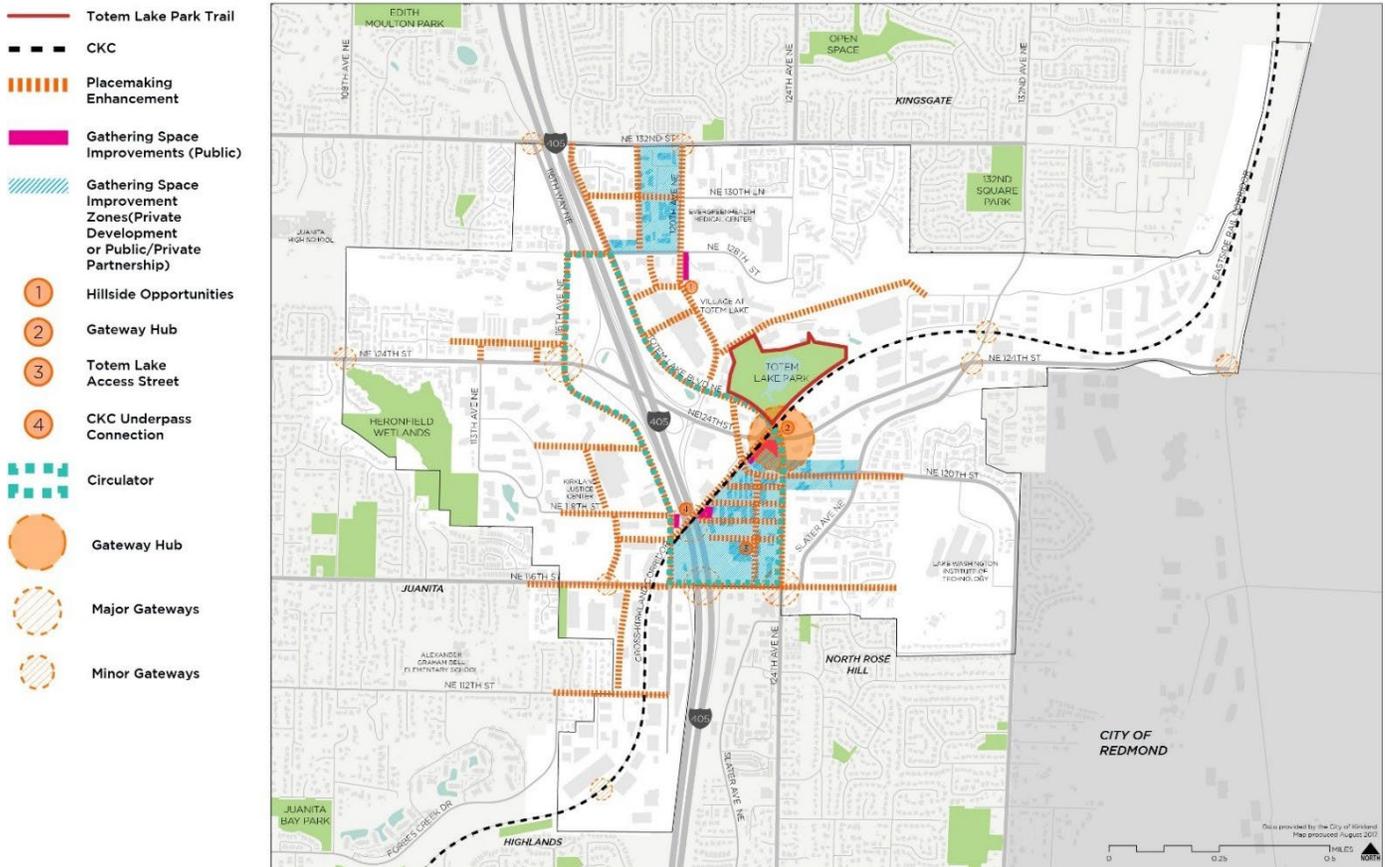


Figure 1. Design Concept for the Totem Lake Business District.

## **Vision for District TL 5**

The Totem Lake Plan envisions the TL5 zone as a lively mixed-use district. The potential for land assembly in the district, as well as its location adjacent to the freeway and Cross Kirkland Corridor (CKC) create an opportunity for substantial redevelopment. The western portion of the site is situated at an elevation somewhat lower than the freeway, enabling greater building heights with minimal impacts on surrounding development. The district's frontage along the CKC brings recreation and commuter users to the area, opportunities for connectivity to areas to the west, and provides opportunities for public gathering spaces.

The Plan envisions the expansion of the network of local access roads within the district and designates through-block pathways connecting the new north/south street to the east and west. Vehicular, pedestrian and bicycle access to properties within and beyond the district would be improved, and reliance on major arterial routes would be reduced.

This network would be the foundation for an attractive grid of streets, wide sidewalks, and a supporting combination of commercial, office, and residential uses. The focal point of the village will be the spine of 123<sup>rd</sup> Avenue NE – extending from NE 116<sup>th</sup> Street over the CKC to NE 124<sup>th</sup> Street. Much of the road would resemble a “main street” with its storefronts, street trees, wide sidewalks, and on-street parking. 124<sup>th</sup> Avenue NE would be upgraded with wide sidewalks, protected bicycle facilities, landscaping and wayfinding elements. Building frontages along 124<sup>th</sup> Avenue NE will be important, but the street will also be defined by landscaping, lighting and wider sidewalks. Driveways would be consolidated and coordinated with the internal street grid and properties on the east side of 124<sup>th</sup> Avenue NE. Storefronts would be clustered around major entry points to the development providing a welcoming entry. Also, building design and landscaping at the southeast corner of the village are important, as they will function as a major gateway to the village.

Parking would be provided in strategically located surface parking lots and within structures above, below, or behind commercial uses. Parking areas located adjacent to surrounding arterials would feature landscaping and other design features to maintain visual continuity along the street. Parking structures would either contain commercial uses at ground level or a combination of landscaping and architectural elements enhance the pedestrian environment.

Taller residential or office buildings in the area bordering I-405 would create a striking skyline for the village. While the buildings may stand out from other village structures, they would be configured in a way that complements the village. Easy pedestrian connections, landscaping, and common streetscape features link the structures to the village's diversity of commercial uses and amenities. As in other mixed-use areas within the business district, developments will have publicly accessible spaces at their primary frontage, which contribute to the character and pedestrian-oriented quality of the area. These public spaces will be enhanced through including public art, water features, and distinctive landscaping that will lend a civic quality to the spaces and create opportunities for both passive and interactive elements.

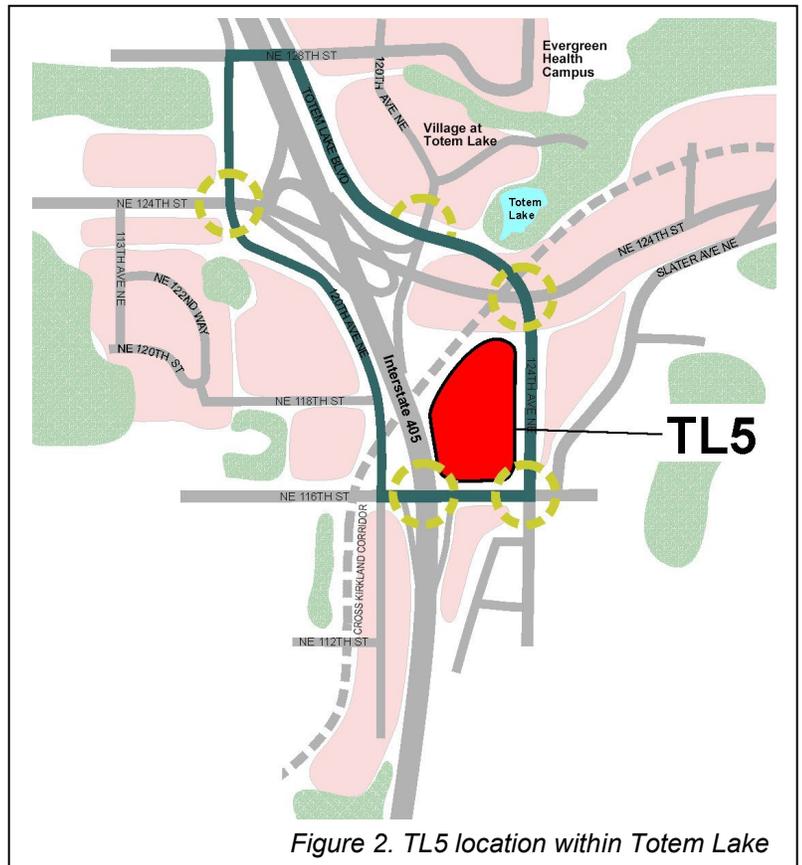


Figure 2. TL5 location within Totem Lake

Figure 3. Design concept for TL5.

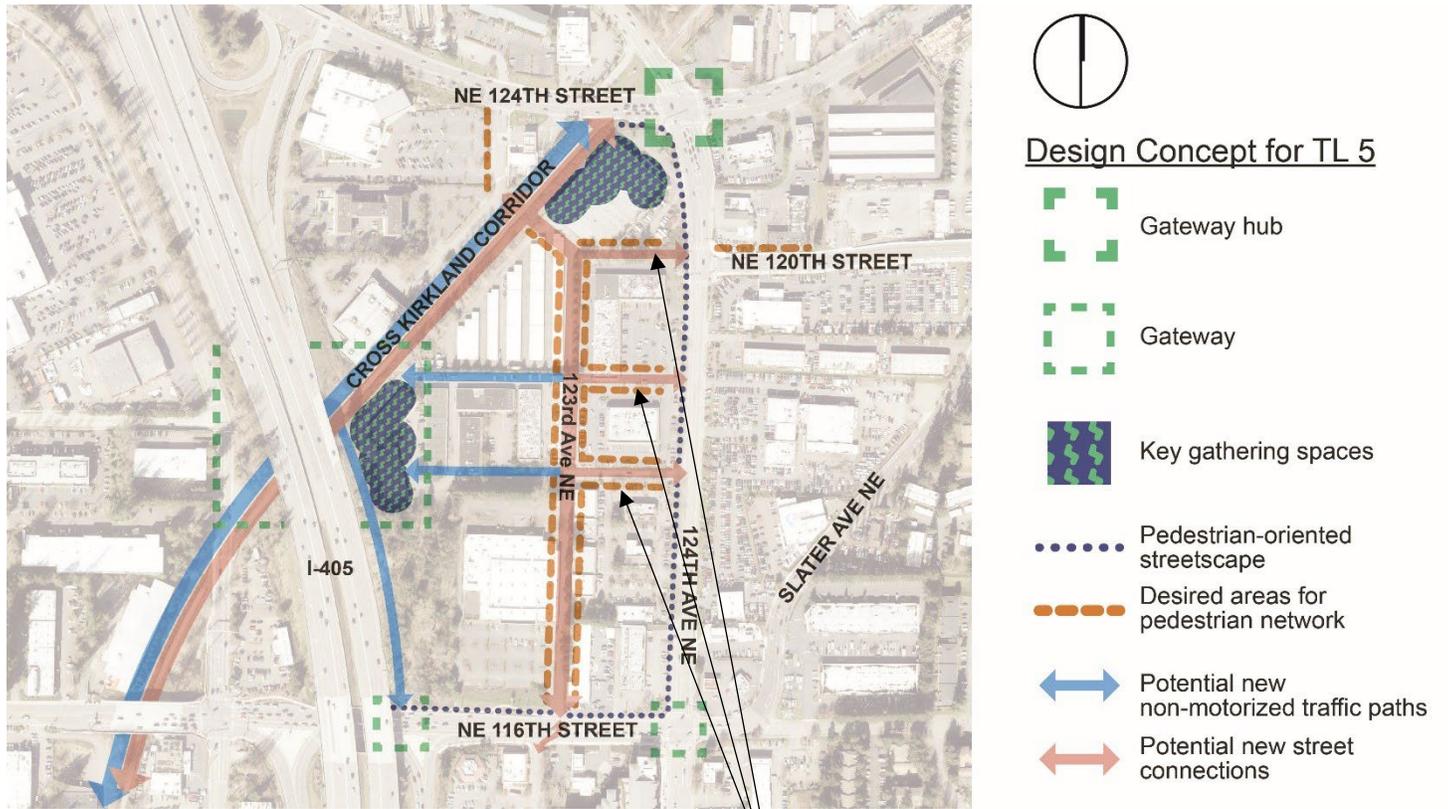
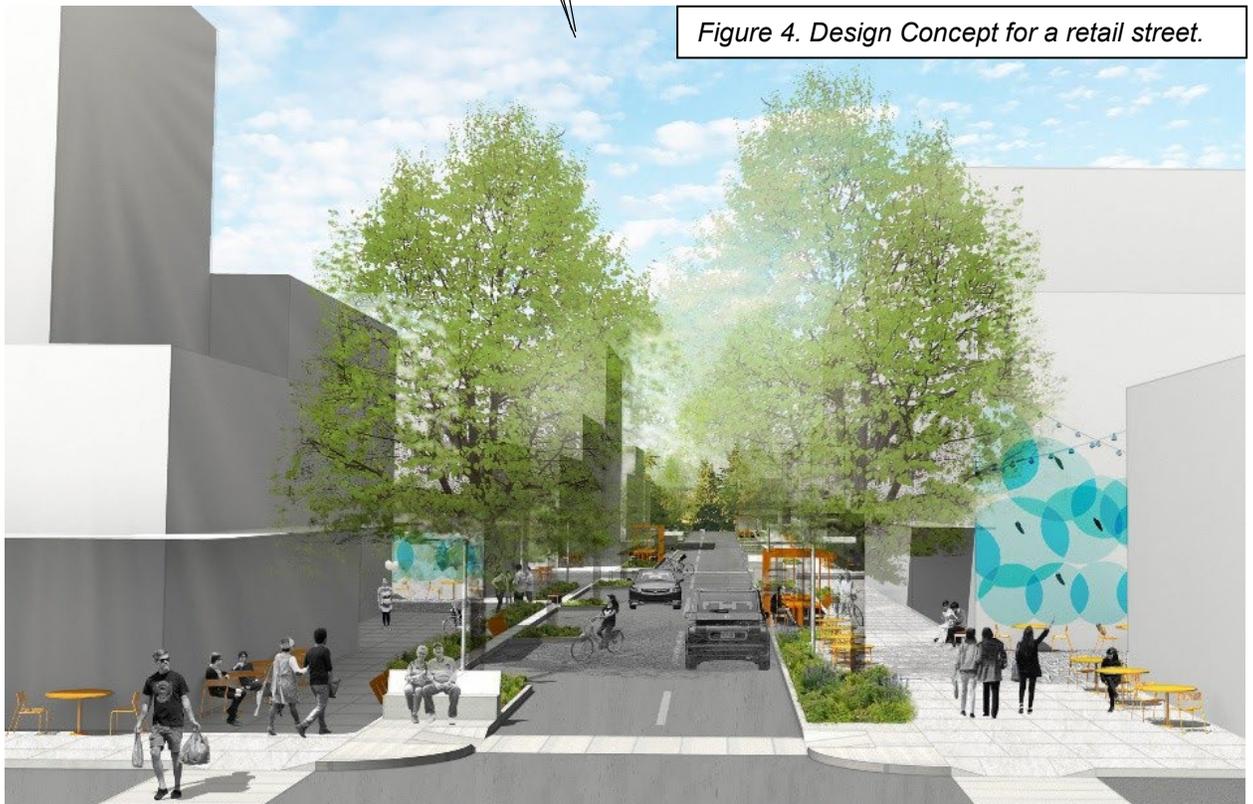


Figure 4. Design Concept for a retail street.



### **Vision for District TL 6A**

The TL6A district, located at the eastern edge of the Totem Lake Business District, would feature an attractive mix of commercial uses along 124<sup>th</sup> Avenue NE and NE 124<sup>th</sup> Street, developed with terraced multifamily or office uses on uphill sites towards Slater Avenue NE. NE 120<sup>th</sup> Street provides a convenient east-west connection, linking the Lake Washington Institute of Technology and residential neighborhood to the east to the core of the business district. Developments along NE 120<sup>th</sup> Street will provide publicly accessible spaces along their primary frontage, enhancing the streetscape for pedestrians. 124<sup>th</sup> Avenue NE will be significantly upgraded, with wide sidewalks, protected bicycle facilities, landscaping and wayfinding elements.

Auto dealers may remain clustered along both 124<sup>th</sup> Avenue NE and NE 124<sup>th</sup> Street. Adjacent to the dealerships, attractive landscaping strips along the sidewalks with seasonal plantings and low-level signage will be provided. While some surface parking areas may remain on the lots, many of the dealers will incorporate some structured parking to accommodate their vehicular stock. Other sites along 124<sup>th</sup> Avenue NE and NE 124<sup>th</sup> Street will retain a mix of commercial uses.

In areas where significant elevation change exists from the east to west, Individual buildings will be able to stairstep down the hillside, following the natural earth form and creating a dramatic visual setting. The topography also allows parking areas to be hidden under buildings. Buildings can be designed to cluster around courtyards and useable open spaces. A system of pathways will connect buildings within the district to the surrounding streets and to adjacent properties in some areas.

### **Vision for District TL 6B**

Located in the northwest quadrant of the District, TL 6B can become a key mixed-use village in the Totem Lake Business District. The zone will contain an attractive grid of through-block pathways with wide sidewalks and storefronts.

The village will have one or more gathering spaces surrounded by commercial uses with residential and/or office uses on upper floors. Residential uses will be clustered at the north end of the site to take advantage of the greenbelt setting. A loop trail will be developed around this greenbelt, providing a tremendous amenity for the area.

