

RESOLUTION R-5545

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF KIRKLAND
ADOPTING STREET STANDARDS TO BE APPLIED WITHIN THE HOLMES
POINT OVERLAY ZONE

1 WHEREAS, on July 21, 2009, in anticipation of a major
2 annexation, called the "Juanita—Finn Hill—Kingsgate Annexation," the
3 City Council enacted Ordinance 4196 to establish pre-annexation zoning
4 for the area proposed to be annexed; and
5

6 WHEREAS, the Finn Hill area was included in the above-
7 referenced annexation, and within Finn Hill is a neighborhood called
8 Holmes Point; and
9

10 WHEREAS, Ordinance 4196 established a new chapter 70 in the
11 *Kirkland Zoning Code*, entitled the "Holmes Point Overlay Zone"; and
12

13 WHEREAS, on December 15, 2009, the City Council enacted
14 Ordinance 4229, which set an effective date for annexation of the
15 Juanita—Finn Hill—Kingsgate area to the City; and
16

17 WHEREAS, the City worked with the Finn Hill community to
18 create the Finn Hill Neighborhood Plan, which was adopted by the
19 Council on January 16, 2018, via Ordinance 4636; and
20

21 WHEREAS, within the Finn Hill Neighborhood Plan is policy FH-
22 14.3, which states in part that the City is to conduct studies to determine
23 street standards for the Holmes Point Drive corridor and the residential
24 streets in the Holmes Point Overlay area; and
25

26 WHEREAS, working with the Holmes Point community, the City
27 and its consultant have completed the "Holmes Point Overlay Zone
28 Street Design Standards & Holmes Point Drive Corridor Study" (the
29 "Study") which was reviewed with the City Council during its May 17,
30 2022 Regular Meeting; and
31

32 WHEREAS, the Council made amendments to the Study and
33 directed staff to investigate the cost of certain entry area improvements;
34

35 WHEREAS, said amendments have been made and staff will
36 return to the Council with information about the entry area
37 improvements as part of the update of the Capital Improvement Plan;
38

39 NOW, THEREFORE, BE IT RESOLVED by the City Council of the
40 City of Kirkland as follows:
41

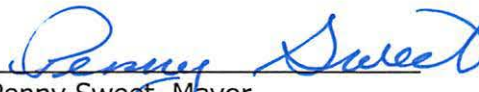
42 Section 1. The City Council of the City of Kirkland hereby adopts
43 the "Holmes Point Overlay Zone Street Design Standards & Holmes Point

44 Drive Corridor Study," attached hereto as **Exhibit 1**, and directs staff
45 to apply it for development and redevelopment projects within the
46 Holmes Point Overlay Zone.

47
48 Section 2. Staff will take the administrative steps to incorporate
49 the provisions of the Study into the City's Pre-approved Plans
50 maintained by the Department of Public Works.

51
52 Passed by majority vote of the Kirkland City Council in open
53 meeting this 21 day of June, 2022

54
55 Signed in authentication thereof this 21 day of June, 2022.


Penny Sweet, Mayor

Attest:


Kathi Anderson, City Clerk



April 2022— Deliverable for Review

Holmes Point Overlay Zone Street Design Standards & Holmes Point Drive Corridor Study





Table of Contents

<i>Introduction and Purpose of the Study</i>	2
<i>Study Area Context</i>	3
<i>Methodology and Process</i>	4
<i>Review of Background Materials and Guiding Documents</i>	4
<i>Summary of Field Work and Existing Conditions</i>	5
<i>Public and Neighborhood Engagement Process</i>	10
<i>Holmes Point Recommendations by Location</i>.....	11
<i>Holmes Point Drive Recommendations</i>	11
<i>Street Typologies</i>	11
<i>Entry Areas</i>	12
<i>Residential Areas</i>	17
<i>OO Denny Park Frontage</i>	24
<i>Neighborhood Streets Recommendations</i>	33
<i>Street Typologies</i>	34
<i>Sidewalk, Curb, and Gutter Street Connections</i>	35
<i>Streets with 24' to 28' Nominal Pavement Width</i>	37
<i>Streets with 20' to 24' Nominal Pavement Width</i>	40
 <i>Appendix A: Neighborhood Meetings and Public Surveys Summaries</i>	
<i>Appendix B: Toolbox Solution Concepts</i>	
<i>Appendix C: Recommendations: Cross Section Diagrams and Maps</i>	

Introduction and Purpose of the Study

The Holmes Point area of the Finn Hill Neighborhood has unique character inherent to the setting along the shores of Lake Washington—mature trees, sloping terrain, outstanding views toward the water, an extensive trails network that connects to multiple public parks and open spaces, and other features that exemplify Pacific Northwest beauty. O.O. Denny Park, named for Orion Denny, son of Seattle founder Arthur Denny, was originally donated to the public by the family in 1916. The neighborhood's character and historic identity must be considered, preserved, and enhanced as part of any recommendations to improve multimodal connectivity and the roadways throughout the neighborhood, including Holmes Point Drive.

This project analyzes and creates a range of suitable multimodal and street standards for this area—standards that are contextual and specific for application in the Holmes Point Overlay Zone. Kirkland Zoning Code Chapter 70 defines the majority of the Holmes Point area as the Holmes Point Overlay Zone, which sets maximum impervious surface standards for resident development to increase the level of environmental protection in this defined area. These standards differ from Kirkland's street standards for other locations in the City in that they allow for a flexible range of options to fit the unique Holmes Point setting and characteristics (steep slopes, trees along the right-of-way, drainage conditions, and other features).

For the most part, these are anticipated to be implemented when infill development occurs over time in the neighborhood. Therefore, private development projects will be required to make street improvements along their frontage (typically half-street improvements) and incorporate these standards as part of design. In this approach, the property owner on the other side of the street would not be required to pay for street improvements on the opposite side. To address equity, a Construction-in-Lieu Program is recommended for development properties located on the opposite side of the street from recommended street improvements whereby certain circumstances could warrant its usage. The street standards will help fulfill the purposes of the Finn Hill Neighborhood Plan and the Holmes Point Overlay Zone of the neighborhood (see goals in the box to the right).

This project also presents a corridor study of Holmes Point Drive, addressing existing conditions and recommending a specific set of design standards for three separate segments of the corridor. If these design standards are implemented, there will be opportunities to enhance walking and bicycling safety and use through a continuous pedestrian facility along the entirety of the corridor. Recommended improvements and street standards also would enhance general safety along the corridor for all modes of transport by encouraging slower speeds and enhanced crossings. Otak anticipates that improvements along the park frontage would likely need to be implemented through a capital improvement project, since this area is not likely to redevelop through infill.

The City retained Otak in early Fall 2020 to complete this study and develop recommendations and specific street standards for the Holmes Point Drive corridor and the public local access neighborhood streets in the Holmes Point Overlay Zone. The process continued through to Spring 2022 and included three separate neighborhood meetings and public surveys to gather input to form the recommendations. This report provides information about the study process, purpose of the study, background information, a summary of field work and existing conditions analysis. It then provides specific recommendations for Holmes Point Drive and the local access neighborhood streets.

This study includes recommended street cross section standards. These cross sections address the following elements:

- Preferred lane widths for Holmes Point Drive
- Parking
- Pedestrian facilities
- Bicycle facilities
- Street crossings
- Tree preservation areas and landscape strips
- Space for ditches/stormwater management, utilities, and roadside features

In addition, the study provides:

- Summaries of engagement meeting comments, in-meeting polls, and extended surveys (See Appendix)
- Best practices and innovative concepts for neighborhood traffic and speed management (See Appendix)
- Background information related to speeds and recorded crashes in the study area



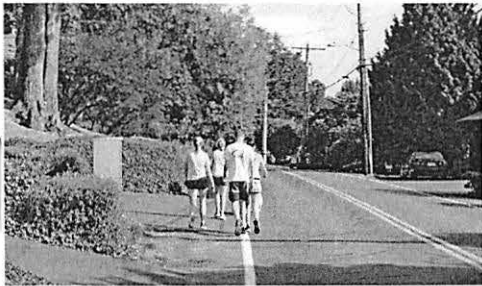
Holmes Point Overlay Street Design Standards Report 12

Study Area Context

The study area includes:

- The Holmes Point Drive NE corridor
- Public neighborhood streets within the Holmes Point Overlay Zone.

Right: Refer to the Study Area Map for a depiction of the study area context.



Above: Pedestrians on Holmes Point Drive



Above: Neighborhood streets in the Holmes Point Overlay Zone



Previous planning efforts: Beginning in 2016 and lasting two years, the City undertook a planning process with the community to develop the neighborhood plan for the Finn Hill area, which includes the Holmes Point Overlay Zone. Concerns about street connectivity and the use of Kirkland's citywide street standards in the Holmes Point area were topics that predicated the purpose of this study.

Excerpt from Finn Hill Neighborhood Plan:

Policy FH-14.3: Conduct studies to determine the design standards for the following streets:

- Residential streets within the Holmes Point Overlay area
- Holmes Point Drive corridor

Residents would like the character of the neighborhood to influence the design of pedestrian, bicycle, and street facilities that are built. For example, some residents feel sidewalks may not be appropriate for all areas and that "walking lanes" may be more appropriate for areas of the neighborhood with a more rural character. Developing the design standards for the streets should be created through a public involvement process. The standards should consider alternative designs for streets consistent with the City's Complete Streets Ordinance, such as the type of sidewalks, whether on-street parking is allowed, lighting, vegetation, pedestrian amenities, topographic or critical area constraints, tree retention, neighborhood character, all while providing emergency vehicular access. The presence of physical constraints such as steep topography, critical areas or to retain trees in a particular location may also require modification to city standards for right-of-way improvements.

This study has addressed these elements as a response to the previous neighborhood planning process.

Methodology and Process

Review of Background Materials and Guiding Documents

As part of the field work and observations phase of this study, the study team reviewed available background information, including the Kirkland Comprehensive Plan, Finn Hill Neighborhood Plan, the Kirkland Zoning Code, existing street standards, on-street parking provisions, Holmes Point Drive speed study data, and recorded crash history.

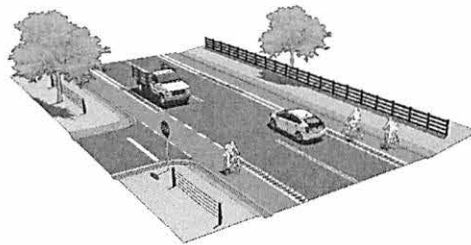
Other examples of existing street standards and guidelines were reviewed to determine applicability for adaptation to the Holmes Point area. These include the National Association of City Transportation Officials (NACTO) guides, Federal Highway Administration (FHWA) provisions (including the Small Town and Rural Multimodal Networks guide), American Association of State Transportation Officials (AASHTO) provisions (as applicable to local streets), Kirkland's Standard Roadway Policies and Roadway Pre-Approved Plans were also reviewed as part of this process.

The study team conducted field visits to the study area to document and measure conditions along Holmes Point Drive and the local access neighborhood streets. Because many of the streets in the Overlay Zone were originally built when the area was unincorporated King County, the streets were designed with a focus on motor vehicle traffic. As such, pedestrian and bicycle facilities are limited or non-existent on many streets. While some segments of sidewalks and narrow paths exist, many streets do not have pedestrian facilities. These inconsistent developments have yet to establish a pedestrian-forward connected community. Street crossing facilities (signs, crosswalk markings, etc.) are also lacking at key points along Holmes Point Drive. These existing conditions have prompted the need to develop these specific street standards to guide future development in the area.

Holmes Point Drive and all streets within the study area have a posted speed limit of 25 MPH. However, many drivers travel through the neighborhood at higher speeds in both directions, in particular on the steeper grades where Holmes Point Drive descends to meet Juanita Drive NE.

Preserving the character of the neighborhood (with mature trees, views, lowlands along the lake, parks and green spaces, etc.) has been an important consideration in the development of these design standards, as established in the Finn Hill Neighborhood Plan. The five guiding values of the Holmes Point design standards are described on the right of this page.

Right: Diagram of a paved shoulder for pedestrians and bicyclists from the Small Town and Rural Multimodal Networks guide. This document was one of the background resources used in developing street standard recommendations.



Excerpt from Finn Hill Neighborhood Plan:

Comprehensive Plan—Chapter XV.P Finn Hill Neighborhood Guiding Neighborhood Values

The Holmes Point subarea is located within the larger Finn Hill Neighborhood. There are five guiding values to the policies stated in the Finn Hill Neighborhood Summary.

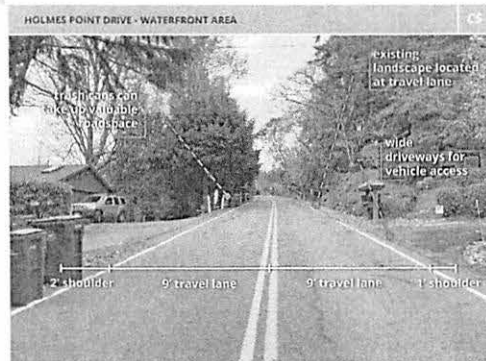
- *Value 1: Promote human and wildlife connectivity through multifunctional, interconnected green spaces.*
- *Value 2: Provide a consistent land use pattern that supports the neighborhood's desire to retain its low-density character.*
- *Value 3: Protect critical areas and preserve tree canopy cover and wildlife habitat to maintain the natural environment.*
- *Value 4: Develop the neighborhood commercial districts into pedestrian-oriented villages that are human in scale, provide needed services and gathering places within walking distance of residential, support transit options, are developed with sensitivity to the neighborhood's environmental and traffic concerns, and maintain the neighborhood's character.*
- *Value 5: New development in the neighborhood should be in keeping with the neighborhood's vision of preserving and enhancing Finn Hill's natural environment.*

Summary of Field Work and Existing Conditions

Field Work Summary

The consultant team analyzed existing conditions, spending several days in the Holmes Point Overlay Zone. The team documented existing street dimensions and photographed conditions and features in the right-of-way of all public streets; private streets were not part of the scope of this study. The team identified various types of street segments for Holmes Point Drive, and for public local neighborhood streets throughout the study area.

Observed street types and conditions were mapped in a Geographic Information Systems (GIS) database. The map shown on this page represents the typologies identified immediately following the field work. As the team began to work on design recommendations, the number of street typologies was reduced to fit the range of standards proposed. These revised typologies are presented later in this report.

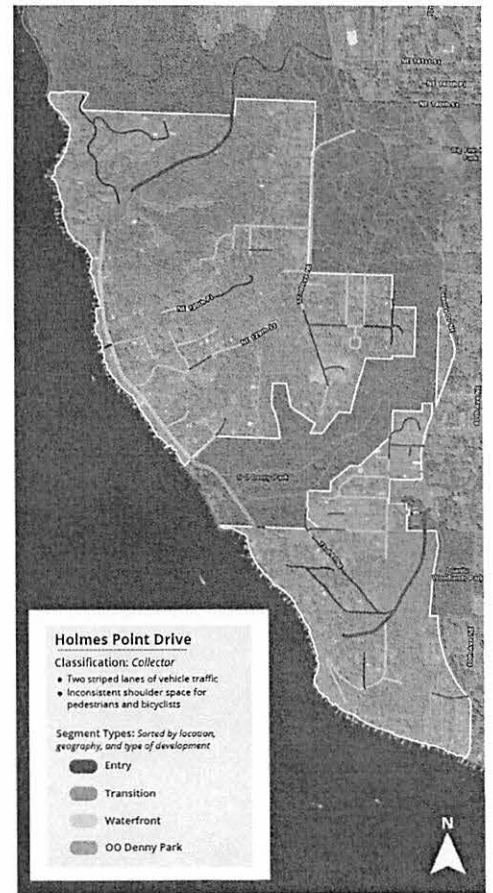


Above: Example of a street in the Waterfront Area of Holmes Point Drive.

Holmes Point Drive Corridor

Field work for the Holmes Point Drive corridor grouped areas based upon four typologies that influence lane widths, on-street parking conditions, existing improvements, and other characteristics. These typologies are described in more detail below.

- **Entry** - These areas, shown in red, occur at either end of the Holmes Point Drive loop where the corridor rises to meet intersections with Juanita Drive NE. These areas are heavily wooded, with steeper curving roadway grades and minimal private development along the corridor.
- **Transition** - The transition areas are shown in orange on the map and represent areas where the grade becomes less steep and curving conditions occur in a few locations. Private development (homes, driveways, residential structures, etc.) occurs along both sides of Holmes Point Drive.
- **Waterfront** - This area provides access to homes located along the Lake Washington waterfront and is shown in yellow. This section of Holmes Point Drive corridor is relatively level or flat in grade and more densely populated with homes, driveways, and other residential features and structures. This area is relatively straight.
- **OO Denny Park** - This portion of the Holmes Point Drive corridor, shown in green, passes through OO Denny Park, with public open space on both sides of the corridor and no private development. Similar to the Waterfront area, this section is also relatively level in grade and includes minimal curvature. This area tends to be the most congested area for all modes of transport and currently provides space for parking along one side of the street for those accessing the park.



Holmes Point Drive Corridor: Existing Conditions

Holmes Point Drive is classified as a Collector street with less than 3,000 average daily trips. Most of the development along Holmes Point Drive is single family residential (LDR), but segments of the drive travel through undeveloped and developed open space areas, including Juanita Woodlands Park, Saint Edward State Park, and OO Denny Park, which is located at the lower west segment of the loop drive, along the lakefront.

In Kirkland, a typical Collector street requires a minimum 60-foot right-of-way width, with two 11-foot travel lanes, two 5-foot bike lanes, street parking, curb and gutter, 4.5-foot landscape strip, and 5-foot sidewalk on both side of street. The right-of-way for Holmes Point Drive varies from 40 - 60 feet in width, while the existing roadway ranges from 20-feet wide to 30-feet wide. Existing constraints such as steep slopes, wetlands, drainage features, walls, and private improvements within the right-of-way limit the applicability of the existing street standards.

While the typical Collector street carries between 1,500 and 3,000 average daily trips (ADT), Holmes Point Drive typically carries an average of less than 1,500 ADT. Holmes Point Drive does function as a Collector as the only major road in the overlay zone even though it carries less traffic than a typical collector street. Speeding is a prevalent problem (observed and based on neighborhood input). The posted speed limit along the drive is 25 MPH, but motorists frequently exceed the limit.

A majority of the length of the Holmes Point Drive corridor is located within the shoreline jurisdiction recognized by the state and City of Kirkland (200 feet from the ordinary high water mark [OHWM]). Soil mapping indicates that there are areas of the corridor with high liquefaction potential. Some areas near OO Denny Park are designated as Urban Conservation in the City's Shorelines Management Plan. Potential expansion of impervious surfaces within the Holmes Point Drive right-of-way should be kept to a minimum through this area, and it is also an important stormwater management best practice. There are also steep slopes and landslide hazard areas in proximity to the corridor. Most of the neighborhoods also have extensive mature trees (mostly coniferous), which should be retained where feasible to help strengthen the slopes.



Above: Looking northwest on Holmes Point Drive at the northern side of OO Denny park.

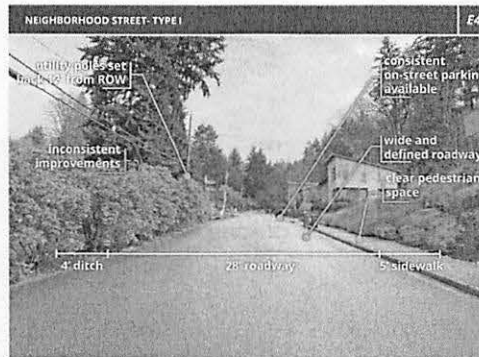


Above: Entry to OO Denny Park from Holmes Point Drive, across from parking lot.

Local Access Neighborhood Streets

The existing conditions analysis and field work for the local access neighborhood streets in Holmes Point identified four different types of existing streets, as described below.

- **Type I** - These local access streets (shown in dark blue on the map) tend to have approximately 24 - 28 foot pavement widths with dedicated sidewalks on at least one side of the street and have been developed in accordance with City of Kirkland's current street standards.
- **Type II** - These local access streets (shown in lighter blue on the map) tend to have approximately 24 - 28 foot pavement widths with curbs on at least one side of the street, but no existing sidewalks or pedestrian paths.
- **Type III** - These local access streets (shown in light purple on the map) tend to have approximately 24 - 28 foot pavement widths with no curbs, sidewalks, or pedestrian paths.
- **Type IV** - These local access streets (shown in darker purple on the map) tend to have narrower pavement widths of approximately 24 foot width or less with no curbs, sidewalks or pedestrian paths. These streets tend to be more wooded, with mature trees on both sides of some segments.



Above: Example of a Type I neighborhood street in the Holmes Point Overlay Area.



Above: Example of a Type III neighborhood street in the Holmes Point Overlay Area.