The surrounding arterials (NE 124<sup>th</sup> St and 116<sup>th</sup> Ave.NE) will be upgraded with wider sidewalks and bicycle facilities, new landscaping and lighting, and landscaped medians. Existing landscaping along NE 124<sup>th</sup> Street should be retained and enhanced. The connected system of internal streets and pathways will allow the development to focus most vehicular traffic to one major entry point

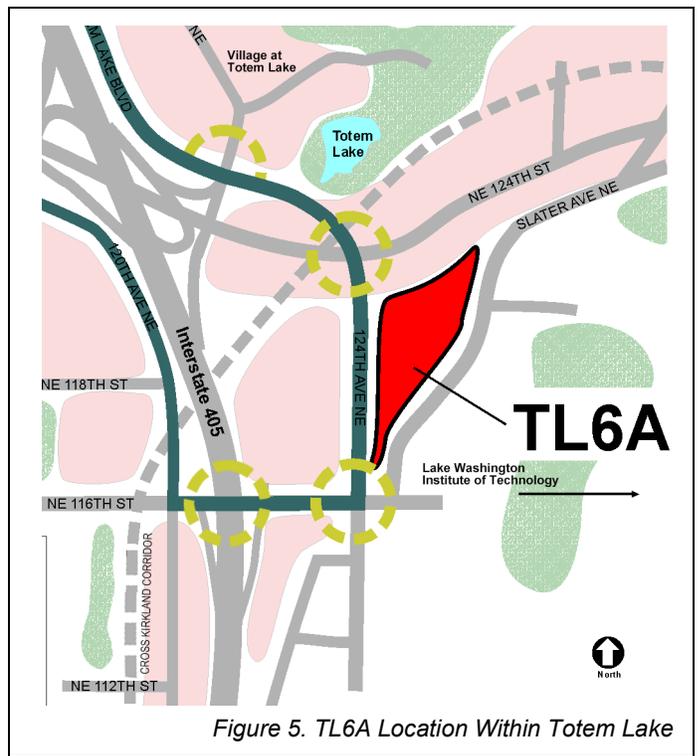


Figure 5. TL6A Location Within Totem Lake

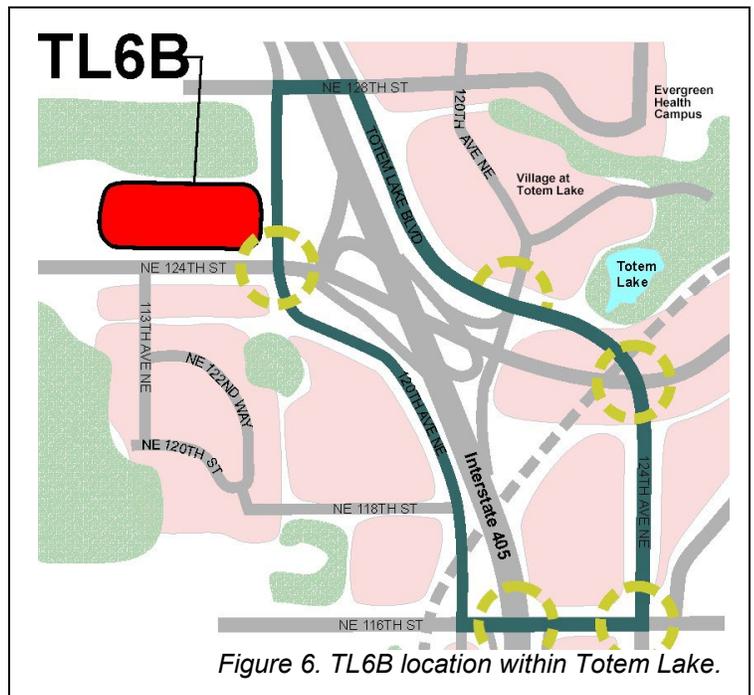


Figure 6. TL6B location within Totem Lake.

off of each arterial. The pedestrian environment will be substantially upgraded through the consolidation of vehicle access points, and the orientation of buildings to sidewalks and pathways. While many of the large, older street trees will have been retained along NE 124th Street, a colorful mix of low maintenance plantings will be added to upgrade the visual character and identity of the corridor. Gateway signage and special landscaping at the NE 124th St and 116th Ave NE intersection will announce the entry into the village.

Parking will be provided in strategically located parking lots and within structures above, below, or behind commercial uses. Parking areas located along the perimeter of the district will provide landscaping and other design features to maintain visual continuity along the street. Parking structures will contain either commercial uses at ground level or a combination of landscaping and architectural elements to enhance the pedestrian environment.

Village buildings will use a variety of materials and colors and modulated walls and rooflines to reduce their architectural scale. Ground-floor commercial uses will contain attractive details that provide interest at a pedestrian scale. Residential uses will provide prominent building entries, be served by pedestrian connections to shops and/or commercial uses within the development and to nearby streets and be designed to take advantage of the natural area to the north as an amenity for residents.

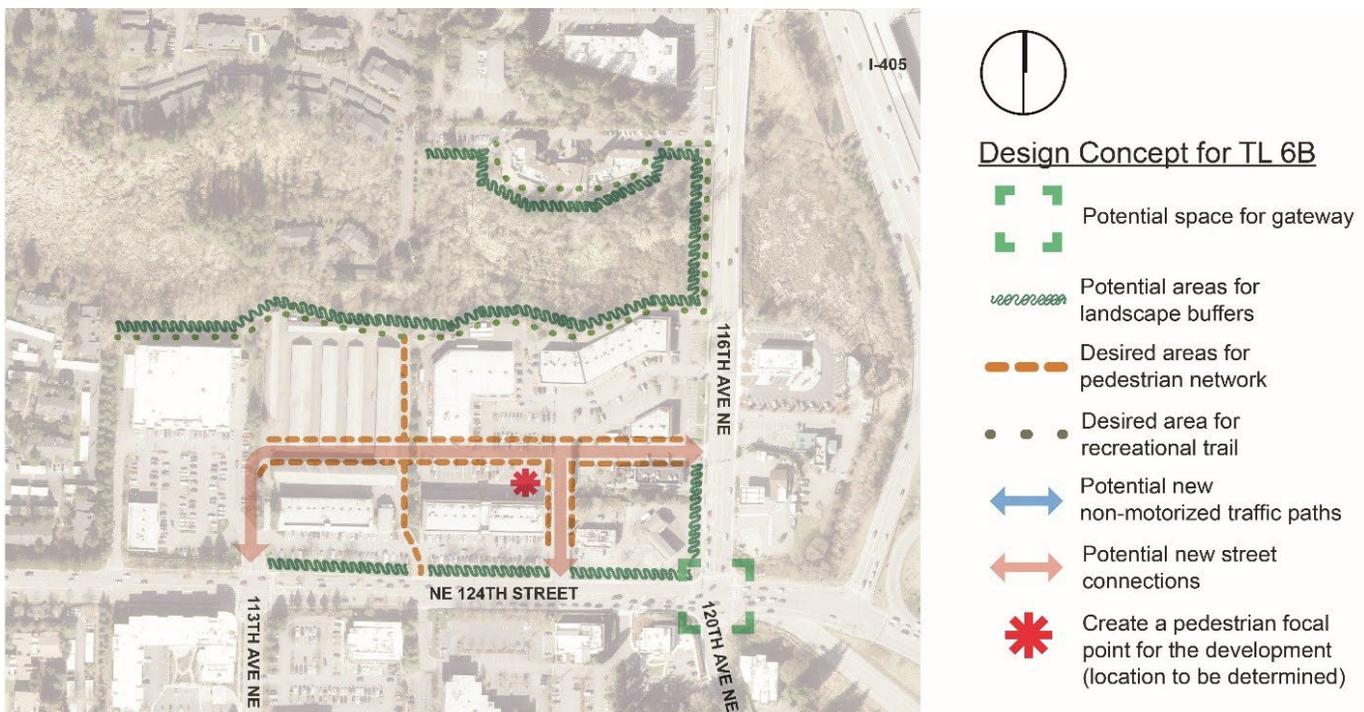


Figure 7. TL 6B Village Design Concept.

### **Vision for TL 7**

Located at the eastern edge of the Gateway Hub, just southeast of the lake itself, the TL 7A subarea lies between the NE 124th Street arterial and the CKC. The Totem Lake gateway hub includes the westernmost tip of the zone. Site design in this area responds to its prominence at the north end of the CKC Connector overpass as a major route for bicycle and pedestrian commuters and recreational users and development includes pedestrian connections from NE 124th Street to the CKC.

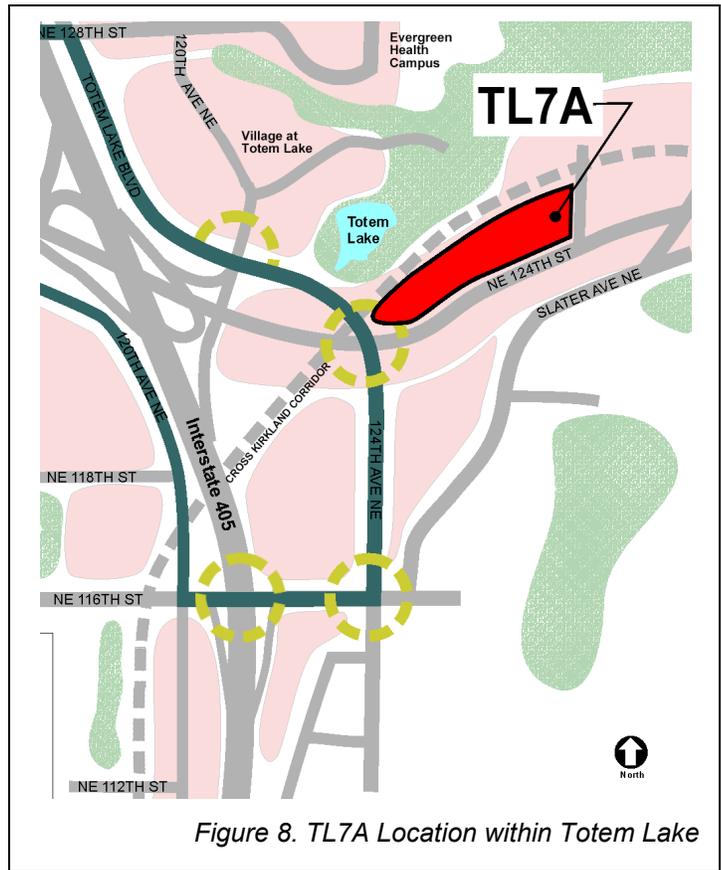


Figure 8. TL7A Location within Totem Lake

### **Vision for TL 10A and TL 10B**

The I-405 Corporate Center in TL 10A is a business park that serves as a model of coordinated efforts in signage and building design for the areas in transition to the south. New development in the area will continue to complement existing structures.

District TL 10B to the south provides the link between the established Corporate Center and the evolving office park area in TL 10D and TL 10E to the south. Development in this partially wooded area provides a mix of housing and office uses. The topography and vegetation in the area enable taller residential buildings to be well situated to avoid impacts to the residential areas to the west, while providing a significant housing resource for the business district and the city.

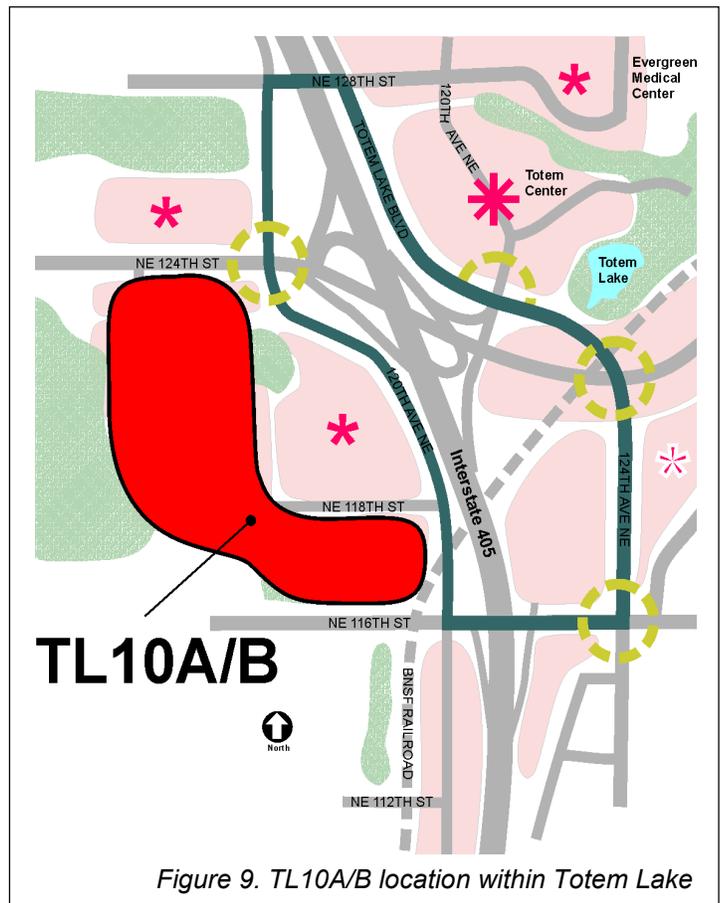


Figure 9. TL10A/B location within Totem Lake

## **Vision for TL 10D and TL 10E**

Visibility and proximity to I-405, as well as the land's elevation below the freeway to the east and the residential areas to the west, provide for substantial redevelopment opportunities throughout these districts. As uses in the area convert from manufacturing and light industrial to office, high-tech and residential (within the western perimeter of TL 10D), development in the area can begin to create a more cohesive and distinct visual image. Supportive service and retail uses will add to the area's appeal for workers and residents and reduce the need for travel outside the district.

Taller buildings can be accommodated here with minimal visual impacts to territorial views from the freeway. Consideration of elements that produce distinctive roof forms and minimize mass at upper levels will contribute to a skyline that is visually interesting.

A gateway to the Totem Lake Business District is located at the south end of the area, along the CKC. The CKC

runs through the area providing opportunities for non-motorized transportation and public open space for employees and residents. Building design along the corridor should be sensitive to and benefit from the corridor's use as a transportation corridor for commuters and recreational users. Design should acknowledge the high visibility of buildings in this area and incorporate measures to address parking garages, blank walls and pedestrian access between the CKC and adjacent structures.

Common streetscape elements aimed at the creation of a pleasant pedestrian-oriented environment will be very important in this area. An enhanced grid of major pedestrian sidewalks will contribute to the pedestrian experience throughout the district.

A successful residential community within the western portion of the area (TL 10D) will provide close-in housing for Totem Lake employees and add evening activity to the district. The CKC and attractive walkways to connect residents within TL 10D to points east and north will be important to ensure the success of the mix of uses throughout the area.

Support for shared and coordinated signage throughout the district will help to minimize visual clutter and contribute to the visual identity of the area.

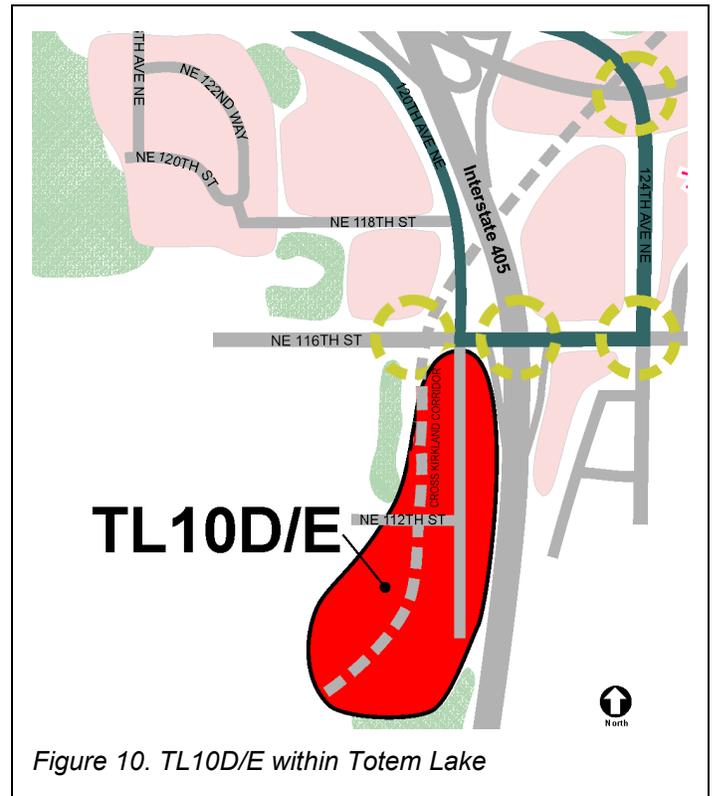


Figure 10. TL10D/E within Totem Lake

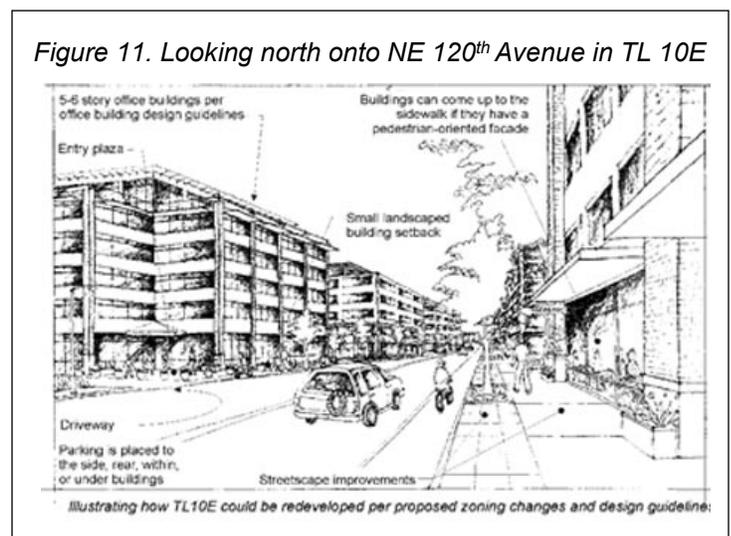


Figure 11. Looking north onto NE 120<sup>th</sup> Avenue in TL 10E

### **Vision for Kingsgate Park and Ride TOD in PR 1.8 Zone**

The Kingsgate Park and Ride is envisioned to transform from a surface parking lot into a multi-story, transit-oriented development (TOD).

The location is ideal for a TOD. Sound Transit, WSDOT, King County Metro and the City of Kirkland are making significant investments in mobility improvements including the inline Bus Rapid Transit (BRT) station on I-405, new freeway ramps at I-405/NE 132nd Street, and two roundabouts at the intersection of NE 132nd Street/116th Way NE. These facilities will provide easy vehicle and transit access to the TOD and a new bike lane on 116th Way NE. The TOD is within walking distance to these transit facilities including the Totem Lake Transit center, to employment, Evergreen Medical Center, and to shops and services at the Village at Totem Lake.

The TOD redevelopment should occur within the context of an approved master plan for the entire subject property that integrates a new residential community with an expanded transit hub. On the south portion of the property will be a stand-alone public parking garage(s) to increase the number of parking stalls for park and ride transit users. The remainder of the site to the north will be developed as a transit-oriented development (TOD) residential community with affordable

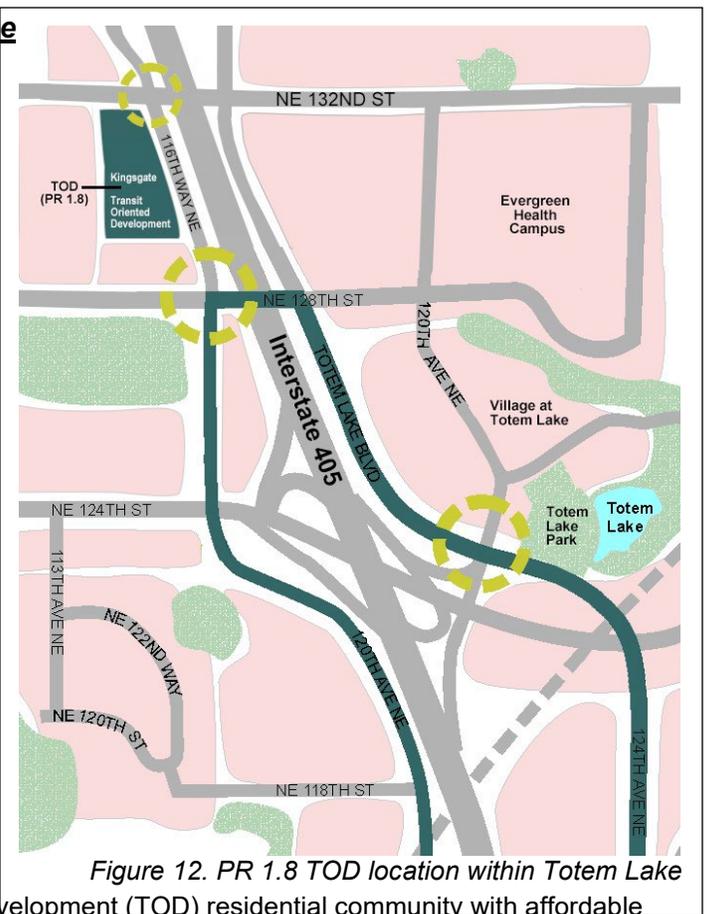


Figure 12. PR 1.8 TOD location within Totem Lake

and market rate housing and opportunities for commercial uses to support transit users and residents. Transit stops in the site could relocate from the current on-site park and ride lot to the curbside of 116th Way NE.

Key design objectives for the master plan include creating an attractive site and building complex where the public transit garages, transit facilities and TOD buildings relate to each other on the site, in context with the surrounding streets and high visibility from the freeway. Preserving and enhancing the existing mature tree lined buffers along the south and west property lines will help mitigate the visual impacts of parking garages and TOD from the adjacent residential uses.

Shared internal vehicular and pedestrian circulation with pedestrian linkages to adjacent streets, transit facilities, building entrances, and within parking areas will be important functions of the site. The gateway designated at the northeast corner of the site should be designed to provide an attractive, welcoming entrance to the Totem Lake Business District. The site should incorporate attractive open space and plazas for residents and transit users.

Providing the appropriate building mass and scale are important in three areas of the site. At the gateway corner, how buildings are oriented, setback from, and visible from the intersection of NE 132nd Street and 116th Way NE, avoiding long, unbroken facades along 116th Way NE by using techniques to break up mass of larger buildings to provide the perception of smaller buildings and design treatments along the west side of buildings adjacent to the residential area to the west.

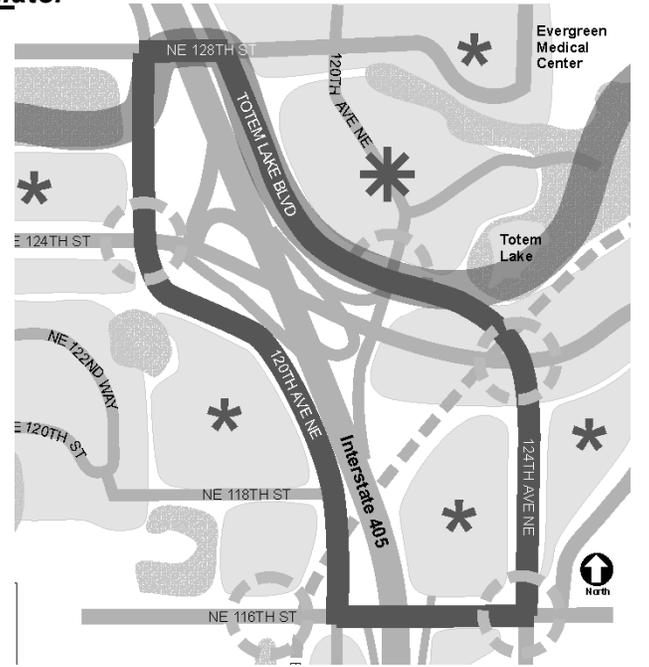
### **Vision for Large-Site Development**

Larger sites within the Totem Lake Business District present opportunities for master planning to provide coordinated development. Within TL 4B for example, a vibrant mixed-use center could be created, combining retail, office and residential uses. While parcels in this area and others in the business district can provide an attractive face along the major traffic corridors including the Totem Lake Circulator, many are large enough to provide their own interior vehicular and pedestrian pathways, as well as gathering areas for pedestrians. These may include a plaza area surrounded by shops, or wide sidewalk areas along an interior access street.

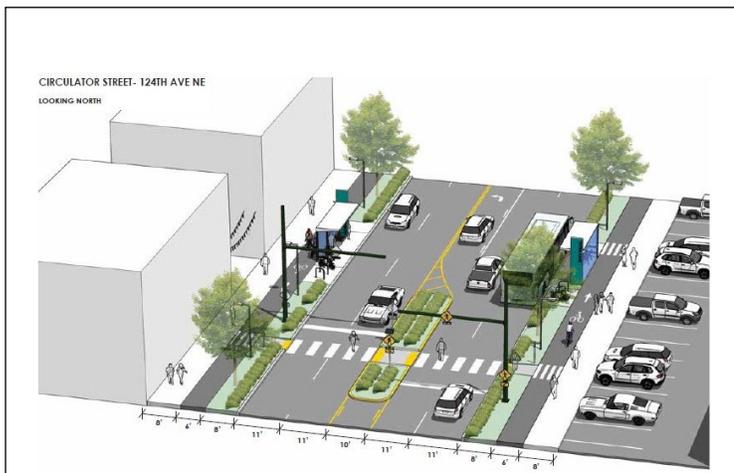
## ***Vision for Landscaped Boulevard, the Totem Lake “Circulator”***

The Totem Lake Business District Plan envisions the creation of a landscaped boulevard that links the four quadrants of the business district with a recognizable character. Improvements including landscaping and public amenities will be provided by both public and private development to ensure a cohesive streetscape experience. The boulevard will provide a hospitable environment for pedestrians, bicyclists and drivers through reducing scale, providing shade and seasonal interest and reducing noise levels. Improvements may include widened and meandering planting areas, continuous and clustered tree plantings and shrubbery, and plantings varying in seasonal color, texture and shape. Other elements, such as lighting, directional signs, benches, varying pavement texture, bike racks, transit shelters, interactive elements, public art and water features will further enhance the route and experience.

The boulevard will not only visually connect the district’s separate areas but will also help local circulation. In most areas, existing rights-of-way can be used to create the boulevard. In others, dedication may be necessary to provide the necessary improvements and amenities.



*Figure 13. Location of Totem Lake Circulator*



*Figure 15. Totem Lake Circulator Cross Section from the Totem Lake Urban Center Enhancement and Multimodal Transportation Network Plan.*



*Figure 14. Colorful streetscape plantings can strengthen the character and identity of the Totem Lake area.*

# Design Guidelines

The following design guidelines for the Totem Lake Business District (TLBD), outside of the Business District Core (BDC), are intended to help guide the future development of the district toward the future vision described in the Totem Lake Business District Plan and elsewhere in this document. These guidelines include both district-wide measures and unique measures specific to individual districts or sites within Totem Lake.

Improvements to streets, parks and the development of new public facilities will create a dynamic setting for civic activities and private development.

## 1. Entry Gateway Features

The Comprehensive Plan calls for gateway features at the key entry points into neighborhoods and business districts.

### **Objectives Intent**

- To enhance the character and identity of the Totem Lake Business District.

### **Guideline**

Incorporate entry gateway features in new development in the vicinity of gateways/nodal intersections identified in the Concept Map (Figure 1). Gateway features should incorporate design elements associated with or desired in the district,

depending on available space. Gateway features should include some or all of the following:

- Distinctive landscaping, including suggested common landscaping elements from the City's Urban Forester.
- Artwork (e.g. vertical sculpture incorporating historical information about Totem Lake).
- A gateway sign with the City logo.
- Multicolored masonry forming a base for an entry sign.
- Decorative lighting elements.
- Elements identified to be provided at gateways to support wayfinding in the business district including the Totem Lake icon and other design elements described in the Totem Lake Urban Center Enhancement and Multimodal Transportation Network Plan.

### **Special Consideration for TOD in PR 1.8 zone**

In addition to the above guidelines, the gateway design at the northeast corner of the site at NE 132nd Street/116th Way NE intersection should provide:

- Hardscape and vegetation materials to create colorful and attractive open spaces.
- Wayfinding signage directing visitors to locations in the Totem Lake Business District.
- Modulation and building forms that emphasize the transition from residential neighborhoods to the north to the Business District. Design techniques should be used to decrease building mass at the corner to reduce overpowering pedestrians at street level, the closeness of residential development to the intersection and visibility of buildings from the freeway.
- Change in materials, colors, and building forms.



*Figure 16. A desirable entry gateway feature*

## 2. Street Trees

### Objectives Intent

- To upgrade the character and identity of the Totem Lake Business District.
- To enhance the pedestrian environment on the Totem Lake Business District.
- To use trees that provide seasonal interest.
- To use trees appropriate to the urban environment of the Totem Lake Business District.

### Discussion

The repetition of trees bordering streets, internal roadways, and pathways can unify a community's landscape. Trees can add color, texture, and form to the urban environment. A strong street tree planting scheme can establish community identity and provide a respite from the weather and the built environment.

### Guidelines

- a. Incorporate street trees along all streets, internal access roads, and pathways.
- b. Encourage developments to use street trees as a unifying feature of the development.
- c. Select and maintain tree species that will accommodate pedestrian and vehicular traffic, and maintain visibility into and through sites for safety purposes.

### **Special Consideration for TOD in PR 1.8 zone:**

- a. Along 116<sup>th</sup> Way NE, add large tree species and evergreens to buffer residential uses from the major intersection and freeway.

## 3. Street Corners

### Objectives Intent

- To enhance the appearance of highly visible locations.
- To upgrade the character and identity of the Totem Lake Business District and its individual districts.
- To enhance pedestrian access and safety.

### Discussion

Street corners, especially along arterial corridors, provide special opportunities for visual punctuation and an enhanced pedestrian environment. Buildings on corner sites that incorporate architectural design elements create visual interest for the pedestrian and provide a sense of human proportion and scale.

### Guidelines

- a. Encourage design treatments that emphasize street corners through the use of building location and design, plaza



*Figure 17. Provide street trees along all streets and internal access roads*



*Figure 18. This building uses a cropped corner with entry and decorative roofline, building materials, and details to provide visual interest*

spaces, landscaping, distinctive architectural features, and/or signage. Street corners can be an excellent location for plazas, particularly where adjacent storefronts and building entries are provided. In auto-oriented areas, landscaping elements on street corners can enhance the character of the area and visual relief from pavement areas. Such landscaping elements should incorporate a variety of plant types and textures that add seasonal interest.

- b. Encourage all buildings located at or near street corner to incorporate special architectural elements that add visual interest and provide a sense of human proportion and scale. This could include a raised roofline, turret, corner balconies, bay windows, special awning or canopy design, and/or distinctive use of building materials (see the following examples).

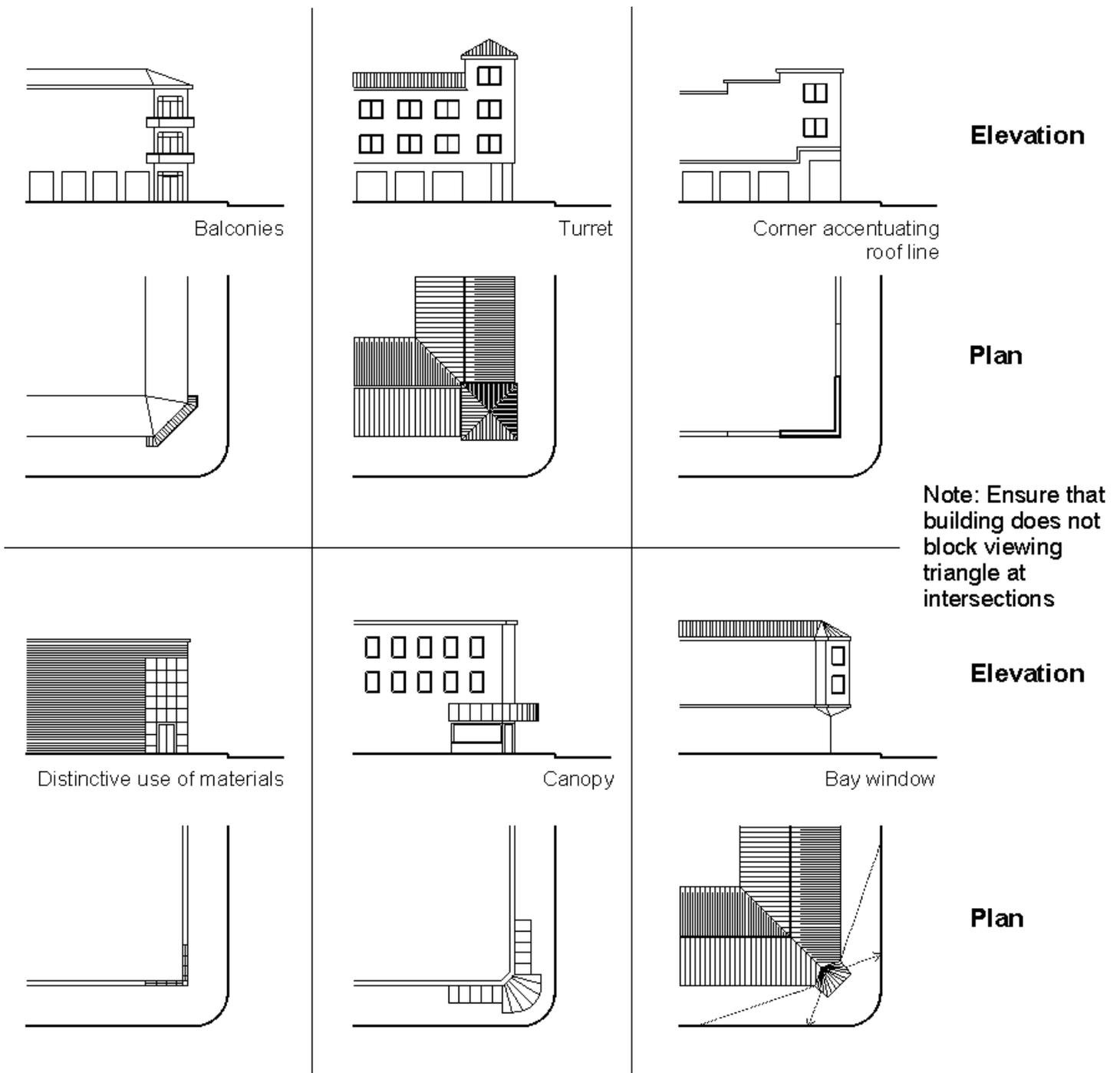


Figure 19. Desirable building elements for street corners.

## 4. Pedestrian-Friendly Building Fronts

---

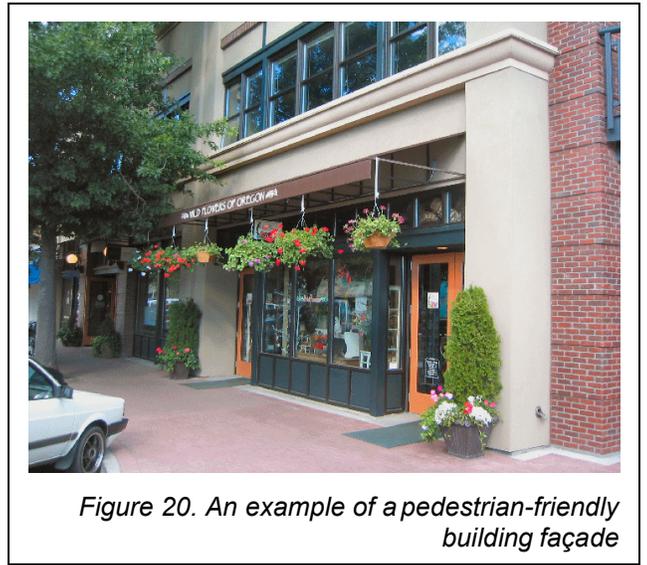
### Objectives Intent

- To enhance the pedestrian environment within the Totem Lake Business District.
- To create safe and active sidewalks and pathways.

### Guidelines

Incorporate transparent windows and doors and weather protection features along all non-residential facades adjacent to a sidewalk or internal pathway. Weather protection features could include awnings, canopies, marquees, or other permitted treatments.

Alternative treatments may be considered if they meet the objectives. For example, reduced transparency and weather protection levels may be considered if an alternative configuration provides other amenities above and beyond what is required by KZC Chapter 92 and the Design Guidelines, and if the building details and architectural treatments provide interest at close range and won't "deaden" the pedestrian environment or create a potential safety problem.



*Figure 20. An example of a pedestrian-friendly building façade*

## 5. Building Location and Orientation

### Objectives Intent

- To enhance the character and identity of the Totem Lake Business District.
- To upgrade the appearance of streets within the Totem Lake Business District.
- To increase pedestrian circulation.
- Create focal points, particularly on large sites.
- To encourage development configurations that minimize negative impacts to adjacent single family residential areas.



Figure 21. Encourage developments to place parking lots to the side or rear, as accomplished here

### Guidelines

- Locate and orient buildings toward streets, plazas or common open spaces, and major internal pathways, with parking to the side and/or rear.
- Configure buildings to create focal points of pedestrian activity. This is particularly important on large sites.
- Configure development to provide opportunities for coordinated pedestrian and vehicular access. Where there are no current opportunities for coordinated access, developments should provide the opportunity for future coordination, where desirable, should the adjacent site be redeveloped in the future.
- Site and orient multi-story buildings to minimize impacts to adjacent single family residents. For example, if a multi-story building is located near a single family property, provide landscaping elements and/or minimize windows and openings to protect the privacy of adjacent homes. Another consideration is to increase upper level building setbacks.
- Ensure vehicle sales uses locate their showrooms towards the street (with parking to the side or rear):
  - Allow designated vehicle display areas between a portion of the property street frontage if the display is integrated creatively with the landscaping.
  - Allow increased signage through coordinated master sign plans.
  - Allow modifications in perimeter landscaping adjacent to a street.
  - Ensure that inventory areas located along the perimeter are visually orderly and landscaped.
- Encourage buildings located adjacent to any street to orient to the street. This includes pedestrian entries from the sidewalk and windows facing the street. Avoid fences or hedges that block visibility between buildings and the street. Exceptions may be considered consistent with the objectives and guidelines herein.

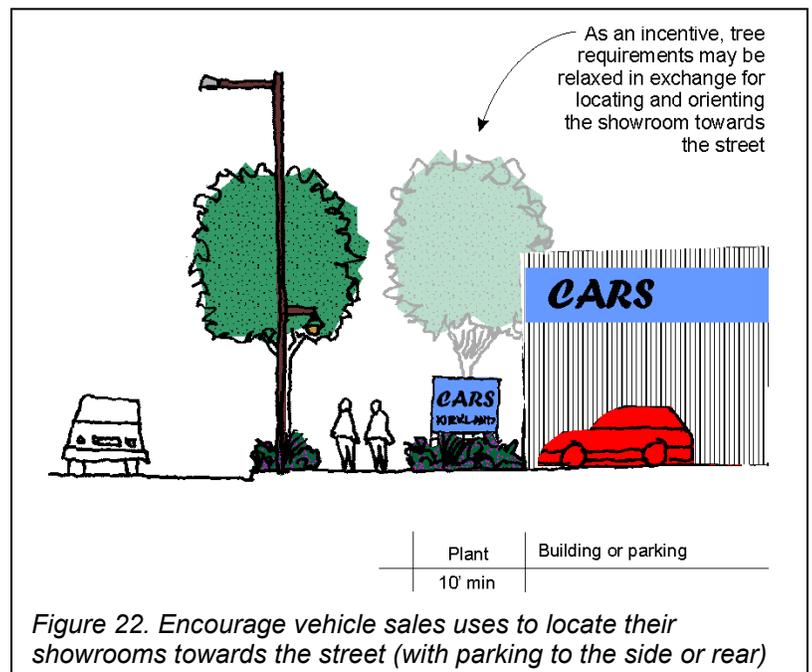


Figure 22. Encourage vehicle sales uses to locate their showrooms towards the street (with parking to the side or rear)

***Special considerations in Districts TL 5, TL 6B, TOD in PR 1.8 zones, and other Large Site Developments:***

1. **TL 5:** In this district where buildings may front on more than one street, first priority for building orientation should be to any designated pedestrian-oriented street.
2. **TL 6A:** Residential buildings located adjacent to NE 120<sup>th</sup> Street should be oriented toward this street and to Slater Avenue NE. Common and/or individual entries and windows should face the street. Parking areas should not be located between the building and the street.
3. **TL 6B:** Single purpose residential buildings should be configured and oriented to take advantage of the greenbelt area to the north. For example, buildings could be arranged in a courtyard layout with the courtyard opening towards the greenbelt area.
4. **TL 5, TL 6B and other Large Site Development:** Where buildings front on both pedestrian oriented streets and through-block pathways, building orientation may be to internal focal points and public-gathering spaces and streets. Parking areas should not occupy the majority of a site’s frontage.
5. **TL 5, TL 6B and other Large Site Development:** Where buildings are oriented to an interior open space or courtyard, primary building entries may orient to the open space provided there is direct visibility into the open space from the sidewalk. Windows should be provided on the street façade.
6. **TOD in PR 1.8 Zone:** Required yards along 116<sup>th</sup> Way NE may be reduced for commercial uses designed with pedestrian-oriented facades with direct access to 116<sup>th</sup> Way NE and residential uses that incorporate front entries, porches, and stoops oriented to 116<sup>th</sup> Way NE.