Local Access Neighborhood Streets: Existing Conditions

Holmes Point Overlay Zone land uses consist of low-density residential (LDR) single-family housing and OO Denny Park, as well as other open space corridors. The land use pattern is generally the same as designated by King County prior to annexation. Based on City of Kirkland's LDR land use zoning, without the adoption of neighborhood-specific street standards through this project, the existing access streets would be required to meet R-20 or R-24 street type requirements pursuant to KZC Chapter 110 - Required Public Improvements. As new and infill development has occurred, City street and sidewalk standards have been referenced in a few locations.

The R-20 and R-24 street standards require a minimum 30 to 50-foot right-of-way width and 80-foot diameter right-of-way width for cul-de-sacs. This cross section includes curb and gutter, 4.5-foot landscape strip, and 5-foot sidewalk on both sides of the street. Deviations are allowed for permanently dead-ended street less than 1,000 feet in length.

The existing right-of-way for most Holmes Point Overlay Zone streets ranges from 30 to 50 feet wide. However, existing paved roadway widths can be as narrow as 10 to 15 feet in some areas, while the average existing roadway is between 20 and 30 feet wide. Only areas that have already been redeveloped to the existing street standards have roadways wider than 30 feet, as well as curb, gutter, sidewalks, and planting strips per the City code.



Right: Photos show the range of site conditions of existing neighborhood streets in the Holmes Point area.

Vehicle Speeds and Safety Conditions

Vehicle Speeds

A vehicle speed study was conducted along the Holmes Point Drive in 2019 to analyze the incidents of speeding (vehicles traveling faster than the posted speed limit) along the corridor. The study documented vehicle counts and vehicles speeds at seven locations (see map this page of locations monitored) along Holmes Point Drive on three separate days (March 17th, June 14th, and June 19th). Traffic counts from near OO Denny Park were collected on June 19th only, while other locations were visited two or three times to collect data. Traffic counts reflect vehicles traveling in both directions along the corridor. The findings of the study show that the average motorist drives more than 5 mph over the posted speed limit and identified that motorists south of OO Denny Park tend to drive more than 10 mph over. In 2021, a second speed study was conducted to check for changes in traffic counts. The 2021 study documented vehicle counts at three locations along the corridor. The data collected during this time showed no changes in vehicle speeds or traffic counts along the drive.

Reported Crashes

Based on data provided by the provided by the WSDOT Crash Data and Reporting Branch (based upon officer-reported crashes), there were twenty reported crashes in the Holmes Point Overlay area from 2016 to 2021. Most of these resulted in no apparent injury or possible injury with only two incidents reported with a suspected minor injury. The most common locations where incidences occurred were at the intersection of Holmes Point Drive and Champagne Point Drive NE, and along the road through OO Denny Park. Crashes occur all along the drive but are most common at intersections or along the undeveloped stretches of road at the north and south ends of the Overlay area. One incident resulting in no apparent injury occurred at the Holmes Point Drive intersections with Juanita Drive NE. See the table and map at right for more detailed information on crashes.

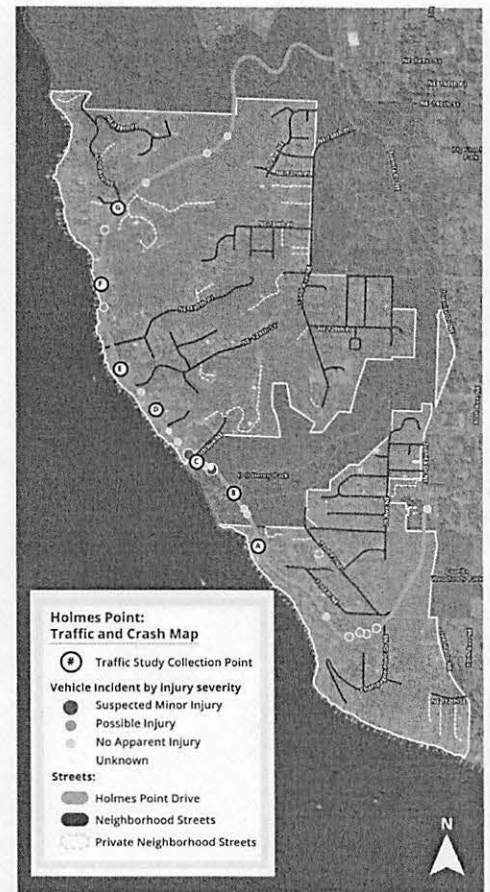
Speed Study Findings Table

Location Point	Site Address	85 th percentile Speed (MPH)*	Vehicles per day	Date
A	11833 S of 68 th Ave NE	35.6	1542	19 th June, 2019
	11833 S of 69 th Ave NE	33.7	1592	21 st June, 2021
B	Denny Park South Crossing	33.1	1543	19 th June, 2019
C	Denny Park North Crossing	33.1	1422	19 th June, 2019
	12534 N of 68 th Ave NE	31.7	1227	17 th March, 2019
D	12534 N of 68 th Ave NE	30.8	1293	14 th June, 2019
	12534 N of 68 th Ave NE	31	1341	19 th June, 2019
E	12543 N of 68 th Ave NE	30.8	1252	21 st June, 2021
	12930 S of NE 130 th Pl	30.3	1058	17 th March, 2019
F	12930 S of NE 130 th Pl	30.1	1316	16 th June, 2019
	13138 N of NE 130 th Pl	33.1	1005	17 th March, 2019
G	13138 N of NE 130 th Pl	32.7	1242	19 th June, 2019
	13419 S of NE 130 th Pl	32.6	995	17 th March, 2019
G	13419 S of NE 135 th St	32.7	1300	19 th June, 2019
	13419 S of NE 135 th St	31.8	1267	21 st June, 2021

*The speed at or below which 85 percent of all vehicles are observed to travel under free-flowing conditions past a monitored point

Crash Data Table

Primary Trafficway	Block Number	Reference Point Name	Most Severe Injury Type	First Collision Type / Object	Date
HOLMES POINT DR NE	13600	64TH PL NE	No Apparent Injury	Fallen tree hit by vehicle (on the road)	2/26/2016
HOLMES POINT DR	7900	JUANITA DR NE	Suspected Minor Injury	Guardrail - Face	2/28/2016
HOLMES POINT DR NE	12700	NE 129TH ST	No Apparent Injury	One car leaving parked position	3/19/2016
HOLMES POINT DR NE	13200	NE 130TH PL	Possible Injury	One parked-one moving	6/11/2016
HOLMES POINT DR NE	12400	NE 125TH ST	No Apparent Injury	One parked-one moving	11/8/2016
HOLMES POINT DR NE	13400	62ND AVE NE	Possible Injury	Tree or Stump (stationary)	6/11/2017
HOLMES POINT DR NE	0		Unknown	Tree or Stump (stationary)	7/2/2017
HOLMES POINT DR NE	11500	CHAMPAGNE POINT RD NE	Possible Injury	Earth Bank or Ledge	7/14/2017
HOLMES POINT DR NE	12400	NE 124TH ST (66TH AVE)	Suspected Minor Injury	From same direction - both going straight - one stopped - rear-end	10/13/2017
HOLMES POINT DR NE	12200	NE 124TH ST CHAMPAGNE POINT RD NE	Suspected Minor Injury	Fire Hydrant	3/25/2018
HOLMES POINT DR NE	11500	POINT RD NE	Possible Injury	Guardrail - Face	7/15/2018
HOLMES POINT DR	6900	JUANITA DR NE	No Apparent Injury	Wood Sign Post	1/10/2019
HOLMES POINT DR NE	13900	JUANITA DR NE	No Apparent Injury	Guardrail - Through, Over or Under	1/14/2019
HOLMES POINT DR NE	11500	CHAMPAGNE POINT RD NE	Possible Injury	Guardrail - Face	6/24/2019
HOLMES POINT DR NE	11500	POINT RD NE	Possible Injury	Earth Bank or Ledge	8/7/2019
HOLMES POINT DR NE	7900	NE 124TH ST	Unknown	Fire Hydrant	12/15/2019
HOLMES POINT DR	6900	DR	No Apparent Injury	Metal Sign Post	7/13/2020
HOLMES POINT DR NE	13800	JUANITA DR	No Apparent Injury	Fallen tree hit by vehicle (on the road)	10/13/2020
HOLMES POINT DR NE	12000	NE 124TH ST	No Apparent Injury	Other Objects	6/6/2021
HOLMES POINT DR NE	8300	NE 124TH ST CHAMPAGNE POINT RD NE	No Apparent Injury	One parked-one moving	6/28/2021
HOLMES POINT DR NE	11500	POINT RD NE	No Apparent Injury	Tree or Stump (stationary)	6/29/2021
HOLMES POINT DR NE	13700	64TH TER NE	No Apparent Injury	Tree or Stump (stationary)	10/24/2021
HOLMES POINT DR NE	12000	NE 124TH ST	No Apparent Injury	Roadway Ditch	10/30/2021



Neighborhood Engagement Process

Summary of Neighborhood Engagement Activities

Input from the public and neighborhood residents was gathered in three online workshops, held in December 2020, May 2021, and December 2021. Each meeting provided a presentation of the status of work on the project and engaged participants in key questions to gather input. In addition, the City posted an online survey directly following all three online meetings, with the final responses collected January 2022.

Comments from the neighborhood meetings are summarized below. Overall, preserving neighborhood character and trees were mentioned as high priorities for residents. This means that keeping the width of improvements on streets to the narrowest possible dimensions is important. Having various flexible approaches along standards for different streets will be necessary to preserve and enhance the neighborhood character.

Summary of Neighborhood Meetings How Public Input Shaped

Holmes Point Drive Corridor

Priorities noted by residents included improved walking and bicycling conditions along the entire length of the Holmes Point Drive corridor, along with slowing down cars (traffic calming), particularly in the downhill areas. The corridor is heavily used by pedestrians, especially approaching and in the vicinity of OO Denny Park. The community felt that protecting pedestrians along Holmes Point Drive should be a main priority, as most cyclists were comfortable sharing the road with vehicles for most sections of the drive. However, residents expressed concerns about cycling in the entry segments of Holmes Point Drive. Comments mentioned, "going downhill on dark, wet curvy roads with poor pavement" on a bike and riding uphill within curved areas can be dangerous and there is not sufficient space for cars to pass cyclists safely. Residents were also supportive of providing opportunities for beginning or slow moving cyclists to have more protection in flatter road sections. There were concerns from the community about possible impacts to trees and private property in the right-of-way that may be impacted by improvements. The community recognized the value of having pedestrian facilities on Holmes Point Drive, and felt that this was a greater priority than maintaining all available on-street parking.