**6. Sidewalk and Pathway Widths**

**Objectives Intent**

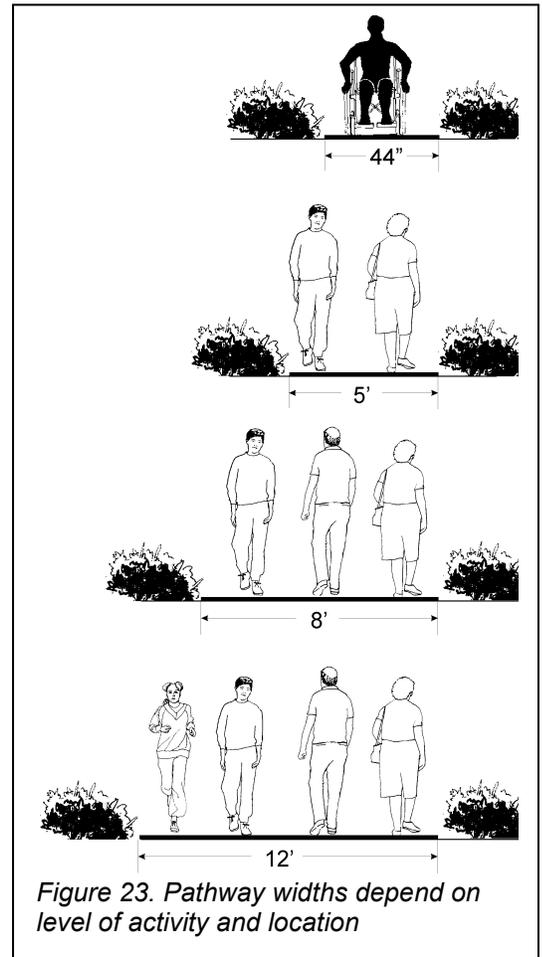
- To provide wide sidewalks and pathways that promote an increase in pedestrian activity within the Totem Lake Business District.

**Discussion**

Sidewalks have three overlapping parts with different functions: the curb zone, the movement zone, and the storefront or activity zone. A well-sized and uncluttered movement zone allows pedestrians to move at a comfortable pace.

**Guidelines**

- a. Integrate a “curb zone” into the sidewalk or pathway width. This space can include street trees, newspaper stands, street signs, garbage cans, phone booths, mailboxes, etc. Subtle changes in paving patterns between the curb zone and the movement zone can be effective and should be considered.
- b. Sidewalks or pathways adjacent to moving vehicular traffic need generous buffers to make them safer and more inviting. Landscaping elements are particularly important physical and visual buffers between walkways and streets or other vehicle access areas. As a general rule, the higher the travel speed, the greater the buffer should be between moving cars and pedestrians.



*Figure 23. Pathway widths depend on level of activity and location*

c. Design sidewalks and pathways to support a variety and concentration of activities and provide a separation for the pedestrian from the busy street. Specifically:

**Considerations for the “movement zone” widths:**

- Curb zones with parallel parking typically need 4’-6’; without parallel parking: 3’-4’.
- 12’ accommodates 4 persons walking abreast.
- 8’ accommodates 3 persons walking abreast.
- 5’ accommodates 2 persons walking abreast.

**Considerations for the “store front zone” widths:**

- Outdoor dining uses: 6’ allows for one table.
- Outdoor displays typically need at least 4’ (6’ preferable).



*Figure 24. High-traffic streets without on-street parking warrant wider planting strip buffers*

## 7. Pedestrian Coverings

### Objectives Intent

- To provide shelter for pedestrians.
- To provide spatial enclosure and add design interest to a retail or office streetscapes.

### Discussion

The design and width of pedestrian coverings should be determined by their function, the building's use and the type of street.

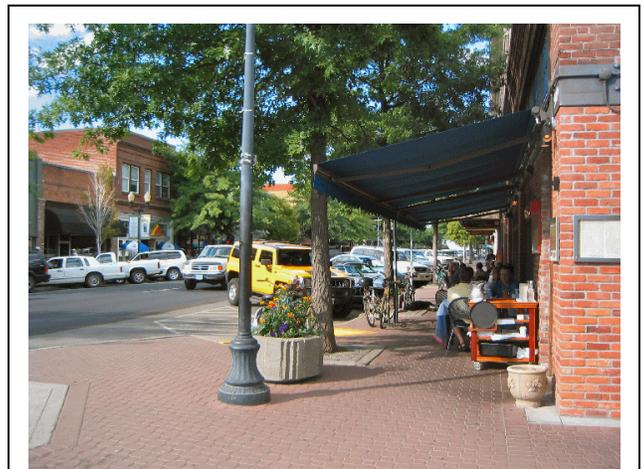
As a general rule, the more traffic an entry is expected to accommodate, the larger the covered area should be at the entry. Larger porches and covered entries also invite pedestrian activity.

For example, a 5' x 5' covered area allows two adults to converse comfortably out of the rain. A 3' to 4' wide canopy will provide rain cover for window-shopping, a 5' wide or greater canopy will provide cover for a street sale, and a 7' to 8' wide canopy will provide room for a window shopper and a passing couple.

The width of the sidewalk should also be considered when sizing the pedestrian covering (wider sidewalks can accommodate wider pedestrian coverings). Canopies and awnings should be appropriately dimensioned to allow for tree growth, where applicable. The architecture of the building and the spacing of individual storefronts should help determine the appropriate placement and style of the canopy or awning. Continuous, uniform awnings or canopies, particularly for multi-tenant retail buildings, can create a monotonous visual environment and are discouraged.

### Guidelines

- a. Provide weather protection along the primary exterior entrance of all businesses, residential units, and other buildings.
- b. Design weather protection features to provide adequate width and depth at building entries and along building facades that are oriented toward sidewalks and pathways.
- c. Pedestrian covering treatments may include: covered porches, overhangs, awnings, canopies, marquees, recessed entries or other similar features. A variety of styles and colors should be considered, where compatible with the architectural style of the building and the ground floor use.
- d. Back lit, plastic awnings are not appropriate.



*Figure 25. Wider pedestrian coverings allow for outdoor dining*



*Figure 26. Note how these awnings have been integrated into the building's storefront space*

## 8. Blank Walls

### Objectives Intent

- To minimize visible blank walls.
- To enhance public safety along sidewalks and pathways.
- To encourage design elements that enhance the character of buildings at all perceived distances.

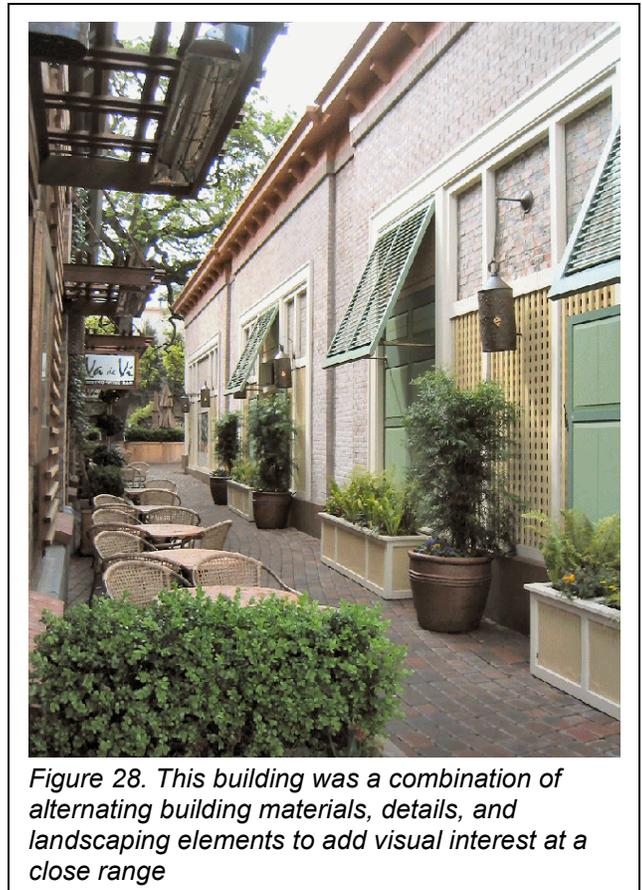
### Discussion

Blank walls on commercial street frontages deaden the pedestrian environment and can break the continuity of uses along a street or pathway. Blank walls can also create a safety problem, particularly where adjacent to pedestrian areas, as they don't allow for natural surveillance of those areas. However, in some cases fire walls, for example, require the intrusion of a flat, unadorned surface. The adverse impact of a blank wall on the pedestrian streetscape can be mitigated through the methods listed in the Guidelines below.

### Guidelines

Avoid blank walls near sidewalks, major internal walkways, parks, and pedestrian areas. The following treatments mitigate the negative effects of blank walls (in order of preference):

- Configure buildings and uses to minimize blank walls exposed to public view.
- Provide a planting bed with plant material to screen most of the wall.
- Install trellises with climbing vines or plant materials to cover the surface of the wall. For long walls, a trellis or trellises should be combined with other design treatments to avoid monotony.
- Provide artwork on the wall surface.
- Provide architectural techniques that add visual interest at a pedestrian scale. This could include a combination of horizontal building modulation, change in building materials and/or color, and use of decorative building materials.
- Other treatments may be proposed that meet the intent of the guidelines.



## 9. Lighting

### Objectives Intent

- To enhance safety.
- To create inviting pedestrian areas.
- To provide adequate lighting without creating excessive glare or light levels.

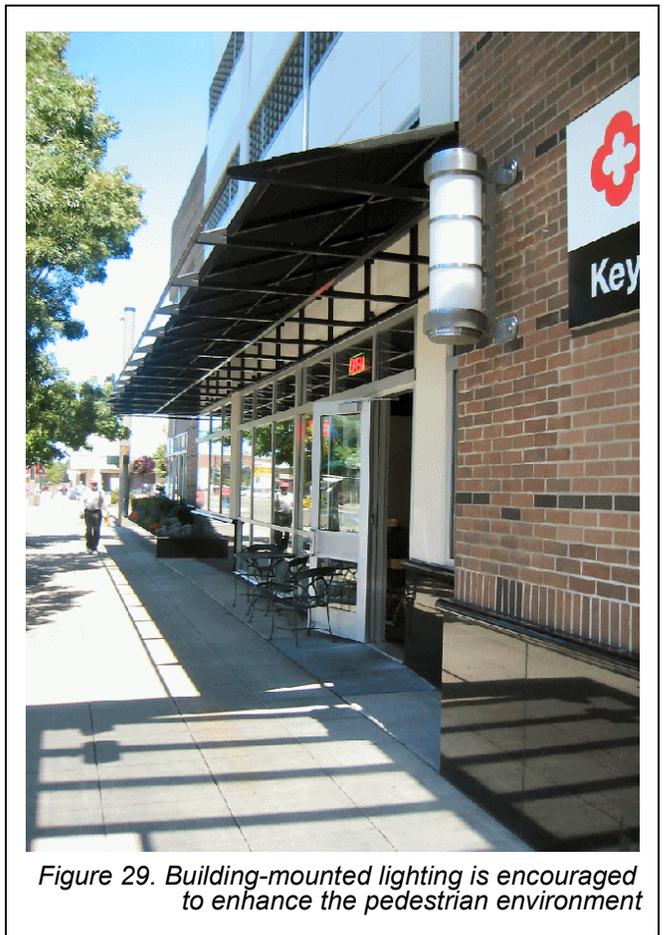
### Discussion

Overpowering and uniform illumination from commercial uses creates glare and destroys the quality of night light, especially for adjacent residential areas. Well placed light fixtures will form individual pools of light and maintain sufficient lighting levels for security and safety purposes.

### Guidelines

- Provide adequate lighting levels in all areas used by pedestrians and automobiles, including building entries, walkways, parking areas, circulation areas, and open spaces. Recommended minimum light levels:
  - Building entries: 4 foot candles
  - Primary pedestrian walkway: 2 foot candles
  - Secondary pedestrian walkway: 1-2 foot candles
  - Parking lot: .60 -1 foot candle
  - Enclosed parking garages for common use: 3 foot candles
- Lighting should be provided at consistent levels, with gradual transitions between maximum and minimum levels of lighting and between lit areas and unlit areas.
- Building facades in pedestrian areas should provide lighting to walkways and sidewalks through building mounted lights, canopy- or awning-mounted lights, and display window lights. Encourage variety in the use of building-mounted light fixtures to give visual variety from one facade to the next.
- Minimizing impacts of lighting on adjoining activities and uses should be considered in the design of lighting. This is particularly important adjacent to residential uses.

Parking lot light fixtures should be non-glare and mounted no more than 15' above the ground. Lower level lighting fixtures are preferred to maintain a human scale. Lights up to 20' may be used for safety, when needed. Ideally, all exterior fixtures should be fitted with a full cut-off shield to minimize light spill over onto adjoining properties.



*Figure 29. Building-mounted lighting is encouraged to enhance the pedestrian environment*

## 10. Pedestrian Amenities

### Objectives Intent

- To provide amenities that enrich the pedestrian environment.
- To increase pedestrian activity in the Totem Lake Business District.

### Discussion

Site features and pedestrian amenities, such as lighting, benches, paving, waste receptacles, and other site elements, are an important aspect of a business district's character. These elements reduce apparent walking lengths and unify the district's visual character. In zones where public pedestrian space is required to be provided at along the frontage of a building, additional amenities may be required to provide an attractive gathering space.

### Guidelines

Provide pedestrian amenities along all sidewalks, interior pathways and within plazas and other open spaces. Desired amenities include:

- Pedestrian-scaled lighting (placed between 12'-15' above the ground).
- Seating space. This can include benches, steps, railings and planting ledges. Heights between 12" to 20" above the ground are acceptable, with 16" to 18" preferred. An appropriate seat width ranges from 6" to 24".
- Pedestrian furniture such as trash receptacles, consolidated newspaper racks, bicycle racks, and drinking fountains.
- Planting beds and/or potted plants.
- Unit paving such as stones, bricks, or tiles.
- Decorative pavement patterns and tree grates.
- Water features.
- Informational kiosks.
- Transit shelters.
- Decorative clocks.
- Artwork.



Figure 30. Consolidated newspaper racks



Figure 31. Bicycle racks



Figure 32. Potted plants



*Figure 33. Decorative pavement patterns (top), benches and pedestrian-scale lighting (middle), and informational kiosk (bottom)*



*Figure 34. This example combines a sculptural water feature with landscaping*

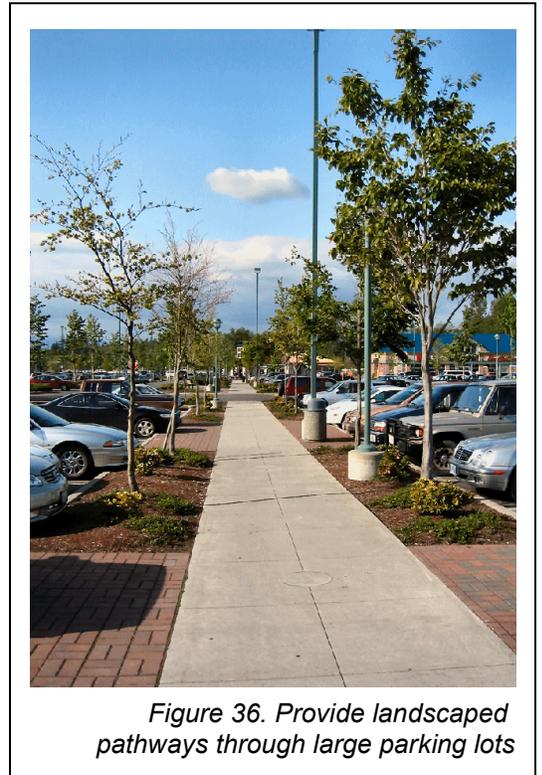
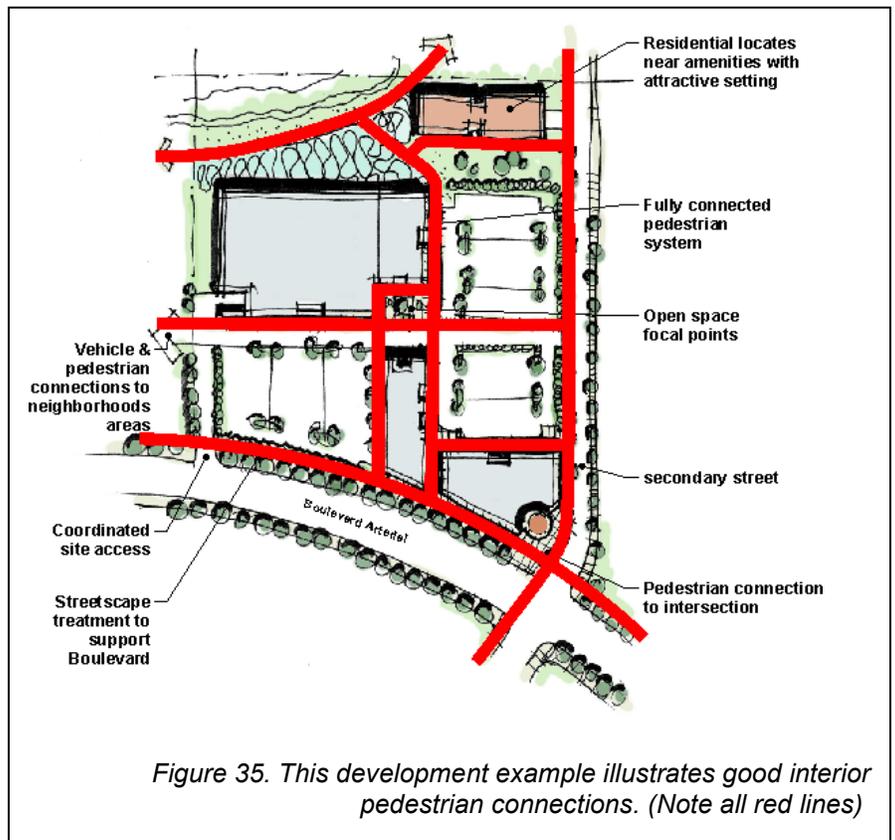
## 11. Interior Pedestrian Connections

### Objectives Intent

- To enhance pedestrian access to the street, adjacent uses, and adjacent sites, where desirable.
- To make it easier to walk between uses.
- To reduce vehicle trips within the district.
- To promote pedestrian activity.
- To enhance pedestrian access through parking lots and between the street and uses.

### Guidelines

- Provide convenient pedestrian access between the street, bus stops, buildings, parking areas, and open spaces. Internal pedestrian connections are particularly important on large sites where some uses may be placed away from a street.
- Design all buildings abutting a public sidewalk or major internal pathways to provide direct pedestrian access to the sidewalk or pathway.
- Provide interior pedestrian connections to adjacent properties containing similar uses or complementary uses. This is most applicable to large lots and where storefronts or other uses are set back away from the street. Where an existing connection is not desirable or possible due to the nature of development on the adjacent site, the applicant should provide an opportunity for a future pedestrian connection where such a connection is desirable and future redevelopment of the adjacent site is possible.
- Provide paved walkways through large parking lots. One walkway should be provided for every three parking aisles. Such access routes through parking areas should be separated from vehicular parking and travel lanes by use of contrasting paving material which may be raised above the vehicular pavement and by landscaping.



**Special Considerations in TL 4, TL 6B, TL 7A:**

- e. **TL 6B:** Develop a trail along the northern edge of the property to take advantage of the site's greenbelt setting. Provide a landscaped buffer area between the trail and any adjacent residential buildings to enhance the character of the trail and provide privacy to adjacent residents.
- f. **TL 6B:** Enhance connections to TL 10A to the south, to ensure safe and convenient access for employees in TL 10A and the shopping district in TL 6B.
- g. **TL 4:** Provide for safe and convenient access between development in TL 4 (west of I-405) and the business park directly to the west in TL 10A.
- h. **TL 7A:** Provide for safe and convenient public pedestrian access between NE 124<sup>th</sup> Street through the subject property to the CKC.

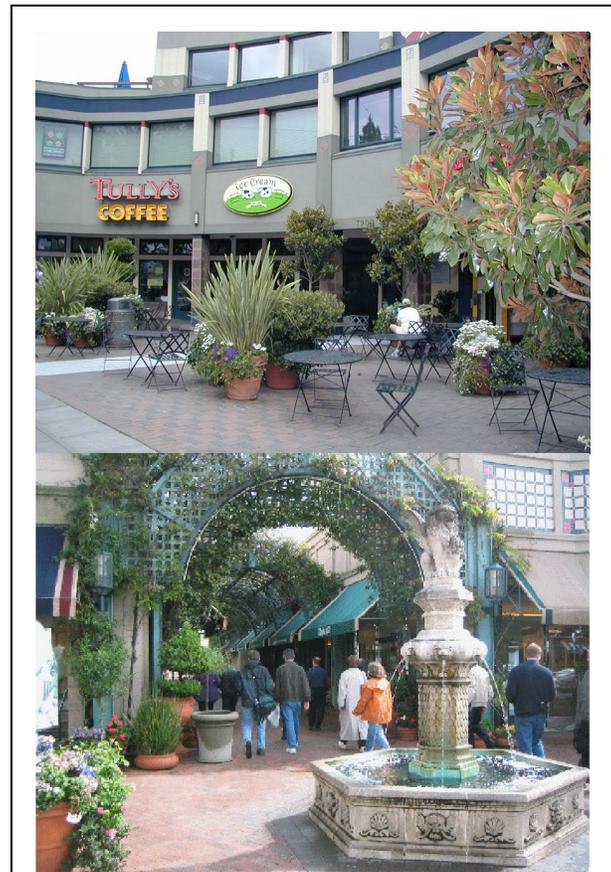
## 12. Pedestrian Plazas

### Objectives Intent

- To provide a variety of pedestrian-oriented areas to attract shoppers to commercial areas and enrich the pedestrian environment.
- To create gathering spaces for the community.
- To configure buildings and uses to encourage pedestrian activity and pedestrian focal points.

### Guidelines

- Provide pedestrian plazas in conjunction with mixed-use development and non-residential uses.
- Publicly accessible space at the primary frontage and between buildings will extend the public realm while creating a transition between public and private spaces, and attract public use by being well-designed, interesting spaces that are integrated with the street environment. The spaces should be of sufficient size to allow for a variety of features, including pedestrian/multi-use paths, plazas, seating, public art and water features.
- Position plazas in visible locations on major streets, major internal circulation routes, close to bus stops, or where there are strong pedestrian flows on neighboring sidewalks. For large sites, development should be configured to create a focal plaza or plazas. Plazas should be no more than 3' above or below the adjacent sidewalk or internal pathway to enhance visibility and accessibility.
- Incorporate plenty of benches, steps, and ledges for seating. A combination of permanent and moveable seating is encouraged. Seating areas should be provided with views of amenities, landscaping elements, or people watching.
- Provide storefronts, street vendors, or other pedestrian-oriented uses, to the extent possible, around the perimeter of the plaza
- Provide landscaping elements that add color and seasonal interest. This can include trees, planting beds, potted plants, trellises, and hanging plants.
- Incorporate pedestrian amenities, as described in Section 10.
- Consider the solar orientation and the wind patterns in the design of the open space and choice of landscaping.
- Provide transitional zones along building edges to allow for outdoor eating areas and a planted buffer.
- Special Consideration for TOD in PR 1.8 zone:**  
Public spaces should be located in the gateway area, near the on-site transit station or along pedestrian routes.



*Figure 37. Good examples of pedestrian plazas. Notice the decorative pavements, landscaping components, adjacent building facades, and other amenities and design details*



*Figure 38. An example of an attractive small plaza space between a sidewalk and a storefront*

Public open space and plazas should be provided on the subject property that can be used by the general public, residents, and transit users.

### **13. Residential Open Space**

---

#### **Objectives Intent**

- To create useable space that is suitable for leisure or recreational activities for residents.
- To create open space that contributes to the residential setting.

#### **Guidelines**

- a. Incorporate common open space into multi-family residential uses. In the Totem Lake Business District, where very high density residential uses are allowed, the quality of the space in providing respite from the buildings on the site is more critical than the amount of space provided. In some developments, multiple smaller spaces may be more useful than one, larger space. Special recommendations for common open space:
  - Consider open space as a focal point of the residential development.
  - Where possible, open space should be large enough to provide functional leisure or recreational activity. For example, long narrow spaces rarely, if ever, can function as usable common space.
  - Open space should provide for a range of activities and age groups. Children's play areas in particular should be visible from dwelling units and positioned near pedestrian activity.
  - Residential units adjacent to the open space should have individual entrances to the space. Preferably, these units should include a small area of semi-private open space enclosed by low level landscaping or hedges (no taller than 42").
  - Open space should feature paths, seating, lighting, and other pedestrian amenities to make the area more functional and enjoyable. It should be oriented to receive sunlight, (preferably south).
  - Separate common space from ground floor windows, streets, service areas, and parking lots with landscaping and/or low-level fencing. However, care should be used to maintain visibility from dwelling units towards open space for safety.
- b. Provide private open space for multi-family residential units. For townhouses and other ground-based housing units, provide patios, decks, and/or landscaped front or rear yards adjacent to the units. For all other units, provide balconies large enough to allow for human activity.



*Figure 39. Good examples of common open space, including street-level courtyards (left), a children's play area (top right), and a pedestrian corridor (lower right)*

## 14. Parking Lots and Vehicular Circulation

---

### **Objectives Intent**

- To minimize the impact of parking facilities on the fronting street, pedestrian environment, and neighboring properties.
- To enhance pedestrian and vehicular safety.
- To maintain desired traffic flow on Totem Lake arterials.
- To promote shared parking
- To provide attractive and connected vehicular circulation routes.

### **Discussion**

Parking lots can detract from the pedestrian and visual character of a commercial area. The adverse impacts of parking lots can be mitigated through sensitive design, location, and configuration. Large parking lots can be confusing unless vehicle and pedestrian circulation patterns are well organized and marked. The Totem Lake Business District Plan encourages shared parking between properties to reduce curb cuts, reduce congestion of cars turning in and out of parking lots and consolidating consumer trips between businesses.

Where not specifically prohibited, drive-through facilities for some uses such as fast food restaurants, pharmacies, or auto oriented uses may be appropriate if designed to minimize vehicle queuing along rights of way, blocking driveways or parking aisles, or impeding pedestrian movement. Aesthetically, drive-throughs should be located away from street frontages or screened as viewed from the right of way.

### **Guidelines**

#### Driveways

- a. Minimize the number of curb cuts into a development, particularly off of arterials. To the extent possible, adjacent developments should share driveways.

#### Parking Lot Location and Design

- b. Locate vehicular parking areas to the side or rear of buildings, to the extent possible.
- c. Avoid parking layouts that visually dominate a development. Break up large parking lots into smaller ones.
- d. Take advantage of topography to hide parking underneath buildings.
- e. Provide a clear and well-organized parking lot design. Space should be provided for pedestrians to walk safely in all parking lots.

#### Parking Lot Landscaping

- f. Integrate landscaping into parking lots to reduce their visual impact. Provide planting beds with a variety of trees, shrubs, and ground cover to provide visual relief, summer shade, and seasonal interest.

#### Parking Lot Screening

- g. Provide low level screening and perimeter landscaping where parking is adjacent to sidewalks in order to improve visual qualities and reduce clutter. While vertical elements such as trees, are encouraged to define the street edge, all screening methods should maintain visibility at eye level between the street and parking area. For instance, hedges or walls should not be taller than 3 feet and trees should be trimmed to allow visibility between 3 and 8 feet above the ground.

- h. Provide extensive screening and landscaping between parking lots and residential uses and open spaces. A combination of a screen wall with a landscape buffer is preferred.

Vehicular Circulation

- j. Develop an efficient internal vehicular access system that minimizes conflicts with pedestrians and vehicular traffic. For TL 5 and TL 6B, see the illustrations in the Design Vision for the Totem Lake Business District section.
- k. Configure development to provide interior vehicular connections to adjacent uses, where desirable. Where current connections to adjacent uses are not feasible, but desirable in the future, configure development to **provide the opportunity for a future connection**, should the adjacent site be redeveloped.
- l. Avoid parking lot configurations with dead-end lanes.
- m. Configure internal access roads to look and function like public streets. This is most applicable to larger sites, such as those in TL 5 and TL 6B, where an internal vehicular circulation system is critical to access interior portions of the sites. The most desirable configuration would include on-street parking, street trees and sidewalks on both sides of the roadway.



Figure 40. Example of an internal vehicular access design.

Drive-Through Facilities

- n. Design drive-through windows to be oriented away from the street frontage and preferably not located between a building and the street. Where drive-through lanes face a street, avoid large featureless walls and provide sufficient landscaping to soften the visual impact of vehicle stacking areas for drive through windows. Locate driving lanes so as not to interfere with pedestrian or vehicular circulation.

**15. Parking Garages**

Objectives Intent

- To mitigate the visual impacts of parking garages in the urban environment.

**Guidelines**

- a. Mitigate the intrusive qualities of parking garages. Along streets, pedestrian pathways and in pedestrian areas, ground-level commercial uses should be incorporated into parking structures. Where garages cannot be located underground and must be located on the ground floor, and intervening commercial uses are not required, techniques such as extensive landscaping around the base of garages, metal or mesh screening or other materials on the building facade should be used to screen the parking garage near residential areas, internal roads and pedestrian paths, and other high visibility locations.
- b. Design and site parking garage entries to complement, not subordinate the pedestrian entry. If possible, locate the parking entry away from the primary street, to either the side or rear of the building.
- c. Use similar architectural forms, materials, and/or details to integrate the garage with the development.

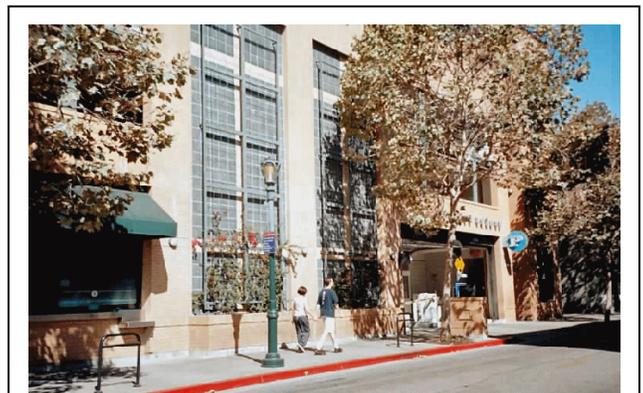


Figure 41. This parking garage includes street front retail space and landscaped trellises to mitigate visual impacts on the streetscape

- d. Locate parking structure service and storage functions away from the street edge and generally not visible from the street, sidewalks, or the CKC.

## 16. Architectural Style

---

### Objectives Intent

- To improve the architectural design of commercial buildings in the business district.
- To provide architecture that fits into the context of the adjacent uses surrounding the business district.

### Discussion

As there is no single predominate architectural style in the Totem Lake Business District, the guidelines contained in this document provide flexibility on the chosen styles (provided the architectural style, human scale, building details, and building materials and color standards in KZC Chapter 92 and these guidelines are met).

## 17. Architectural Scale

---

### Objectives Intent

- To encourage an architectural scale of development that is compatible with the vision for the districts within the Totem Lake Business District.
- To implement the planning concepts for the distinct design districts within the Totem Lake Business District.
- To add visual interest to buildings.

### Discussion

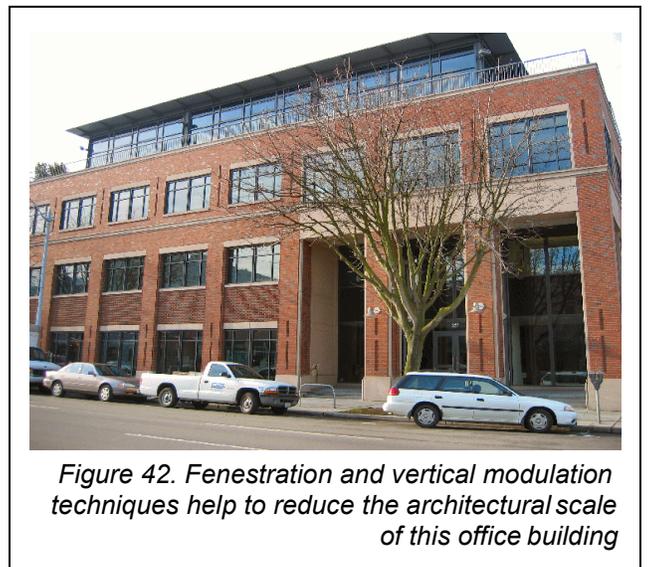
The guidelines in this section describe a variety of techniques to give a comfortable human scale by providing building elements that help individuals relate to the building. “Architectural scale” means the size of a building relative to the buildings or elements around it. When the buildings in a district are about the same size and proportion, we say they are “in scale.” As both the vision and development regulations for the Totem Lake Business District provide for much larger buildings than currently exist, special care must be taken to design buildings, so they do not overpower the others. The exception to this rule is an important civic or cultural building that has a prominent role in the community.

### Guidelines

A combination of techniques is desirable to reduce the architectural scale of buildings. Specifically, these techniques are encouraged at intervals of no more than 70 feet for non-residential uses and 30 feet for residential uses. Office buildings are provided with greater flexibility. Alternatives will be considered provided they meet the objectives of the guidelines.

- a. Incorporate fenestration techniques that indicate the scale of the building. For example, the size, location, and number of windows in an urban setting create a sense of interest that relies on a subtle mixture of correct ratios, proportions, and patterns. This is particularly important on upper floors, where windows should be divided into units no larger than 35 square feet, with each window unit separated by a visible mullion or other element at least 6 inches wide. “Ribbon windows” (continuous horizontal bands of glass) or “window walls” (glass over the entire surface) do little to indicate the scale of the building and are thus discouraged, except in special circumstances where they serve as an accent element.

Patterns of fenestration should also vary depending on whether the street is pedestrian- or automobile-oriented. A



window pattern that is interesting from a car may be monotonous to a slow-moving pedestrian; likewise, a window pattern that is interesting to a pedestrian may seem chaotic from a fast-moving car. Thus, pedestrian oriented fenestration should allow for more complex arrangements and irregularity while automobile-oriented fenestration should have more gradual changes in pattern and larger and simpler window types. An optimum design goal would allow for varied treatment of window detailing with unifying features such as 18" to 24" sills, vertical modulation in structure, varied setbacks in elevation, and more highly ornamented upper-story windows.

b. Encourage vertical modulation on multi-story buildings to add variety and to make large buildings appear to be an aggregation of smaller buildings. Vertical modulation may be particularly effective for tall buildings adjacent to a street, plaza, or residential area to provide compatible architectural scale and to minimize shade and shadow impacts. Vertical modulation is well-suited for residential development and sites with steep topography.

c. Encourage a variety of horizontal building modulation techniques to reduce the architectural scale of the building and add visual interest. Horizontal building modulation is the horizontal articulation or division of an imposing building façade through setbacks, awnings, balconies, roof decks, eaves, and banding of contrasting materials. Elevations that are modulated with horizontal elements appear less massive than those with sheer, flat surfaces. Specifically:

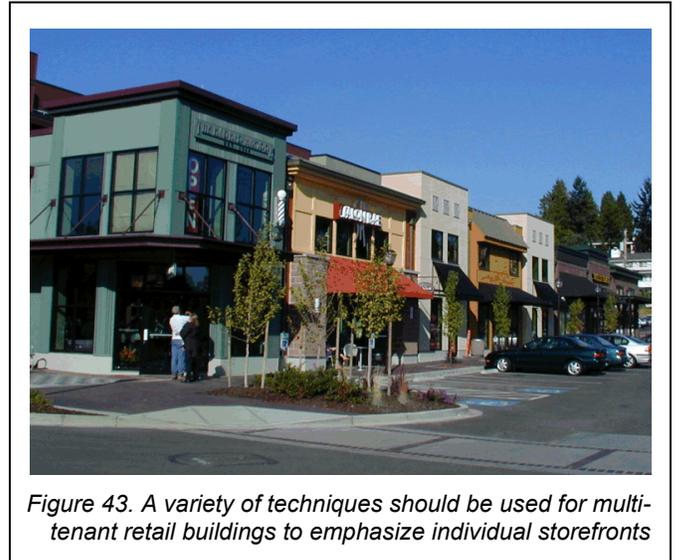
- For single purpose retail buildings, use horizontal building modulation with roofline modulation and a change in building materials, as necessary to meet objectives of the guidelines from all perceived distances. This is particularly important for large scale retail buildings (over 40,000 square feet) or multi-tenant retail buildings placed adjacent to a parking lot where they can be viewed from relatively great distances.
- Provide horizontal building modulation for residential uses based on individual unit size. Horizontal modulation is most effective when combined with roofline modulation and changes in color and/or building materials. The depth and width of the modulation should be sufficient to meet the objectives of the guidelines. Avoid repetitive modulation techniques, since they may not be effective when viewed from a distance. Larger residential buildings will require greater horizontal modulation techniques to provide appropriate architectural scale.

d. Office buildings: Use design techniques to break up long continuous walls. A combination of horizontal building modulation, change in fenestration, and/or change in building materials should be used to accomplish this.

e. Encourage a variety of roofline modulation techniques. This can include hipped or gabled rooflines and modulated flat rooflines. As a general rule, the larger the building or unbroken roofline, the bigger the modulation should be. In determining the appropriate roof type and amount of modulation, consider at what distance the building can be viewed. For example, a large commercial building adjacent to a parking lot is capable of being viewed from a relatively large distance. Consequently, the roofline modulation techniques must be sufficient to provide an appropriate architectural scale that provides visual interest.

f. **Special Consideration for TOD in PR 1.8 zone:**

The location of the subject property makes any new multi-story building highly visible from the surrounding streets and the freeway. The arrangement of building mass should address key vantage points and respond to the context of existing and/or planned improvements, gateway features, location of plazas and open space, and surrounding streets. In addition to the architectural scale techniques described above, long, unbroken facades along 116<sup>th</sup> Way NE should be avoided through limiting building façade length or providing a separation between buildings for a pedestrian corridor. Building mass should be reduced where reduced setbacks are desired along 116<sup>th</sup> Way NE for pedestrian



*Figure 43. A variety of techniques should be used for multi-tenant retail buildings to emphasize individual storefronts*

oriented development and in the gateway area. Special attention should also be given to the west sides of the buildings adjacent to the residential uses to mitigate the height and mass of the structures by using modulation or other design techniques described above.