OO Denny Park

Residents noted that overflow parking and parking along the park frontage is chaotic, confusing, and causes congestion and safety concerns at times with pedestrians walking around, between, and behind vehicles. Residents also commented that improved crossings are needed to enhance safety for people walking and bicycling to the park. There are safety concerns in this area due to vehicle speeds, and the community wanted to see more traffic calming. There was also interest from the community in creating new pathways for pedestrians that allow for greater separation from vehicles. In response, the design recommendations presented later in this report propose a new cross section along the OO Denny frontage, with pedestrian paths on both sides of Holmes Point Drive, parallel parking on one side (well delineated and dimensioned for only parallel use), and raised, delineated, and signed street crossings at the south and north ends of the park area.

Local Access Neighborhood Streets

Residents noted that on narrower local access streets throughout the neighborhood, volumes of traffic are lower, and there are minimal conflicts between cars, pedestrians, and bicyclists. Motorists drive more slowly on these streets, and as such it may be possible for cars, bicyclists, and pedestrians to share the pavement surface in some of these areas. At the same time, residents mentioned a strong interest in pedestrian connectivity and having continuous pedestrian routes of travel throughout the neighborhood. Residents also commented that due to the variety of conditions in neighborhood streets, it would be useful to have multiple street standards that address different road widths, parking availability, and existing pedestrian facilities. The community also emphasized the desire to maintain street trees, and preferred concepts that minimized the road widening. Recommended street standards presented later in this report have been developed based on preservation of trees and neighborhood character in balance with providing continuous pedestrian facilities in some locations and retaining some local streets in the same condition as today while introducing signage to encourage shared street usage.

Holmes Point Recommendations by Location

Design of streets within the Holmes Point Overlay Zone area requires unique considerations related to neighborhood character and environmental conditions (steep slopes, soil conditions, shorelines jurisdiction compliance, mature trees, stormwater management, etc.). The City of Kirkland Comprehensive Plan emphasizes maintaining the low-density residential character and natural environment of the neighborhood as a priority. The Finn Hill Neighborhood Plan supports limiting development in environmentally critical areas in order to mitigate disruption to wildlife, retain tree canopy, and conserve land for open space and parks.

Recommendations have been refined over the course of the project to best reflect the character and needs of each location. Input from the public and neighborhood engagement process was integrated into the development of these options. Special consideration was given to address community concerns about character, mature trees, road widths, and environmentally critical areas. These solutions were designed to be installed within the existing right-of-way width, while also minimizing impacts to built features (ditches, fencing, walls, etc.) within and adjacent to the right-of-way.

Based on background documents, feedback from the City of Kirkland, and input from public and neighborhood engagement, the study team has developed recommendations for street design standards and guidelines that are unique for Holmes Point Drive and the local access neighborhood streets within the Holmes Point Overlay Zone. The recommended standards on the following pages should be applied with flexibility to preserve trees and in consideration of encroachments that may not be feasible to remove.

With infill development and redevelopment efforts, individual projects will be responsible for making improvements along their frontages. A Construction-in-Lieu Program is recommended for development properties located on the opposite side of the street from recommended street improvements, whereby certain circumstances could warrant the use of this program to pay for street improvements on the opposite side. In most cases, if infill development or redevelopment projects occur, the developer would be responsible for removing encroachments (such as fencing, walls, and/or other structural elements), within the right-of-way along their frontage.

If encroachments occur within the right-of-way in the area of a proposed capital improvement project, the City will notify property owners that the encroachments may need to be removed. If removal of encroachments is not feasible (such as if the encroachment is part of a driveway access, e.g. retaining walls), the design process will look at specific retrofit solutions to achieve the design standards proposed.

Street Typologies

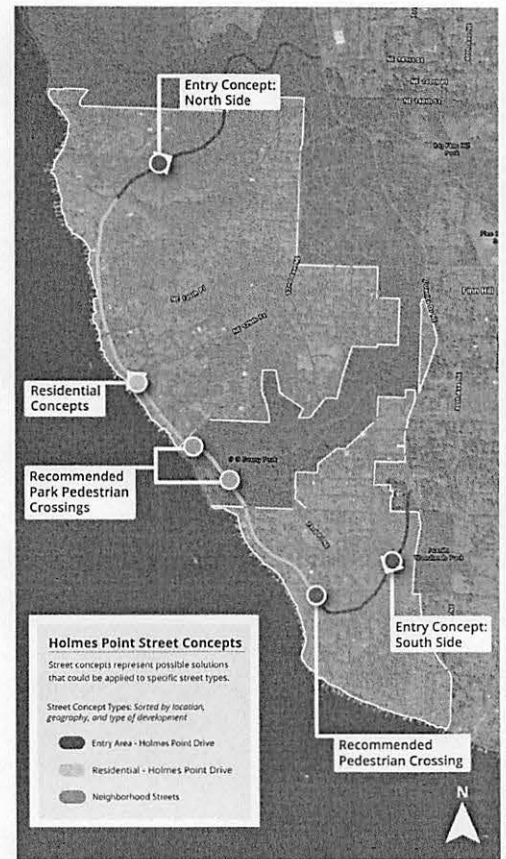
In order to develop site-specific recommendations, the Holmes Point Overlay Zone was categorized by street type. The two main categories were Holmes Point Drive/76th PINE and neighborhood streets. Subcategories and associated recommendations were developed for each, and are presented in this document.

Holmes Point Drive Corridor

- Entry Areas
- Residential Areas
- OO Denny Park

Local Access Neighborhood Streets

- Sidewalk, Curb, and Gutter Street Connections
- Streets with 24' - 28' Nominal Pavement Width
- Streets with 20' - 24' Nominal Pavement Width



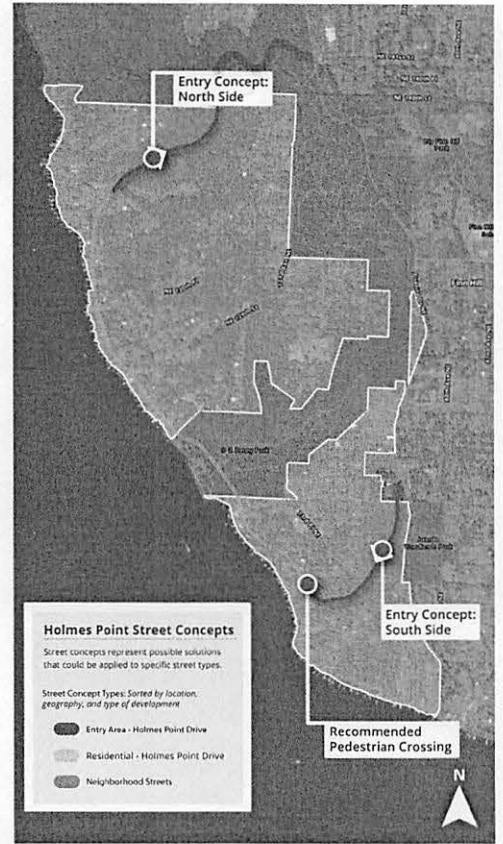
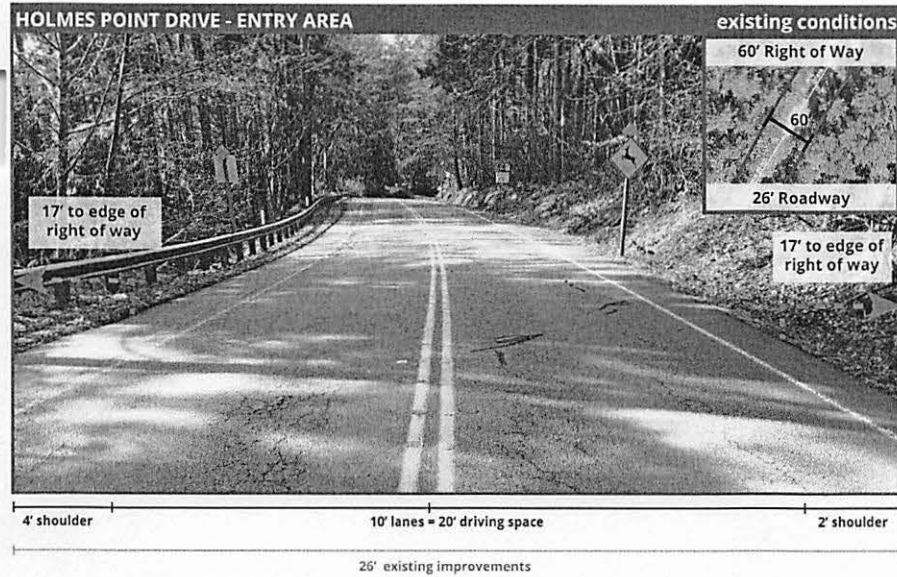
**Holmes Point Drive
Entry Area**
Recommendations

Entry Areas - Holmes Point Drive

Existing Conditions:

The entry segments of the Holmes Point Drive corridor are unique, justifying the need for recommendations specific to this area. The entry areas act as forested entrances to the Holmes Point Overlay Zone. These segments are generally steeper than other areas of the Holmes Point Drive corridor and also have the least amount of adjacent residential development. In both the north and south entry segments, there is a 60-foot right-of-way, and the road contains two 10-foot wide travel lanes and shoulders of varying widths. Generally, the total pavement width of the entry segments is about 26-feet.

Below: A photo from the southern entry segment shows existing conditions and approximate road dimensions.



Entry Areas - Holmes Point Drive

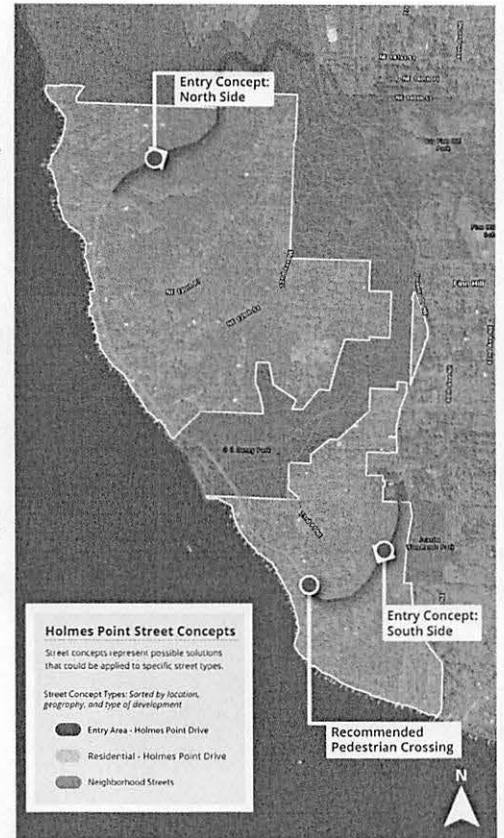
Neighborhood Input:

Initial feedback from the community indicated that while biking conditions are generally good along Holmes Point Drive, the entry segments required more protection for riders. This is due to the curves and steep conditions in which the entry segments prevent cars from safely passing bicyclists that are slowly climbing uphill. Some residents also commented that walkers and runners cannot safely use the entry segments, as the lack of space forces them into the road. The community also shared that pedestrians are a key user group along Holmes Point Drive, and providing facilities for them should be a priority throughout the Overlay Zone. These comments led to the recommendation of a continuous pedestrian path along Holmes Point Drive, that would also include the entry areas.

Recommendations:

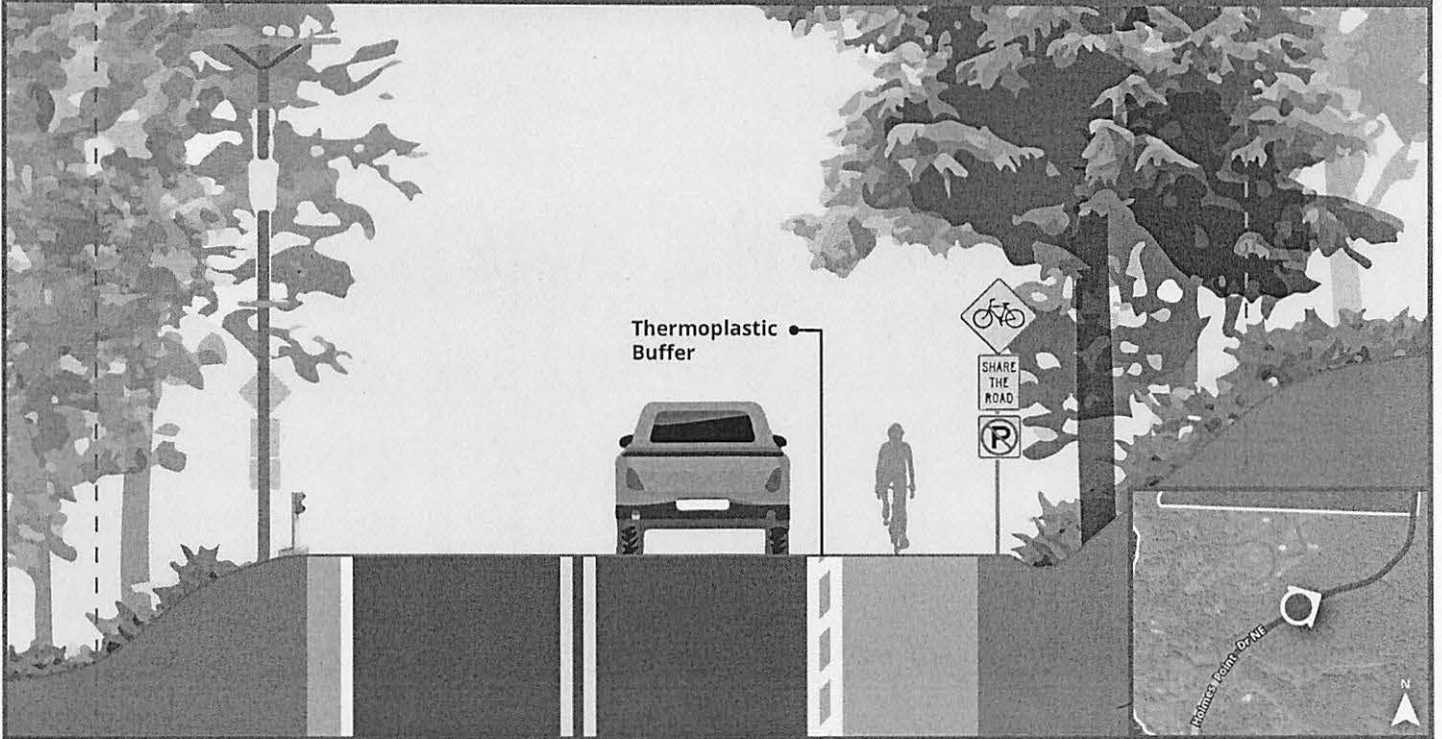
The recommendation for the entry areas of Holmes Point Drive includes a buffered climbing shoulder on the uphill side of the road. This mixed-use shoulder will be 8-feet in width, with a 2-foot thermoplastic buffer to separate it from traffic. This shoulder will provide a designated space for bicyclists to climb uphill when they are traveling slower, while also allowing them to cross the buffer and ride in the driving lane. Similar climbing lanes have been installed in other areas in Kirkland, such as on NE Juanita Drive. 10-foot wide drive lanes will be maintained through the entry areas. Though these segments tend to have lower foot traffic than other areas along Holmes Point Drive, the 8-foot wide buffered shoulder will allow enough space for both bicyclists and pedestrians to travel. Neighborhood feedback indicated that priority should be placed on providing pedestrian facilities throughout the Holmes Point Drive corridor, so the shoulder width recommendation was widened to accommodate both bicycle and pedestrian users. Providing these facilities will also accomplish the community identified goal of a continuous pedestrian path along the Holmes Point Drive.

The recommendations for the entry areas of Holmes Point Drive would require a total width of 32-feet, including two 10-foot drive lanes, the 8-foot climbing shoulder, the 2-foot buffer, and a 2-foot shoulder. These improvements could easily be accommodated in the 60-foot right-of-way, but would require widening the existing footprint of the paved area by about 6-feet. Due to the small amount of private development in these areas, it is not likely that these improvements will be made through infill activity, and would likely be driven by Capital Improvement Projects.



ENTRY AREA - HPD: NORTH SIDE

BUFFERED SHOULDER ON UPHILL SIDE OF THE ROAD



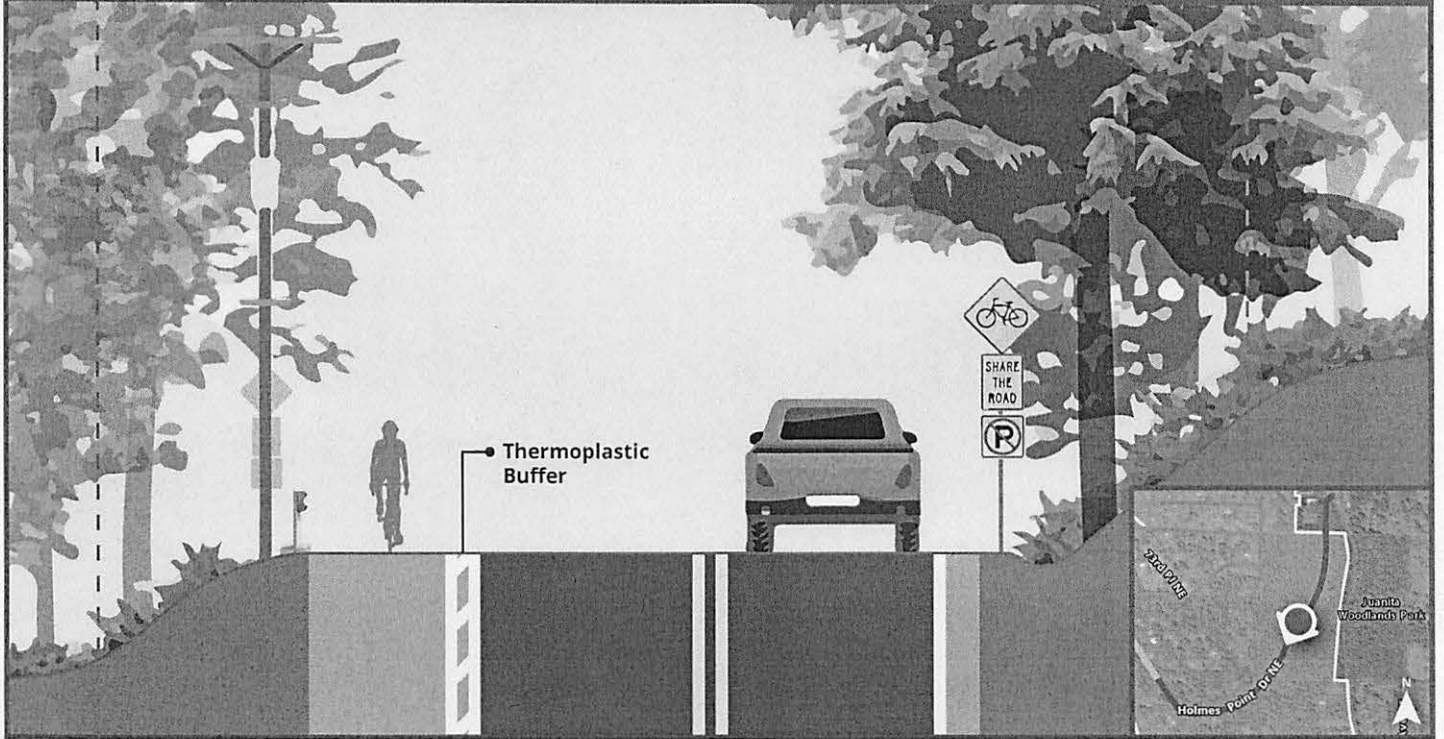
2'
Shoulder

10' Lanes = 20' Driving Space

2'
8'
Protected
Mixed-Use
Shoulder

ENTRY AREA - HPD: SOUTH SIDE

BUFFERED SHOULDER ON UPHILL SIDE OF THE ROAD



8'
Protected
Mixed-Use
Shoulder

2'

10' Lanes = 20' Driving Space

2'
Shoulder

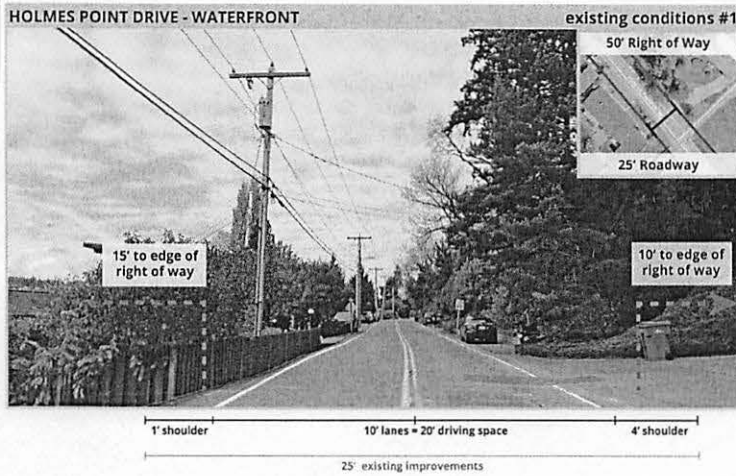
**Holmes Point Drive
Residential Areas**
Recommendations