## 18. Human Scale

---

### Objectives Intent

- To encourage the use of building components that relate to the size of the human body
- To add visual interest to buildings.

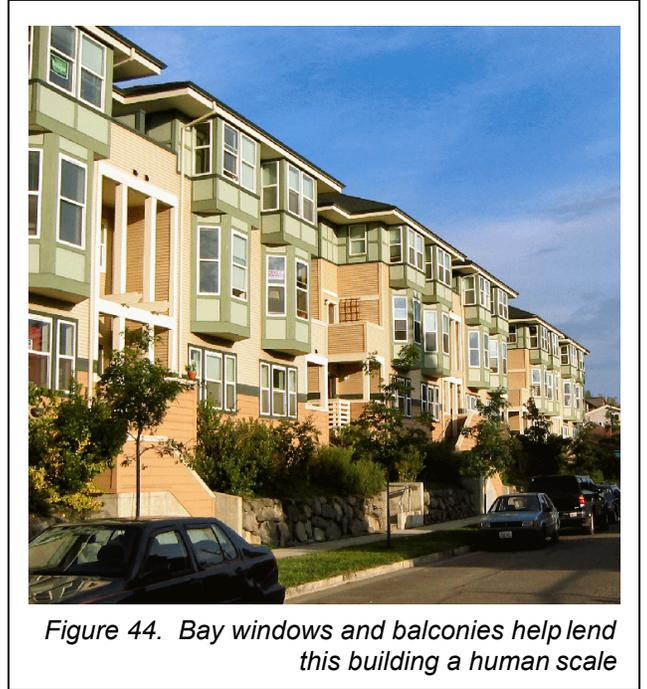
### Discussion

The term “human scale” is generally used to indicate a building’s size relative to a person, but the actual size of a building or room is often not as important as its perceived size. A variety of design techniques may be used to give a space or structure the desired effect; for example, to make a room either more intimate or spacious, or a building either more or less imposing.

Special elements in a building facade create a distinct character in an urban context. A bay window suggests housing, while an arcade suggests a public walkway with retail frontage. Each element must be designed for an appropriate urban setting and for public or private use. A building should incorporate special features that enhance its character and surroundings. Such features give a building a better-defined “human scale.”

### Guidelines

- a. Encourage a combination of architectural building elements that lend the building a human scale. Examples include arcades, balconies, bay windows, roof decks, trellises, landscaping, awnings, cornices, friezes, art concepts, and courtyards. Window fenestration techniques described in Section 17 can also be effective in giving humans clues as the size of the building. Consider the distances from which buildings can be viewed (from the sidewalk, street, parking lot, open space, etc.).



*Figure 44. Bay windows and balconies help lend this building a human scale*

## 19. Building Details and Materials

### Objectives Intent

- To use building and site design details that add visual interest to buildings/sites at a pedestrian scale.
- To use a variety of quality building materials such as brick, stone, glass, timber, and metal, which are appropriate to the Pacific Northwest climate, and complementary to the desired visual character of the district.

### Guidelines

- a. Encourage the integration of ornament and applied art with the structures and the site environment. For example, significant architectural features should not be hidden, nor should the urban context be overshadowed. Emphasis should be placed on highlighting building features such as doors, windows, eaves, and on materials such as wood siding and ornamental masonry. Ornament may take the form of traditional or contemporary elements. Original artwork or hand-crafted details should be considered in special areas. Ornament and applied art can be used to emphasize the edges and transition between public and private space, and between walls to ground, roof to sky, and architectural features to adjacent elements. Ornament may consist of raised surfaces, painted surfaces, ornamental or textured banding, changing of materials, or lighting.
- b. Use a variety of quality building materials such as brick, stone, timber, and metal, to add visual interest to the buildings and reduce their perceived scale. Masonry or other durable materials should be used near the ground level (first 2 feet above sidewalk or ground level).



## 20. Signs

---

### **Objectives Intent**

- To encourage the use of creative, well-crafted signs that contribute to the character of the district.

### **Discussion**

Kirkland's Zoning Code regulates signs throughout the city in order to create a high-quality urban environment. Automobile-oriented signs typically found on commercial strips can be overpowering and obtrusive. Pedestrian signs are smaller and closer to viewers; thus, creative, well-crafted signs are more cost effective than large signs mounted high on poles. A balance between the needs of a high traffic corridor and pedestrians should be considered in the design of signs. Signs should be an integral part of a building's façade or act as a center identification for the passing motorist to a commercial center. The location, architectural style, and mounting of signs should conform to a building's architecture and not cover up or conflict with its prominent architectural features. A sign's design and mounting should be appropriate for the setting.

### **Guidelines**

- a. Provide pedestrian oriented signs on all commercial facades where adjacent to a sidewalk or walkway. This includes signs located within 15' of the ground plane, such as "blade" signs which hang below canopies. Small signs located on canopies or awnings are also effective along building facades at the street. Signs with quality graphics and a high level of craftsmanship are important in attracting customers. Sculpted signs and signs that incorporate artwork add interest.
- b. External lighting is preferred. If internal lit cabinet signs are used, darker background with lighter lettering is more aesthetically pleasing. Neon signs are appropriate when integrated with the building's architecture.
- c. Ground-mounted signs should feature a substantial base and be integrated with the landscaping and other site features.
- d. Mounting supports should reflect the materials and design character of the building or site elements or both. Too much variety, too much uniformity though unified by common design elements, signs can still express the individual character of businesses.
- e. Master-planned, larger commercial centers are encouraged to combine signage for the whole complex that complements the architectural design of the center and oriented to automobile traffic.

## 21. Service Areas

---

### **Objectives Intent**

- To provide essential service areas without adversely impacting the quality of development.
- To locate and design site service and storage areas to promote ease of use, safety, and visual cohesion.

### **Guidelines**

- a. Locate and design service and storage areas to minimize impacts on the pedestrian environment and adjacent uses. Service elements should generally be concentrated and located where they are accessible to service vehicles and convenient for tenant use.
- b. The design of service enclosures should be compatible with the design of adjacent buildings. This may be accomplished by the use of similar building materials, details, and architectural styles. Such enclosures should be made of masonry, ornamental metal, heavy wood timber, or other durable materials.
- c. Roof-mounted mechanical equipment should be located so as not to be visible from the street, public open space, parking areas, or from the ground level of adjacent properties. Screening features should blend with the architectural character of the building. Equipment screening and preferred location should be included in the early design of a building.

## 22. Visual Quality of Landscapes

---

### **Objectives Intent**

- To enhance the visual quality of the urban environment.

### **Discussion**

The relationship between landscaping and architecture is symbiotic; plant materials add to a building's richness, while the building points to the architectural qualities of the landscaping. Foliage can soften the hard edges and improve the visual quality of the urban environment. Landscaping treatment in the urban environment can be categorized as a pedestrian/auto, pedestrian, or building landscape.

The pedestrian/auto oriented landscape applies to where the pedestrian and auto are in close proximity. Raised planting strips can be used to protect the pedestrian from high-speed and high-volume traffic. Street trees help create a hospitable environment for both the pedestrian and the driver by reducing scale, providing shade and seasonal variety, and mitigating noise impacts.

The pedestrian landscape offers variety at the ground level through the use of shrubs, ground cover, and trees. Pedestrian circulation, complete with entry and resting points, should be emphasized. If used effectively, plant materials can give the pedestrian visual cues for moving through the urban environment. Plant materials that provide variety in texture, color, fragrance, and shape are especially desirable.

The Building Landscape. Landscaping around urban buildings, particularly buildings with blank walls, can reduce scale and add diversity through pattern, color, and form.

Examples of how landscaping is used to soften and enhance the visual quality of the urban environment include:

- Dense screening of parking lots;
- Tall cylindrical trees to mark an entry;
- Continuous street tree plantings to protect pedestrians;
- Several clusters of dense trees along long building facades;
- Cluster plantings at focal points;
- Parking with trees and shrubs planted internally as well as on the perimeter.

### **Guidelines**

- a. Consider the purpose and context of the proposed landscaping. The pedestrian/auto oriented landscape requires strong plantings of a structural nature to act as buffers or screens. The pedestrian landscape should emphasize the subtle characteristics of the plant materials. The building landscape should use landscaping that complements the building's favorable qualities and screens its faults while not blocking views of the business or signage.

Other considerations:

- Encourage a colorful mix of drought tolerant and low maintenance trees, shrubs and perennials. Except in special circumstances, ivy and grass lawn should be avoided.
- Take advantage of on-site topography to hide parking and enhance views.
- Use wooded slopes as a natural site amenity and to screen unwanted views, where applicable.

## **23. Territorial Views**

---

### **Objectives Intent**

- To encourage development to take advantage of views, while minimizing impacts to public views.
- To configure buildings and site features to enhance views from surrounding properties.

### **Guideline**

- a. Encourage rooflines to roughly follow the slope of the existing terrain. Parking garages should be terraced into slopes to minimize building bulk, wherever possible. Buildings are encouraged to step down hillsides.

The City of Kirkland

# Design Guidelines

For Yarrow Bay Business District

Adopted by the City Council pursuant to  
Kirkland Municipal Code Section 3.30.040  
Updated November 15, 2011.



  
Joan McBride,  
Mayor



Eric Shields  
Director,  
Planning & Community  
Development

# Table of Contents

## Introduction

---

Purpose of Design Guidelines .....	1
Vision for Yarrow Bay Business District .....	2

## design guidelines for YBd 1

---

High Quality Building and Design .....	3
Building Scale and Massing .....	4
Pedestrian Amenities .....	5
Streetscape .....	6
Gateways .....	7
Parking Facilities .....	8
Public Amenities and Open Space .....	9

## design guidelines for YBd 2 and 3

---

Building Location and Orientation .....	9
Parking Lots and Vehicular Circulation .....	11
Parking Structures .....	12
Architectural Scale .....	13
Human Scale .....	15
Pedestrian Connections .....	16
Natural Features .....	16
Blank Walls .....	17
Pedestrian-Friendly Building Fronts .....	18
Pedestrian Plazas .....	18
Residential Open Space .....	19
Pedestrian Amenities .....	20.
Pedestrian Coverings .....	22
Building Details and Materials .....	23
Entry Gateway Features .....	24
Sidewalk and Pathway Widths .....	25
Street Trees .....	26
Landscaping .....	27
Service Areas .....	28
Lighting .....	29
Signs .....	31

# Introduction

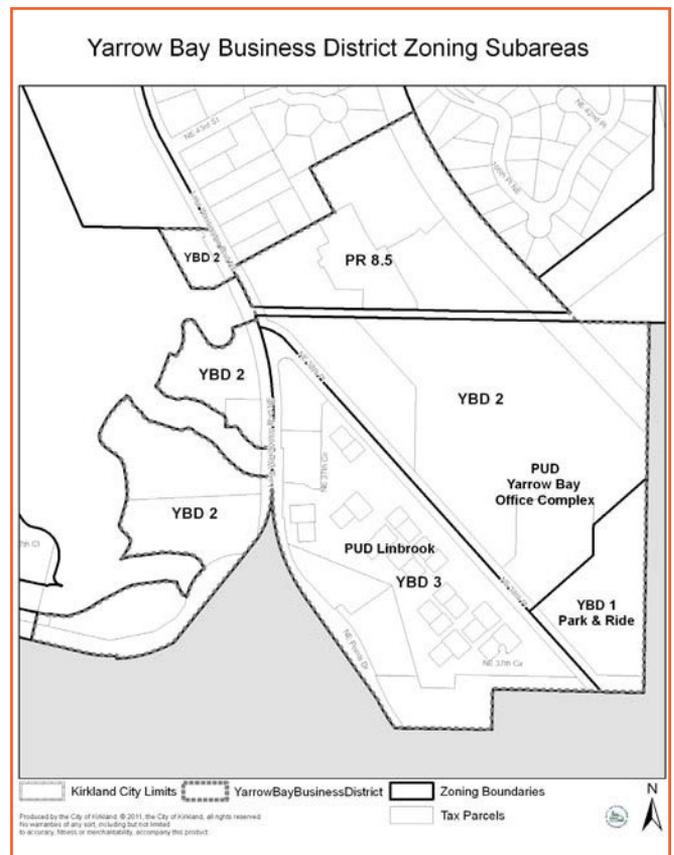
This document sets forth a series of Design Guidelines, adopted by Section 3.30. of the Kirkland Municipal Code that will be used by the City in the design review modification and alternative compliance process for development in the Yarrow Bay Business District located in the Lakeview Neighborhood. The Yarrow Bay Business District includes the YBD 1, YBD 2, YBD 3 and PR 8.5 zones. Other documents that should be referred to during design review are the YBD (Chapter 56) and PR (Chapter 25) Use Zone Charts found in the Kirkland Zoning Code.

## Purpose of the Design Guidelines

The Design Review Board will use these guidelines to evaluate development proposals during the design review process. The Design Guidelines are intended to establish a greater sense of quality, unity, and conformance with Kirkland's physical assets and civic identity. These guidelines are not intended to slow or restrict development, but rather to add consistency and predictability to the permit review process.

KZC Chapter 142 permits design modifications to specific zoning regulations. The Planning Official will use these guidelines to administer the design modification authority of KZC 142.

The Alternative Compliance process (outlined in KZC Chapter 142) is an optional path that allows applicants to propose substantial design modifications in place of the design regulations outlined in Chapter 92, or other zone-specific sections of the KZC. The Alternative Compliance Proposals will be reviewed by the Design Review Board (DRB) and will be reviewed for consistency with the objectives of the design guidelines.



## Urban Design Goals and Objectives

The key design objectives promoted in the Lakeview Neighborhood Plan for the Yarrow Bay Business District include:

- Promote quality architectural and site design.
- Encourage architectural vertical and horizontal modulation along all street frontages and perimeter of district.
- Preserve public scenic views and natural features that contribute to Lakeview's visual identity.
- Provide interconnected street and pedestrian improvements throughout the district that tie the district together, contribute to a sense of identity and enhance visual quality. Included in this concept are pedestrian linkages on site, to adjacent properties, and to transit facilities. Provide directional signs that indicate path locations.
- Enhance streetscapes distinctive to the neighborhood along Lake Washington Blvd., Northup Way and NE 38th Pl with wide sidewalks, street trees, decorative pedestrian lighting, benches, or other street furniture.
- Incorporate gateway features to the neighborhood to strengthen neighborhood identity at the locations identified in the Lakeview Neighborhood Plan.
- Especially along retail uses and streets promote using pedestrian oriented design techniques such as, generous ground floor window treatments, awnings, superior building materials, open space plazas, and pedestrian amenities.
- Locate development away from streams and wetlands. Enhance stream corridors for both habitat and as a natural amenity.

### Vision for the Yarrow Bay Business District

The Lakeview Neighborhood Plan vision for the Yarrow Bay Business District is to transform the large suburban style office park development into a more integrated, mixed use residential and commercial district. Several strategies will help achieve this vision such as allowing a broader range of uses, and improving

pedestrian connections between properties, businesses, the Transit Oriented Development at the South Kirkland Park and Ride facility and the Eastside Rail Corridor. Incorporating public plazas, green spaces and pedestrian amenities into new development will help create an inviting environment for employees, residents



and visitors. New design standards and design review for development will ensure quality architecture, site design and identity for the district.

The Lakeview Neighborhood Plan envisions improvements to NE 38th Pl, Lake Washington Blvd. NE and Northup Way to upgrade the streets with wider sidewalks, street trees and decorative pedestrian lighting, directional signs, benches and varying pavement textures. On-street parking is encouraged along NE 38th Pl to support pedestrian-oriented uses or retail frontage.

## Vision for YBD 1

The YBD 1 zone contains the South Kirkland Park and Ride property, planned for transit-oriented development (TOD). In light of the district's role in fostering a vibrant and desirable community that includes mixed use and promotes the use of transit, design guidelines are established to create a supportive built environment.

The guidelines implement the vision for the area described in the Lakeview Neighborhood Plan. They emphasize coordinated development of the site through high quality building design and pedestrian amenities and connections within and beyond the site. The importance of building orientation and connections to NE 38th Place, and the creation of a welcoming and attractive gateway at the entrance to the district at NE 38th Place and 10.8th Avenue NE are also highlighted in the guidelines.

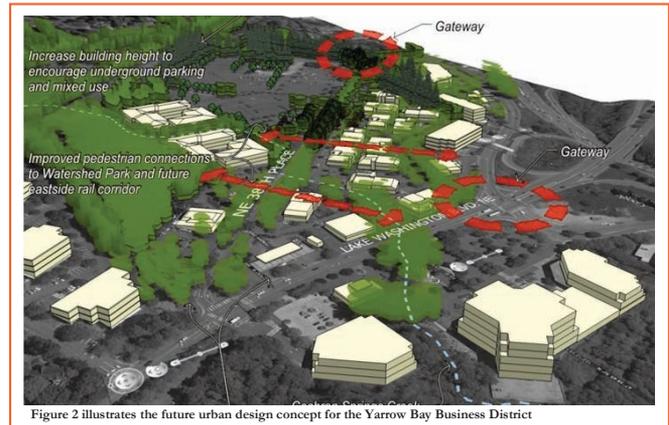
## Vision for YBD 2 and 3

Larger sites within the Yarrow Bay Business District provide opportunities for coordinated development. Within YBD 2 and YBD3, for example, mixed-use developments combining retail, office and residential uses with an attractive face along the major traffic corridors or provide interior vehicular and pedestrian pathways and open space as focal points for pedestrians. Focal points may include plazas surrounded by shops, offices, services or wide sidewalk areas along an interior access street.

Buildings should front along NE 38th Pl. and orient toward Lake Washington Blvd. Storefronts may be clustered around major entry points to the development to provide a welcoming entry. Use of a variety of materials and colors and modulated walls and rooflines is encouraged to reduce architectural scale.

Residential buildings should feature prominent building entries and individual balconies. Orienting residential buildings around a courtyards, plazas, or natural features also is encouraged.

The following design guidelines for the Yarrow Bay Business District (YBD) are intended to help guide the future development toward the vision described in the Lakeview Neighborhood Plan and in this document.



## Design Guidelines for YBD 1

### 1. High Quality Building and Design

Intent: Ensure that buildings contribute to the liveliness of the public spaces, and overall community character.