Residential Areas - Holmes Point Drive

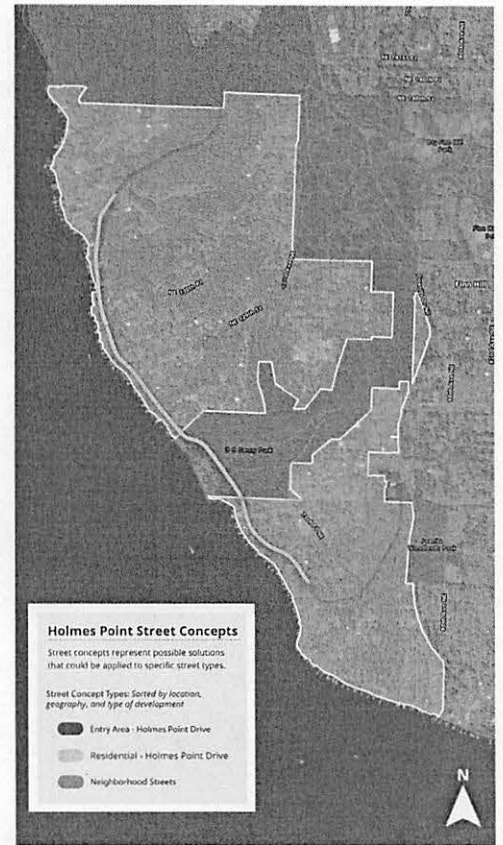
Existing Conditions:

The residential segments beyond the entry segments of Holmes Point Drive are mostly flat to moderately sloping and there is typically more pedestrian activity than in the entry segments. Though pedestrian activity is high from residents and park visitors, there is a lack of pedestrian or bike facilities throughout the residential corridor. Speeding is prevalent in this area, and can be a major safety issue due to pedestrian presence and the large amount of hidden residential driveways on Holmes Point Drive.

Conditions throughout the residential areas are diverse, and the right-of-way varies from about 40-feet to 60-feet. Private improvements such as fences, carports, and landscape features are located within the right-of-way, leading to the potential for conflicts with private property if more right-of-way is utilized for parking, road, pedestrian, and bicycle developments. While there are typically two 10-foot wide driving lanes throughout the residential area, shoulder width varies greatly. Some road segments have wide stretches of shoulder that are used for parking, while other segments have very narrow shoulders which bring pedestrians very close to the road.



Left: A photo from the residential area of Holmes Point Drive shows existing conditions and approximate road dimensions. This area has limited shoulder space, and many private improvements within the right-of-way.



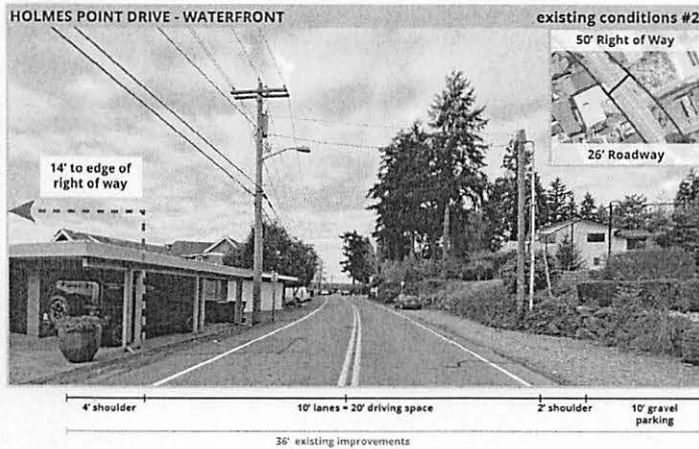
Residential Areas - Holmes Point Drive

Neighborhood Input:

A major desire from the community was to improve walking conditions for pedestrians. Due to the residential population and proximity to OO Denny park, this area receives high amounts of pedestrian activity. A lack of pedestrian space combined with speeding issues can lead to major safety issues for walkers and runners, and a desire for speed reduction and traffic calming was identified by the community. Residents were interested in providing safe facilities for children to walk and bike to their homes, school, or the park. While bicyclists also travel on Holmes Point Drive, in general, community members felt that most bike riders could safely share the road with vehicles throughout the residential area. However, some residents thought that slower moving or less confident bicyclists should have an options to ride separately from traffic. In order to best serve the needs of the residents, the community felt that a continuous, protected path was needed along the Holmes Point Drive corridor. This path would be utilized by pedestrians and learning cyclists.

One of the main concerns from the neighborhood was the effect that improvements may have on road widening. There was a strong desire to maintain existing private developments, such as landscaping and garden features, in the right-of-way. Some homes have non-conforming garages or carports located in the right-of-way. These existing structures provide residents access to their homes safely along the steep grades in the study area. Additionally, the community had a strong interest in preserving trees along the roadway. Based on this feedback, recommendations were developed that aimed to provide safe facilities for pedestrians and bicyclists, while reducing the amount of pavement widening, wherever possible.

Additionally, residents responded that continuous street parking was not a priority, but they were supportive of providing incremental sections of parking where existing conditions allowed. For this reason, multiple recommendations were developed for the residential areas along Holmes Point Drive, in order to provide options with and without street parking. These options could be implemented based on existing site conditions to minimize conflicts with private property access and vegetation.



Left: A photo from the residential area of Holmes Point Drive shows existing conditions and approximate road dimensions. This area has a wide shoulder that allows for street parking on one side of the road, and many private improvements within the right-of-way are present.

Residential Areas - Holmes Point Drive

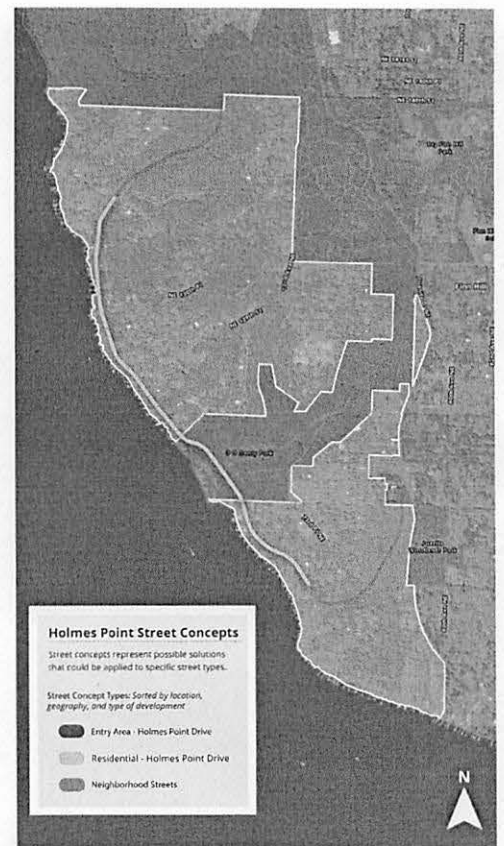
Recommendations:

Three different street design recommendations were provided for the residential areas of Holmes Point Drive, in order to best respond to the needs of the community and existing site conditions.

The first recommendation is for a continuous mixed-use shoulder on the east side of Holmes Point Drive. This 8-foot wide shoulder would be separated from the roadway by a curb, in order to prevent street parking in the pedestrian walkway area and to separate pedestrians from vehicles. The protected pedestrian shoulder is intended to provide a continuous facility for pedestrians to use on Holmes Point Drive. It also could function as a shared space for slow speed bicyclists, such as children on bicycles, as it is anticipated that more experienced commuter cyclists would prefer to ride in lane with vehicles. However, a 4-foot wide shoulder is also provided on the other side of the road for faster moving bicyclists. Both the protected 8-foot shoulder and the 4-foot shoulder for cyclists will extend throughout the Holmes Point Drive corridor, and connect users to the buffered climbing lanes in the entry segments. Vehicle lanes are narrowed from the existing 10-foot width to 9.5-feet, in order to reduce road widening and provide a traffic calming measure. The total width of recommended improvements is 31-feet. The existing paved footprint of Holmes Point Drive currently varies from about 20-feet to 40-feet in width. In many areas, this design could be implemented within the existing paved footprint to reduce impacts residents. In narrower sections, pavement widening of up to 11-feet may be required. In this case, relocation or removal of private property or trees in the right-of-way would be determined by a case-by-case and site specific basis.

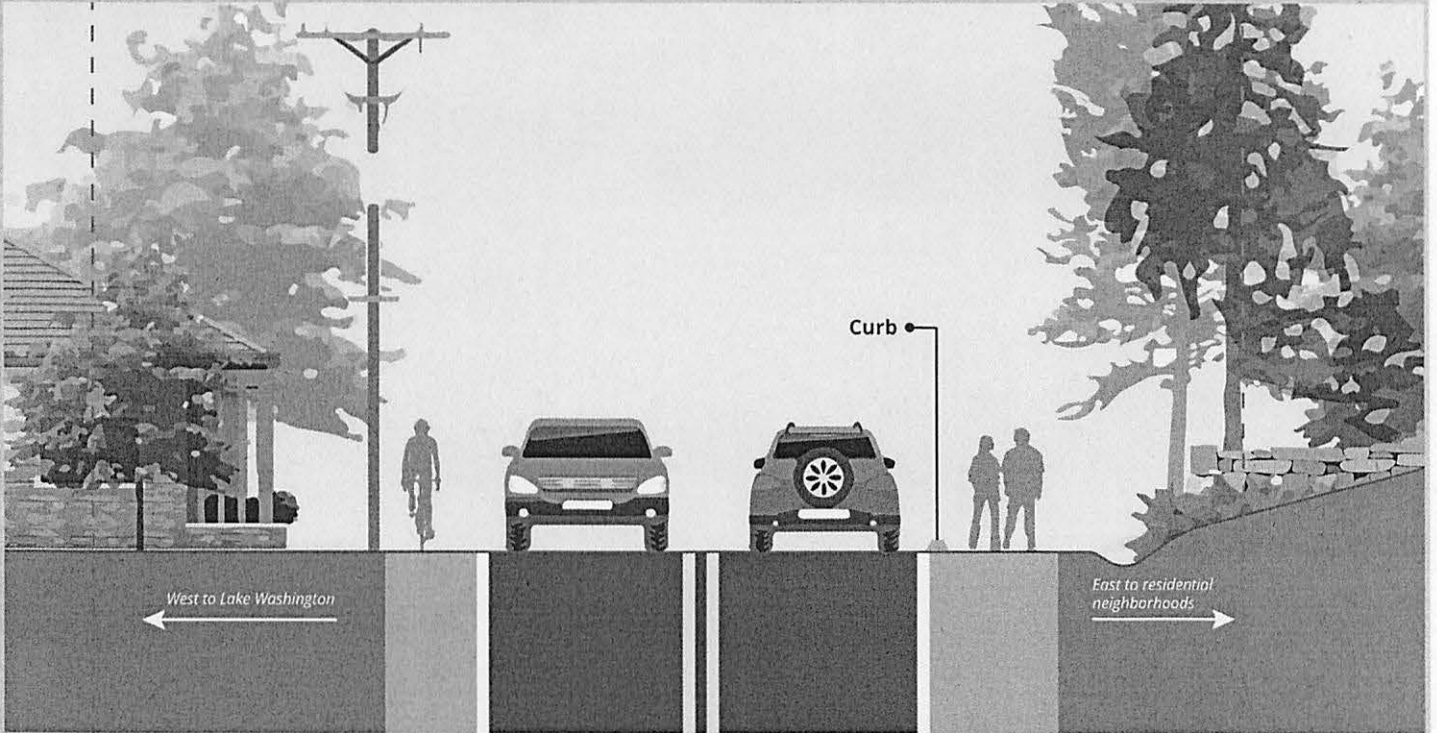
The second recommendation provides the same solutions as the first, with the addition of on-street parking on the east side of the road, between the travel lane and the protected shoulder. This design would be implemented in road sections that already have wide shoulders on the east side of the road to accommodate parking. This recommendation will provide parking where site conditions allow while still leaving room for a continuous mixed-use shoulder.