Guidelines:

- a. Building materials should exhibit permanence.
- b. Building materials and color should be selected to integrate with each other and complement architectural design.
- c. Ornament and applied art should be integrated with the structures and the site environment and not haphazardly applied.
- d. Emphasis should be placed on highlighting building features such as doors, windows, and eaves, and on the use of materials such as wood siding and ornamental masonry. Ornament may take the form of traditional or contemporary elements
- e. Original artwork or hand-crafted details should be considered in special areas.

### 2. Building Scale and Massing

Intent:

- To encourage an architectural scale of development that is compatible with the vision for the districts.
- To add visual interest to buildings.

Guidelines:

- a. Large window areas should be avoided. Instead smaller window units should be used to achieve human scale.



- b. Facing the street, buildings above the 2nd story should use upper story step backs to create receding building forms as building height increases to maintain human scale. A rigid stair step or “wedding cake” approach to upper story step backs is not appropriate. Prescribed upper story step backs in the gateway area at the intersection of NE 38th Place and 10.8th Avenue NE are appropriate to prevent the building from overpowering the gateway design.



- c. Decks and/or balconies should be designed so that they do not significantly increase the apparent mass of the building.
- d. The location of the subject property makes any new multi-story building highly visible from the surrounding streets and properties. Building design should be based on viewpoints or vantages to be identified through the Design Review process. The final arrangement of building mass should therefore address the key vantage points and respond to the context of existing and/or planned improvements, gateway features, and location of plazas and open space.
- e. All building facades should be designed carefully, i.e. there should be no “backside” of a building.
- f. Building facades should be well modulated to avoid blank walls and provide architectural interest.
- g. Landscaping should be used to provide visual interest and help soften building form at appropriate locations, including upper level terraces.
- h. To help moderate the vertical scale of buildings, buildings should incorporate design techniques which clearly define the building’s top, middle, and bottom.  
  
Examples include using a sloped roof and strong eave lines to help define the top; using windows, balconies, and material changes to define a building’s middle; and pedestrian-oriented storefronts, awnings, and use of ‘earth’ materials such as concrete and stone to help define the building’s bottom.
- i. Roof forms should be varied and attractive. Where appropriate, roof forms should also help reinforce the modulation or articulation interval of the building façade.
- j. Roof forms should be designed to screen rooftop mechanical units.
- k. A predominantly flat roof design is discouraged. For portions of the building where a flat roof design is used, architectural details such as eaves, cornices, or other articulation elements should be used to provide interest at the ground level.
- l. Vertical building modulation should be used to add variety by avoiding monotonous design. A technique that may be used is to make large buildings appear to be an aggregation of smaller buildings. Different colors and/or materials may be used to help differentiate between façade planes.

- m. Horizontal building modulation should be used to reduce the perceived mass of a building and to provide continuity at the ground level of large building complexes. Building design should incorporate strong pedestrian-oriented elements at the ground level and distinctive roof treatments. Different colors and/or materials may be used to help differentiate between façade planes.

### 3. Pedestrian Features and Amenities

#### Intent:

- To provide amenities that enrich the pedestrian environment.
- To increase pedestrian activity.

#### Guidelines:

- a. Pedestrian walkways should be placed throughout the site to allow for efficient access between the residential, commercial, transit center uses, and adjacent streets. The walkways should be situated to minimize walking distance from the public sidewalk and transit facilities to building entrances.
- b. Pedestrian and bicycle pathways and/or connections should be well-defined and safe.
- c. Pedestrian connections should be provided to adjacent properties to allow for efficient access to the transit facilities and commercial uses.
- d. Landscaping should be used to help define and provide visual interest along pedestrian walkways.
- e. Convenient and safe pedestrian areas should be designed in centralized locations to accommodate transit users.
- f. Lighting should be provided to walkways and sidewalks through building mounted light and canopy or awning mounted lights.
- g. Low level lighting in the form of bollards or similar style of lighting should be encouraged along pedestrian pathways not adjacent to buildings.
- h. Through-block pathways should be designed so that it is clear that access by the general public is allowed.

The following guidelines also apply:

1. Because the subject property is steep along NE 38th Place, stairways may be used in the design of the through-block pathway where connecting to the street. If located along NE 38th Place, the stairway should function as a focal entry/exit point and contain design



elements that make it a welcoming, safe, and attractive entry.

- 2 If located within a parking lot, the following guidelines should be incorporated into the design of the through-block pathway:



- a. Increased landscaped island size adjoining the pathway. This helps to narrow the driveway width where appropriate to help slow parking lot traffic.
  - b. Raised landscape beds.
  - c. Raised pathway with pavement material, texture, and color different from traffic lanes.
  - d. Selection of tree species that provide the broadest canopy possible to produce a dense landscaped environment.
3. If the through-block pathway is located between buildings, appropriate plants and trees should be selected based on solar access and the location of proposed improvements.

enough space for decks, balconies, and other activities overlooking the street.

#### 4. Streetscape

Intent: Maintain a continuous and safe streetscape with a pedestrian-friendly character.

##### Guidelines:

- a. Street trees species should be selected and spaced to allow for visual continuity along NE 38th Place, buffer pedestrians from the street, and provide visibility of ground floor retail uses.
- b. Buildings should be oriented towards the street when located along NE 38th Place.
- c. Design elements such as multiple storefronts, pedestrian-oriented signs, exterior light fixtures, glazing, landscaping, and awnings should be utilized to add human scale and interest at the street level.
- d. Ground floor spaces along NE 38th Place should be transparent with windows of clear vision glass beginning no higher than 2' above grade to at least 10' above grade. Windows should extend across, at a minimum, 75% of the façade length. Continuous window walls should be avoided by providing architectural building treatments, mullions, building modulation, entry doors, and/or columns at appropriate intervals.
- e. Varied window treatments should be encouraged. Architectural detailing at window jambs, sills, and heads should be emphasized. Use of ribbon windows should be avoided.
- f. A street wall is a wall or portion of a wall of a building facing a street. Continuous street walls should incorporate vertical and horizontal modulations into the building form.
- g. Along pedestrian oriented streets, upper story building facades should be stepped back to provide



- h. Awnings or canopies should be required on facades adjoining sidewalks. Blank walls should be avoided near sidewalks, open spaces, and pedestrian areas.
- i. Blank walls should not be visible from the street or sidewalk. Where blank walls are unavoidable, they should be treated with landscaping, art, or other architectural treatments.

well organized.

- d. Screening and landscaping should be used to reduce the visual impact of parking lots and/or parking structures to the surrounding neighborhood.
- e. Parking structures shall be designed and located to obscure the view of parked cars from adjacent properties. Parking structures should be located to the back of buildings or underground with intervening uses.

## 5. Gateways

### Intent:

- To enhance the character and identity of the Lakeview Neighborhood.
- To provide a welcoming statement for visitors entering the City.

### Guidelines:

- a. A gateway is an urban design feature that signifies a sense of place and arrival into a city or neighborhood. A gateway should be designed in the location shown in the Comprehensive Plan. The design elements of the gateway should include a combination of landscaping, architectural features, and artwork which:
  1. Contain a highly visible and welcoming public space between the sidewalk and the building which is easily accessible, comfortable, safe, and includes pedestrian amenities;
  2. Establish a landmark that reflects the TOD elements of the site;
  3. Reinforce NE 38th Place and 10.8th Avenue NE as a focal point;
  4. Transition between Kirkland and Bellevue and the Yarrow Bay Business District to the west; and
  5. Are integrated with the TOD building design

## 6. Parking Facilities

Intent: Mitigate the visual impact and intrusive qualities of parking garages in pedestrian areas.

### Guidelines

- a. Parking areas should not be located between NE 38th Place and buildings.
- b. Access driveways to parking areas should be minimized.
- c. Parking lots should be designed to provide for clear vehicular and pedestrian circulation and be



- f. Portions of parking structures visible from the street that cannot be placed behind an intervening use due to site topography, should be constructed with high quality materials and be architecturally compatible with the character of surrounding buildings.
- g. Architectural treatment, artwork, building setbacks, and/or dense landscaping should be used to further reduce the visual impact of parking structures along the street.
- h. If adjacent to the required gateway, the exterior of a parking structure should reflect the design elements of the gateway. Design should avoid the appearance of a parking structure.

## 7. Public Amenities and Open Space

Intent: Mitigate the visual impact and intrusive qualities of parking garages in pedestrian areas.

### Guidelines

- a. Public open space should be provided on the subject property which can be used by the general public, residents, and transit users.
- b. Public open space should be open to the sky except where overhead weather protection is provided (e.g. canopies and awnings). The space should appear and function as public space rather than private space.
- c. Public open space should be located in close proximity to commercial and retail uses that are required along NE 38th Place. The public open space should be well defined and contain amenities such as outdoor dining, seating areas, art, water features, and/or landscaping. Adequate room for pedestrian movement through the space should be maintained. Additional public open space in a location convenient to the site's transit users may also be appropriate.
- d. Careful attention should be paid to the transition between transit operations and the building to create a well defined pedestrian space such as a small plaza with landscaping features.
- e. A combination of lighting, access to sunlight, paving, landscaping, and seating should be used to enhance the pedestrian experience with the public open space.

## Design Guidelines for YBD 2 and 3

### 1. Building Location and Orientation

### Objectives-Intent

- To enhance the character and identity of the Yarrow Bay Business District.



- To upgrade the appearance of streets in the Yarrow Bay Business District.
- To enhance pedestrian circulation.
- To create focal points, particularly on large sites

- To enhance pedestrian and vehicular safety.

### Guidelines

- Locate and orient buildings toward sidewalks along streets.
- Within interior portions of sites orient buildings to plazas, common open spaces or major internal pedestrian pathways.
- Where buildings are located at the sidewalk with direct pedestrian access, provide pedestrian oriented building façade treatments described in the Pedestrian Friendly Facades Section 9.
- Provide landscaping, plazas or building façade treatments to enhance the pedestrian experience. In general, buildings that have less pedestrian orientation will merit more landscaping and façade treatments to prevent blank walls.
- Locating parking to the side and/or rear of buildings is preferred.
- Configure development to provide focal points and opportunities for coordinated pedestrian and vehicular access. Where there are no current opportunities for coordinated access provide the opportunity for future coordination should adjacent site redevelop in the future.

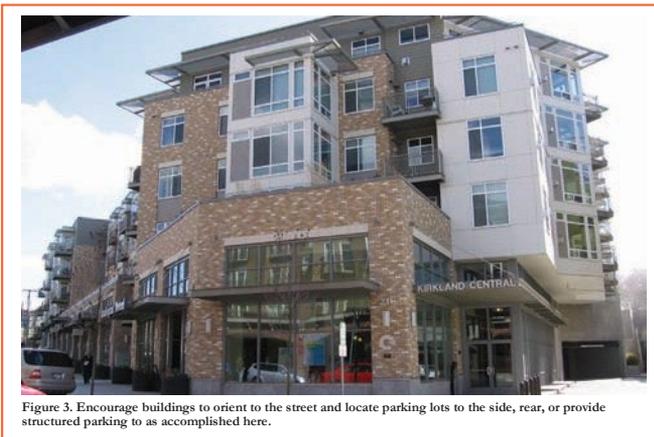


Figure 3. Encourage buildings to orient to the street and locate parking lots to the side, rear, or provide structured parking to as accomplished here.

## 2. Parking Lots and Vehicular Circulation

### Objectives Intent

- To minimize the impact of parking facilities on the fronting street, pedestrian environment, and neighboring properties.



- To maintain traffic flow on streets.
- To promote shared parking.
- To provide attractive and connected vehicular circulation routes.

## Discussion

Parking lots can detract from the pedestrian and visual character of a commercial area. The adverse impacts of parking lots can be mitigated through sensitive design, location, and configuration. Large parking lots can be confusing unless vehicle and pedestrian circulation patterns are well organized and marked.

## Guidelines

### Driveways

- Minimize the number of driveways into a development along Lake Washington Blvd, NE 38th Pl. and Northup Way. To the extent possible, adjacent developments should share driveways.

### Parking Lot Location and Design

- Surface parking lots are discouraged. Where they are provided, locate parking to the side or rear of buildings so it is not between a building and the sidewalk.
- Avoid parking layouts that visually dominate a development. Design parking lots to be attractive to pedestrian's walking by and to break up large parking lots into smaller ones.
- Provide a clear and well organized parking lot design. Space should be provided for pedestrians to walk safely in all parking lots.

### Parking Lot Landscaping and Screening

- Integrate landscaping into parking lots to reduce their visual impact. Provide planting beds with a variety of trees, shrubs, and ground cover to provide visual relief, summer shade, and seasonal interest.
- Provide low level perimeter landscaping where parking is adjacent to sidewalks in order to maintain a visual screen and reduce clutter. Use screening methods that maintain visibility at eye level between the street and parking area.
- Provide extensive screening and landscaping between parking lots, residential uses, and open spaces. A combination of a screen wall with a landscape buffer is preferred.



Figure 4 A good example of incorporating trees between parking lot and along pedestrian paths in front of uses.



Figure 5 An example of interior parking lot landscaping.

## 3. Parking Structures

### Objective Intent

- To mitigate the visual impacts of parking structures in the urban environment.

### Guidelines

- Structured parking garages are preferred over surface parking lots provided they can be designed to mitigate the intrusive qualities of parking garages along streets, pedestrian pathways and in pedestrian areas using the following design techniques:
  - Locate parking structures, service areas, and storage away from the street edge and so they are not visible from the street or sidewalks.
  - Incorporate ground-level commercial space, oriented to the adjacent street, into parking structures.
  - Use landscaping to screen the parking garage façade.
  - Design and locate parking garage entries to complement, not subordinate the pedestrian entry. Where possible, locate the parking entry away from the primary street, to either the side or rear of the building.
  - Use architectural forms, materials, and/or details to integrate parking structure with the design of other buildings on the property.
  - Locate and design parking structures to obscure the view of parked cars from adjacent properties.



Figure 6. This parking garage includes street front retail space and landscaped trellises to mitigate visual impacts on the streetscape



## 4. Architectural Scale

### Objective Intent

- To encourage an architectural scale of development desired for each zone within the Yarrow Bay Business District.
- To add visual interest to buildings.

### Discussion

“Architectural scale” means the size of a building relative to the buildings or elements around it. When the buildings in a neighborhood are about the same size and proportion, we say they are “in scale.” The vision and development regulations for the Yarrow Bay Business District provide for larger buildings than currently exist. Care must be taken to design buildings so they appropriately respond to the evolution of the District from the current low rise condition to its vibrant mixed use future. For example, a new project need not step down to a one story edge condition to acknowledge an existing one story building on an adjoining site, but it can incorporate horizontal and vertical modulation that allow it to “fit” with the existing context and provide cues for future development of the adjoining site.

### Guidelines

A combination of techniques to reduce the architectural scale of buildings is important. In general the following techniques should be included at intervals of 70. feet for office uses and 30 feet for residential uses. Alternatives will be considered if they meet the objectives.

- a. Incorporate fenestration techniques proportionate in size and pattern for the scale of the building. This is particularly important on upper floors, where windows should be divided into individual units with each window unit separated by a visible mullion or other element. “Ribbon windows” (continuous horizontal bands of glass) or “window walls” (glass over the entire surface) do little to indicate the scale of the building and are thus discouraged, except in special circumstances where they serve as an accent element.
- b. Encourage vertical modulation on multi-story buildings to add variety. Vertical modulation may be particularly effective for tall buildings adjacent to a street, plaza, or residential area to provide compatible architectural scale and to minimize shade and shadow impacts.

- c. Incorporate horizontal building modulation techniques to reduce the architectural scale of the building and add visual interest. Horizontal building modulation is the horizontal articulation or division of an imposing building façade through upper story setbacks, awnings, balconies, roof decks, eaves, and banding of contrasting materials. Elevations that are modulated with horizontal elements appear less massive than those with sheer, flat surfaces.

Recommended horizontal building modulation techniques include:

- Roofline modulation and a change in building materials.
  - Step back building facades, generally above the second floor.
  - For residential uses, provide horizontal building modulation based on individual unit size, use roofline modulation, and changes in color and/or building materials. The depth and width of the modulation should be sufficient to meet the objectives of the guidelines. Avoid repetitive modulation techniques, since they may not be effective when viewed from a distance. Larger residential buildings will require greater horizontal modulation techniques to provide appropriate architectural scale.
- d. Break up long continuous walls with a combination of horizontal building modulation, change in fenestration, and/or change in building materials. This is especially important for office buildings.
  - e. Encourage a variety of roofline modulation techniques such as hipped or gabled rooflines and modulated flat rooflines. As a general rule, the larger the building or unbroken roofline, the bigger the modulation should be. In determining the appropriate roof type and amount of modulation, consider the distance from which the building can be viewed. For example, a large commercial building adjacent to a parking lot is capable of being viewed from a relatively large distance and will consequently necessitate greater roofline modulation.





Figure 7. Fenestration and vertical modulation techniques help to reduce the architectural scale of this office building. Figure 8. A variety of techniques should be used for multi-tenant retail buildings to emphasize individual storefronts.

## 5. Human Scale

### Objectives Intent

- To encourage the use of building components that relate to pedestrian activity.
- To add visual interest to buildings.

### Discussion

The term “human scale” is generally used to indicate a building’s size in proportion to pedestrians. The actual size of a building is often not as important as its perceived size. A variety of design techniques may be used to make a building less imposing and to make people feel comfortable using and approaching it.

How the pedestrian interacts with the building at street level, along store fronts and portions of the building that are within view and reach of the pedestrian are most important factors. Upper story setbacks can also prevent taller structures from overwhelming the pedestrian scale at the street level.

The use of materials, detailing, and transparency of windows along a building façade are important techniques. A bay window suggests housing, while an arcade suggests a public walkway with retail frontage. Each element must be designed for an appropriate urban setting and for public or private use. A building should incorporate special features that enhance its character and surroundings. Such features give a building a better defined “human scale.”

### Guidelines

- Encourage a combination of architectural elements that give buildings a human scale. Examples include arcades, balconies, bay windows, roof decks, trellises, landscaping, awnings, cornices, friezes, art concepts, street front courtyards and plazas outside of retail spaces. Window fenestration techniques described in Section 4 can also be effective. Consider the distances from which buildings can be viewed (from the sidewalk, street, parking lot, open space, etc.).



Figure 9. Use of building modulation, window patterns, brick, balconies and awnings help lend this building a human scale.

## 6. Pedestrian Connections

### Objectives Intent

- To provide convenient pedestrian access.
- To reduce vehicle trips.
- To encourage pedestrian activity.

### Guidelines

- Provide convenient pedestrian access between the street, bus stops, buildings, parking areas, and open spaces.
- Provide direct pedestrian access from buildings to abutting public sidewalks and major internal pathways.
- Provide paved walkways through large parking lots. Separate walkways from vehicular parking and travel lanes by use of contrasting paving material which may be raised above the vehicular pavement and by landscaping.
- Provide safe and convenient pedestrian connections east to west through the business district consistent with Plate 34 of the Zoning Code.
- Consider installing a public trail along the stream corridor as a pedestrian connection and natural amenity.



Figure 10. Provide landscaped pathways through large parking lots



## 7. Natural Features

- **Objectives Intent** Establish a “greenway” corridor extending in an east/west direction across the business district from the Yarrow Bay wetlands and along stream corridors to the Houghton slope.

### Guidelines

- a. Configure buildings and site features to preserve and enhance stream corridors. Consider these natural features as open space amenities.
- b. Use wooded slopes as a natural site amenity and buffer by using and retaining native vegetation.
- c. Encourage buildings and rooflines to step down or be tucked against hillsides to roughly follow the slope of the existing terrain.



Figure 11. Seek opportunities to expand the existing public pedestrian pathway.

## 8. Blank Walls

### Objectives Intent

- To minimize visible blank walls.
- To enhance public safety along sidewalks and pathways.
- To encourage design elements that enhance the character of buildings at all perceived distances.

### Discussion

Blank walls deaden the pedestrian environment and break the continuity of ground floor activity along a street or pathway. Blank walls can also create a safety problem, particularly where adjacent to pedestrian areas, as they don't allow for natural surveillance of those areas.

### Guidelines

- a. Avoid blank walls near sidewalks, major internal walkways, parks, and pedestrian areas. Use the following treatments to mitigate the negative effects of blank walls (in order of preference):

- Configure buildings and uses to avoid blank walls exposed to public view.
- Provide a planting bed with plant material to screen most of the wall.
- Install trellises with climbing vines or plant materials to cover the surface of the wall. For long walls, use trellises to avoid monotony.
- Provide artwork on the wall surface.
- Provide architectural techniques that add visual interest at a pedestrian scale, such as a combination of horizontal building modulation, change in building materials and/or color, and use of decorative building materials.
- Provide decorative lighting fixtures.



Figure 12. An example of treatment for blank wall or parking structure.



Figure 13. This building was a combination of alternating building materials, details, and landscaping elements to add visual interest at a close range.

## 9. Pedestrian-Friendly Building Fronts

### Objectives Intent

- To enhance the pedestrian environment.
- To create safe and active sidewalks and pathways.

### Guidelines

- a. Incorporate transparent windows, pedestrian entrances, and weather protection along facades adjacent to a sidewalk or internal pathway. Weather protection features could include awnings, canopies, marquees, or other similar treatments.
- b. Where buildings are not located at the sidewalk, incorporate landscaping, a pedestrian plaza or open space between the building and the sidewalk or provide building façade treatment.



Figure 14. An example of pedestrian friendly building façade.



## 10. Pedestrian Plazas

### Objectives Intent

- To provide a variety of pedestrian-oriented areas to attract shoppers and employees to commercial areas and enrich the pedestrian environment.
- To create gathering spaces for the community.
- To configure buildings to encourage pedestrian activity and pedestrian focal points.

### Discussion

Pedestrian plazas serve as open space and places for people to gather.

### Guidelines

- Provide pedestrian plazas in conjunction with building and site spaces that are accessible to the general public, residents and transit users.
- Position plazas in locations adjacent to and visible from major streets, such as along NE 38th Pl, major internal circulation routes, or where there are strong pedestrian flows on neighboring sidewalks. For large sites, development should be configured to create one or more focal plazas. To enhance visibility and accessibility, plazas usually should be no more than 3' above or below the adjacent sidewalk or internal pathway.
- Locate building entrances that open on to plazas.
- Provide landscaping elements that add color and seasonal interest. This can include trees, planting beds, potted plants, trellises, and hanging plants.
- Incorporate pedestrian amenities, as described in Section 12.
- Locate plazas in sunny locations.
- Provide transitional zones along building edges to allow for outdoor seating areas and a planted buffer.



Figure 15. Good examples of pedestrian plazas. Notice the decorative pavements, landscaping components, adjacent building facades, and other amenities and design details

## 11. Residential Open Space

### Objectives Intent

- To create useable space that is suitable for leisure activities for residents.
- To create open space that contributes to the residential setting.

### Guidelines

- Incorporate common open space for use by residents. Guidelines for common open space include:
  - Design space as a focal point of the development.
  - Space may be provided in one large area or in multiple smaller spaces, provided that each space is large enough to provide functional leisure activity. For example, long narrow spaces rarely function as usable common space.
  - Provide space for a range of activities and age groups. Children's play areas should be visible from dwelling units and positioned near pedestrian activity.
  - Separate common space from ground floor windows, streets, service areas, and parking lots with landscaping and/or low-level fencing. However, care should be used to maintain visibility from dwelling units towards open space for safety.
- Provide private open space for individual residential units. For townhouses and other ground-based housing units, provide patios, decks, and/or landscaped front or rear yards adjacent to the units. For all other units, provide balconies large enough for usable space for sitting, outdoor cooking and eating etc.



Figure 16. Good examples of common open space, including internal courtyard (left), a children's play area (right).



## 12. Pedestrian Amenities

- Objectives Intent To provide amenities that enrich the pedestrian environment.
- To increase pedestrian activity.

### Discussion

Site features and pedestrian amenities, such as lighting, benches, paving, waste receptacles, and other site elements, are an important aspect of a business district's character. These elements reduce apparent walking lengths and unify the district's visual character.

### Guidelines

- Provide pedestrian amenities along all sidewalks, interior pathways and within plazas and other open spaces. Examples include:
  - Pedestrian-scaled lighting less than 15' above the ground.
  - Seating space such as benches, steps, railings and planting ledges. Ideal heights are between 12" to 18". An appropriate seat depth ranges from 6" to 24".
  - Pedestrian furniture such as trash receptacles, consolidated newspaper racks, and drinking fountains.
  - Planting beds and/or potted plants.
  - Unit paving such as stones, bricks, or tiles.
  - Decorative pavement patterns and tree grates.
  - Water features.
  - Informational kiosks.
  - Transit shelters.
  - Decorative clocks.
  - Artwork.
  - Bicycle racks.



Figure 17. Consolidated newspaper racks



Figure 18. Bicycle racks



Figure 19. Potted plants



Figure 20. A sculptural water feature with landscaping



Figure 21. Informational kiosk (left), benches and pedestrian-scale lighting (right).



## 13. Pedestrian Coverings

### Objectives Intent

- To provide shelter for pedestrians.
- To provide spatial enclosure and add design interest to a retail or office streetscapes.

### Discussion

The design and width of pedestrian coverings should be determined by their function, the building's use and the type of street.

As a general rule, the more traffic an entry is expected to accommodate, the larger the covered area at the entry should be.

The width of the sidewalk should also be considered when sizing the pedestrian covering (wider sidewalks can accommodate wider pedestrian coverings). Canopies and awnings should be appropriately dimensioned to allow for tree growth, where applicable. The architecture of the building and the spacing of individual storefronts should help determine the appropriate placement and style of

the canopy or awning. Continuous, uniform awnings or canopies, particularly for multi-tenant retail buildings, can create a monotonous visual environment and are discouraged.

### Guidelines

- Provide weather protection along the primary exterior entrance of all businesses, residential units, and other buildings.
- Design weather protection features to provide adequate width and depth at building entries.
- Pedestrian covering treatments may include: covered porches, overhangs, awnings, canopies, marquees, recessed entries or other similar features. A variety of styles and colors should be considered and be compatible with the architectural style of the building and the ground floor use.
- Back lit, plastic awnings are not appropriate.



Figure 22. Wider pedestrian coverings allow for outdoor dining



Figure 23. Awnings integrated into building's storefront spaces



# 14. Building Details and Materials

## Objectives Intent

- To use building and site design details that add visual interest to buildings/sites at a pedestrian scale.
- To use a variety of quality building materials such as brick, stone, glass, timber, and metal appropriate to the Pacific Northwest climate.

## Guidelines

- a Encourage the integration of ornament and applied art with structures and site environments. For example, significant architectural features should not be hidden, nor should the urban context be overshadowed.

Emphasis should be placed on highlighting building features such as doors, windows, eaves, and ornamental masonry. Ornament may take the form of traditional or contemporary elements. Original artwork or hand-crafted details should be considered in special areas. Ornament may consist of raised surfaces, painted surfaces, ornamental or textured banding, changing of materials, or lighting.

- b Use a variety of quality building materials such as brick, stone, timber, and metal to add visual interest to the buildings and reduce their perceived scale. Use masonry or other durable materials - especially near the ground level.
- c Avoid use of concrete block and large expansive tilt up concrete facades.



# 15. Entry Gateway Features

## Objectives Intent

- To enhance the character and identity of the Lakeview Neighborhood.
- To provide a welcoming statement for visitors entering the City.

## Discussion

The Lakeview Neighborhood Plan calls for gateway features at two key entry points into neighborhood and the Yarrow Bay Business District:

- Intersection of SR 520. and Lake Washington Blvd. NE
- Intersection of 10.8th Avenue NE and NE 38th PL at the Transit Oriented Development.

## Guideline

- a Incorporate entry gateway features in new development in the vicinity of gateways/ nodal intersections identified in the Lakeview Neighborhood Plan. Locate and provide a new design for a gateway sign on Lake Washington Blvd. Gateway features may include some or all of the following:
  - Distinctive landscaping.
  - Artwork (e.g. vertical sculpture incorporating historical information about the Lakeview Neighborhood).
  - Decorative lighting elements.
  - Distinctive architectural features that are unique to the neighborhood or provide open space.
  - Incorporation of the Cochrane Springs Creek crossing into a gateway feature as a soft, green entrance to the City.



# 16. Sidewalk and Pathway Widths

## Objectives Intent

- To provide wide sidewalks and pathways that accommodates pedestrian movement and provides a pleasing pedestrian experience.

## Discussion

Sidewalks have three overlapping parts with different functions: the curb zone, the movement zone, and the storefront or activity zone. A well-sized and uncluttered



movement zone allows pedestrians to move at a comfortable pace.

Sidewalks or pathways adjacent to moving vehicular traffic need generous buffers to make them safer and more inviting. Landscaping elements are particularly important physical and visual buffers between walkways and streets or other vehicle access areas. As a general rule, the higher the travel speed, the greater the buffer should be between moving cars and pedestrians.

### Guidelines

- Integrate a “curb zone” into the sidewalk or pathway width to separate the pedestrian from the street. This space should include street trees in a landscape strip or tree grates. Subtle changes in paving patterns between the curb zone and the movement zone can be effective and should be considered.
- Design sidewalks and pathways to support a variety and concentration of activities and provide a separation for the pedestrian from the busy street. Provide decorative pedestrian lighting and amenities described in the pedestrian amenities section below.
- For the movement or storefront activity zone design sidewalks to be wide enough to allow for pedestrians to pass those window shopping or seated at sidewalk cafes.

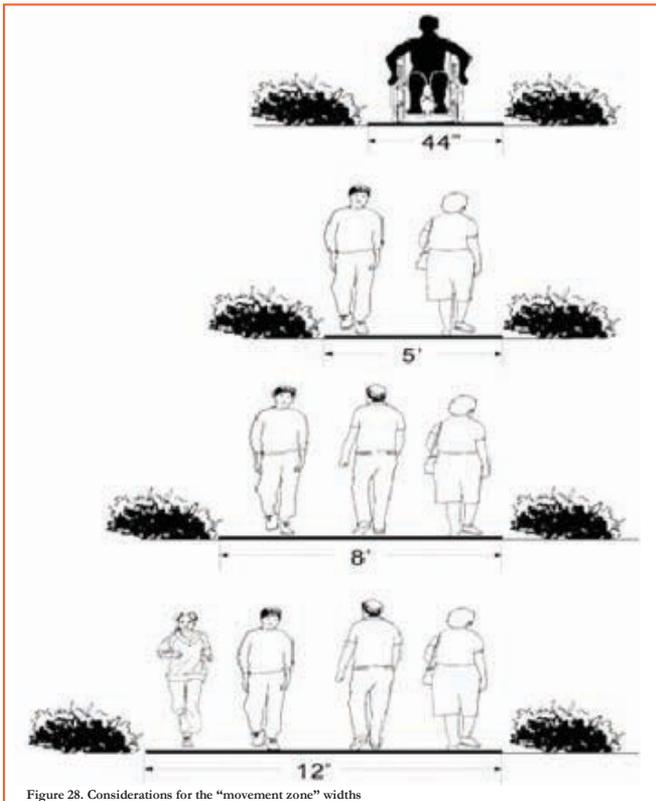


Figure 28. Considerations for the “movement zone” widths



Figure 29. High-traffic streets without on-street parking warrant wider planting strip buffers

## 17. Street Trees

### Objectives Intent

- To utilize street trees to upgrade the character and identity of the Yarrow Bay Business District.
- To enhance the pedestrian environment in the Business District.

To use trees that provide seasonal interest.

To use trees that will not obscure views of businesses from the street.

### Discussion

The repetition of trees bordering streets, internal roadways, and pathways can unify the District. Trees can add color, texture, and form to the urban environment and provide a respite from the weather.

### Guidelines

- Incorporate street trees along all streets, internal access roads, and pathways.
- Encourage street trees to be used as a unifying features for the District.
- Select and maintain tree species that will accommodate pedestrian and vehicular traffic, and maintain visibility into and through sites for safety purposes.



Figure 30. Provide street trees along all streets and internal access roads



## 18. Landscaping

### **Objectives Intent**

- To enhance the visual quality of the urban environment.
- To incorporate greenery into the urban environment.

### **Discussion**

Landscaping can soften the hard edges and improve the visual quality of the urban environment. Landscaping treatment in the urban environment should focus on the automobile, pedestrian, and building landscapes.

Along high speed and high volume traffic areas, raised planting strips can be used to protect pedestrians from traffic. The pedestrian landscape should offer variety at the ground level through the use of shrubs, ground cover, and trees. Pedestrian circulation, complete with entry and resting points, should be emphasized. Landscaping around buildings particularly along blank walls can reduce scale and add diversity through pattern, color, and form.

Examples of how landscaping is used to soften and enhance the visual quality of the urban environment include:

- Screening of parking lots;
- Tall cylindrical trees to mark an entry;
- Continuous street tree plantings to protect pedestrians;
- Clusters of dense trees along long building facades;
- Cluster plantings at focal points;
- Parking lots with trees and shrubs planted internally as well as on the perimeter.

### **Guidelines**

- a. Design landscaping for the purpose and context in which it will be located. The auto oriented landscaping requires strong plantings of a structural nature to act as buffers or screens for pedestrians. The pedestrian landscape should emphasize the subtle characteristics of the plant materials. The building landscape should use landscaping that complements the building's qualities and screens service areas or blank walls while not blocking views of the business or signage.
- b. Encourage a colorful mix of drought tolerant and low maintenance trees, shrubs and perennials. Except in special circumstances, ivy should be avoided.

- c. Consider the on-site topography to hide parking and enhance views.
- d. Use wooded slopes and streams as a natural site amenity and to screen unwanted views, where applicable.

## 19. Service Areas

- **Objectives Intent** To mitigate adverse impacts of service areas.
- To locate and design site service and storage areas to promote ease of use, safety, and visual cohesion.

### **Guidelines**

- a. Locate and design service and storage areas (such as refuse, recycling, loading or mechanical equipment areas) to minimize visibility from public pedestrian spaces and adjacent properties.
- b. Locate service elements where they are accessible to service vehicles and convenient for tenant use.
- c. Design service enclosures to be compatible with the design of adjacent buildings. This may be accomplished by the use of similar building materials, details, and architectural styles. Such enclosures should be made of masonry, ornamental metal, heavy wood timber, or other durable materials.
- d. Locate roof-mounted mechanical equipment so as not to be visible from the street, public open space, parking areas, or from the ground level of adjacent properties. Equipment screening should blend with the architectural character of the building.
- e. Consider the location and screening of mechanical equipment and service areas early in building and site design.

## 20. Lighting

### **Objectives Intent**

- To enhance safety by providing light levels sufficient to adequately illuminate pedestrian areas and building facades.
- To create inviting pedestrian areas using a variety of illumination techniques.
- To provide adequate lighting without creating excessive glare or light levels.



## Discussion

Overpowering and uniform illumination in commercial areas creates glare and destroys the quality of night light especially adjacent to residential areas. Well placed light fixtures provide sufficient lighting levels for security and safety as well as create a positive ambience. A blend of lighting directed downward on walking surfaces and up onto trees can be used to define these spaces.

Care should be taken to avoid spilling excessive glare into adjacent properties and to avoid sky directed light pollution. For this reason utilizing fixtures that shield the light source as much as possible is prudent. In some instances highlighting light fixtures by allowing the light source to be seen is appropriate; however, care should be taken to diffuse the light source to not cause excessive glare. Overall, it is desirable to have different intensities and balances of light to create well defined and comfortable outdoor places.

## Guidelines

- a. Provide adequate lighting levels in all areas used by pedestrians and automobiles, including building entries, walkways, parking areas, circulation areas, and open spaces.

Recommended minimum light levels:

- Building entries: 4 foot candles. This can be a combination of up lighting and down lighting and building mounted lighting. Care should be taken to emphasize the importance of the building entrance.
- Primary pedestrian walkway: 2 foot candles lighting can be ambient light from canopies and building mounted lighting. Emphasis should take place on creating pools of lighting on the pedestrian surface. Lighting sources should not be seen except when lighting is designed as part of a theme for the overall building such as in some sort of lamp lighting.
- Secondary pedestrian walkway: 1-2 foot candles. Focus should be on pooling lighting on the walking surface and hiding the light source. Different levels of lighting should occur at focus or gathering points to provide destinations along pedestrian walkways.
- Parking lot: .60, -1 foot candle
- Enclosed parking garages for common use: 3 foot candles

- b. Provide lighting for walkways and sidewalks through building mounted lights, canopy or awning mounted lights, and display windows. Building-mounted light fixtures are encouraged to give visual variety and provide interest. It is acceptable to use the building mounted light as a piece of visual artwork unto itself. Care should be taken to prevent as much direct glare as possible from the light source. Canopies or awning-mounted lights are not to illuminate a glowing canopy. Canopies and awning lights should be directed onto walking surfaces, on the building façades or directed up under the canopy. Window display ambient light can spill onto the walkways and sidewalk.
- c. Provide parking lot light fixtures that are non-glare. Lower level lighting fixtures in a design that is coordinated with the architecture of the building are preferred. Lights up to 20.' in height may be used for safety and security when needed. However, the light source shall not be seen beyond an approximate 20-degree angle from the light fixture itself.
- d. Prohibit flood illumination of building facades. Some directed façade lighting maybe appropriate when coordinated with the design theme of the building.



## 21. Signs

### Objectives-Intent

- To encourage the use of creative, well-crafted signs that will contribute to the character of the district while providing adequate identification for buildings and tenants.



## Discussion

Kirkland's Zoning Code regulates signs throughout the city in order to create a high-quality urban environment. The type and design of a sign will vary, depending on if it is geared toward the passing motorist, pedestrians or a commercial center. Signs should be an integral part of a building's façade or site design. The location, architectural style, and mounting of signs should conform to a building's architecture and not cover up or conflict with its prominent architectural features. A sign's design and mounting should be appropriate for the setting.

## Guidelines

- a. Provide pedestrian oriented signs on all commercial facades where adjacent to a sidewalk or walkway. This includes signs located within 15' of the ground plane, such as "blade" signs which hang below canopies. Small signs located on canopies or awnings are also effective along building facades at the street. Sculpted signs and signs that incorporate artwork add interest.
- b. Prohibit internally lit cabinet signs. Neon signs are appropriate when integrated with the building's architecture.
- c. For ground mounted signs provide substantial sign bases in proportion to the sign face and install low level landscaping around the sign base.
- d. Use mounting supports for signs that reflect the materials and design character of the building or site elements or both. Too much variety, too much uniformity though unified by common design elements, signs can still express the individual character of businesses.
- e. Provide master sign plans for larger commercial centers to combine signage for the whole complex that describes the general location for signs, complements the architectural design of the center and signs oriented to automobile traffic.



Figure 32. Good example of a sign geared to automobiles for a multi-use development.