The third recommendation provides the same solutions as recommendation two, but the parking lane is located on the west side of the road instead, between the travel lane and the 4-foot shoulder. A curb separates the shoulder and the parking lane, to prevent vehicles from parking on the shoulder. Similar to the second recommendation, this design would be implemented in road sections that already have wide shoulders on the west side of the road to accommodate parking. In this way, parking is limited to wider sections of the residential area, in order to provide amenities for the community while reducing impacts to the neighborhood.



RESIDENTIAL - HPD:

PROTECTED MIXED-USE SHOULDER



4'
Paved
Shoulder

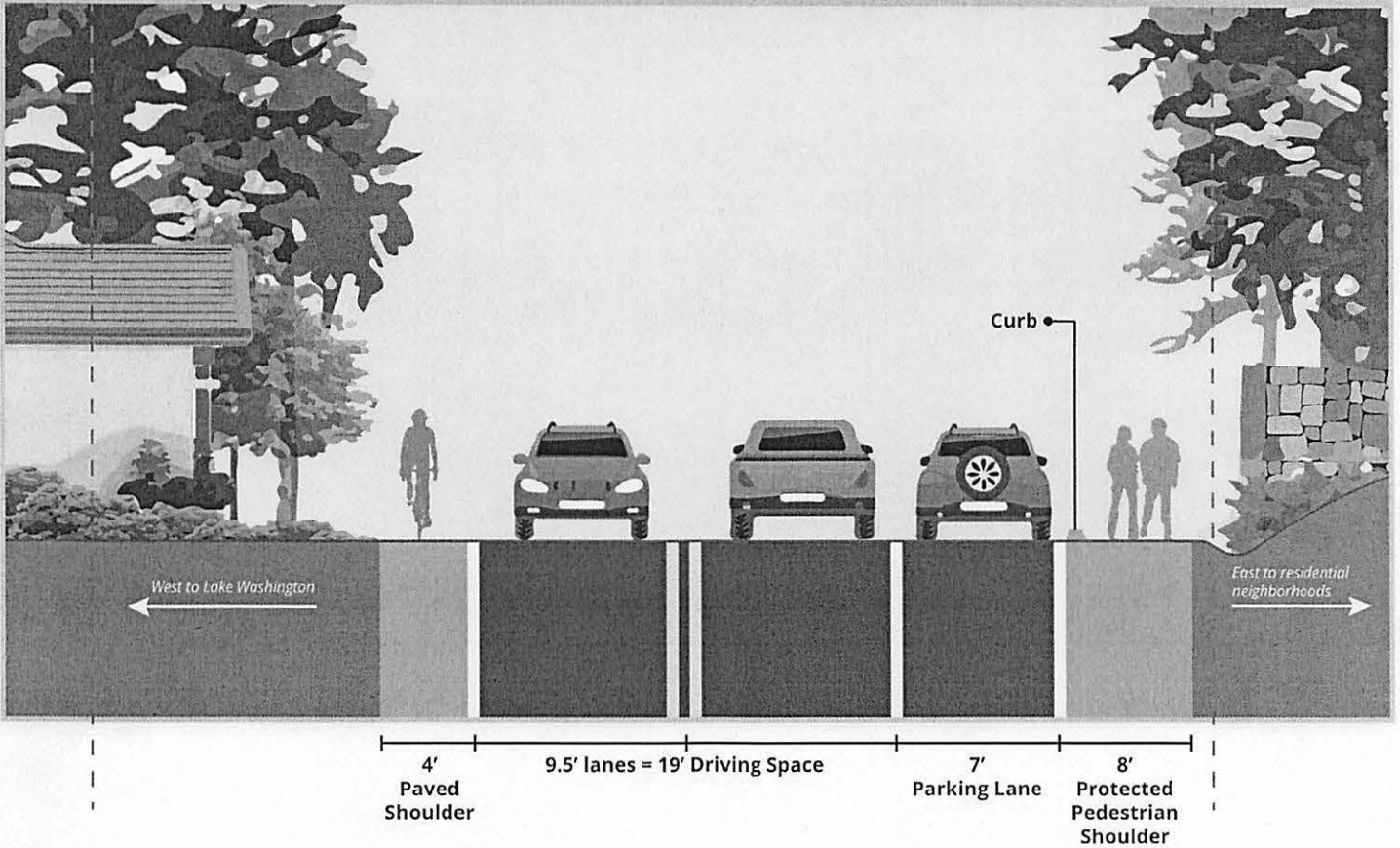
9.5' Lanes = 19' Driving Space

8'
Protected
Pedestrian
Shoulder

April 15, 2022

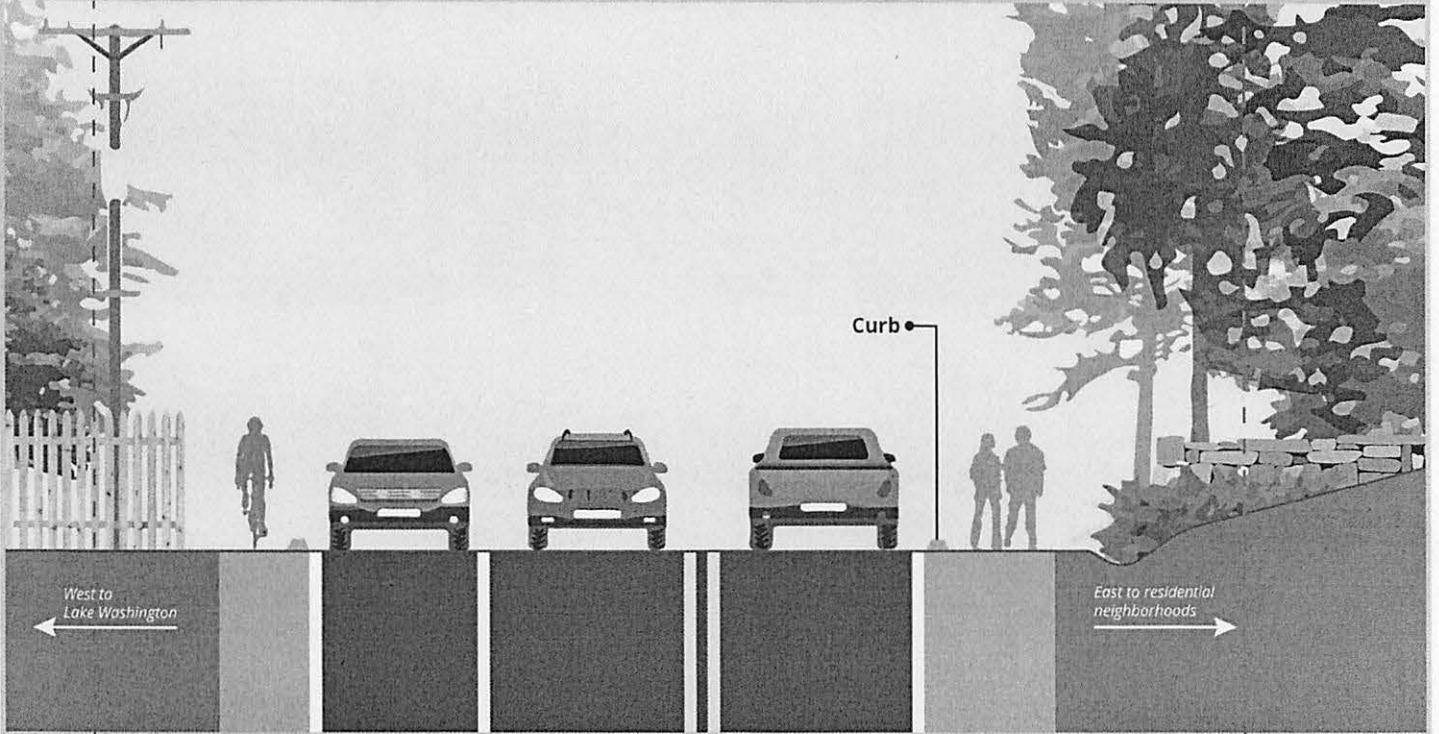
RESIDENTIAL - HPD

ON-STREET PARKING (EAST SIDE) & PROTECTED MIXED-USE SHOULDER



RESIDENTIAL - HPD

ON-STREET PARKING (WEST SIDE) & PROTECTED SHOULDER



4' Protected Paved Shoulder 7' Parking Lane 9.5' Lanes = 19' Driving Space 8' Protected Pedestrian Shoulder

Holmes Point Drive
OO Denny Park
Recommendations

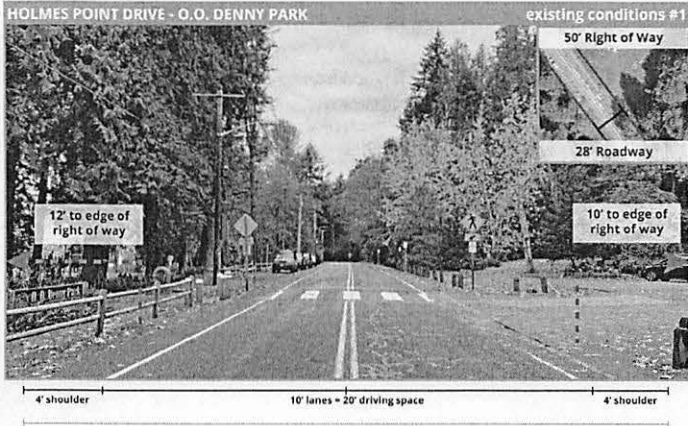
Holmes Point Drive at OO Denny Park

Existing Conditions:

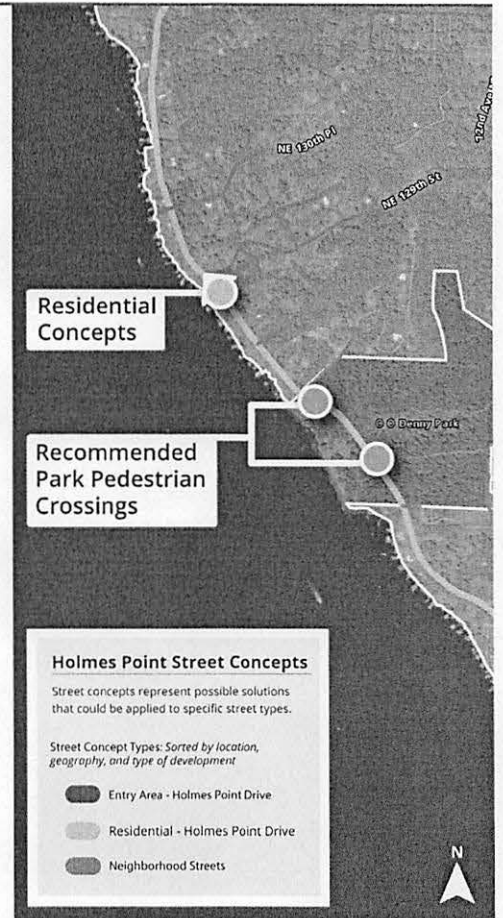
The area of Holmes Point Drive along the frontage of OO Denny Park sees high levels of pedestrians, as well as more vehicle traffic and parking concerns than other segments of the corridor. Parking in this area can be chaotic in summer months, with parked cars overflowing on to the shoulders of Holmes Point Drive and along residential roads. Some vehicles risk parking in the drainage ditch. A pedestrian crossing between OO Denny Park and the parking lot on the east side of Holmes Point Drive is well used, but is at risk from speeding vehicles and a lack of visibility. This area lacks pedestrian crossings on the north side of the park at the intersection of Holmes Point Dr NE and 68th Ave NE. Like most other sections of Holmes Point Drive, the park frontage lacks pedestrian space along the roadway, other than shoulders that are often blocked by park cars. This creates safety conflicts as many people walk from their vehicles and from other parts of Holmes Point to OO Denny Park.

Neighborhood Input:

Residents noted that overflow parking along the park frontage is confusing, and causes congestion and safety concerns for pedestrians. Due to the conflicts between pedestrians and vehicles, the community was interested in providing additional pathways along the park that physically separate people from parked cars and traffic. Moreover, residents were very concerned about speed issues at OO Denny Park, and felt that this area was a high priority segment for traffic calming to reduce speeds.



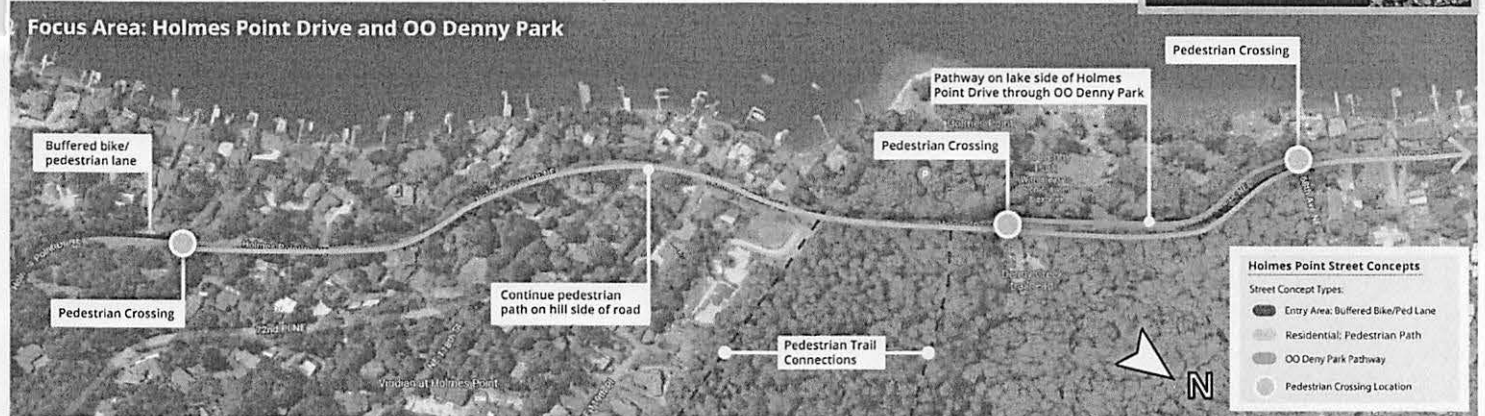
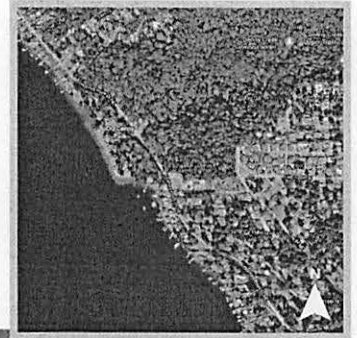
Left: A photo from OO Denny Park shows existing conditions and approximate road dimensions of Holmes Point Drive. The existing pedestrian crossing is shown.



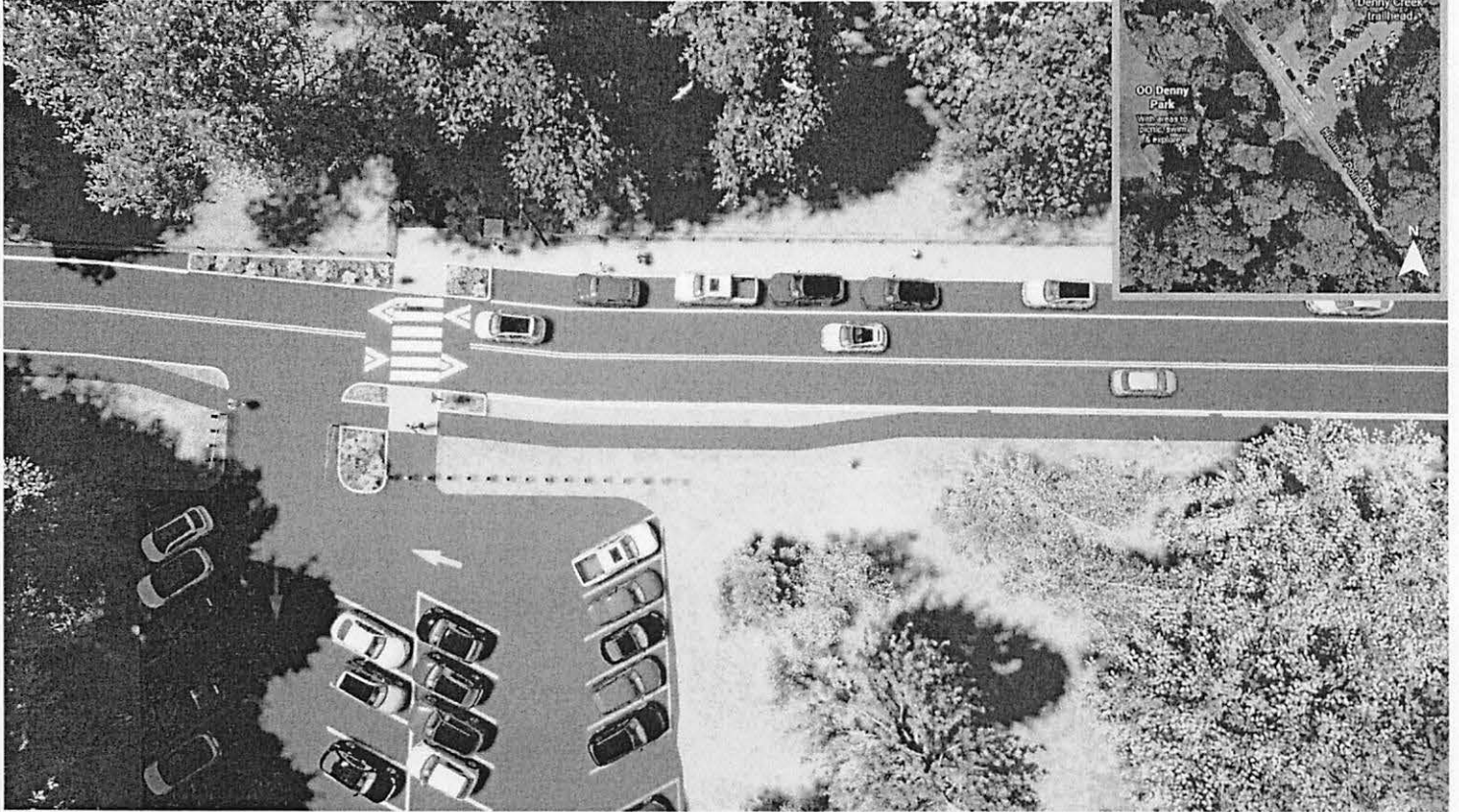
Holmes Point Drive at OO Denny Park

Recommendations:

Recommendations for Holmes Point Drive along OO Denny Park focus on enhanced pedestrian and bicycle safety, experience, and connectivity. Recommendations were developed for traffic calming and alleviating parking concerns. The recommendation for this road segment includes a continuation of the mixed-use shoulder on the east side of the road, shown in the residential area concepts. A new pathway is proposed for the west side of Holmes Point Drive along OO Denny Park. This pathway will allow pedestrians to safely walk along the park away from parked vehicles. In order to connect these two pathways and reduce traffic speed, two raised pedestrian crossings are recommended for the OO Denny Park crossing, where a rectangular rapid flashing beacon (RRFB) is currently located, and the 68th Ave NE intersection. The raised crossing on the north side of the park at 68th Ave NE is recommended to connect to the continuous mixed-use shoulder on the east side of Holmes Point Drive. The curb used to separate the mixed-use shoulder from the driving lane will also prevent overflow visitor parking on the east side of the road. In order to provide safer and more structured parking for OO Denny Park, a parking lane is provided along the western side of the road between the two raised crossings. The map below and renderings on the following pages provide visualizations of this recommendation.



Holmes Point Drive at OO Denny Park

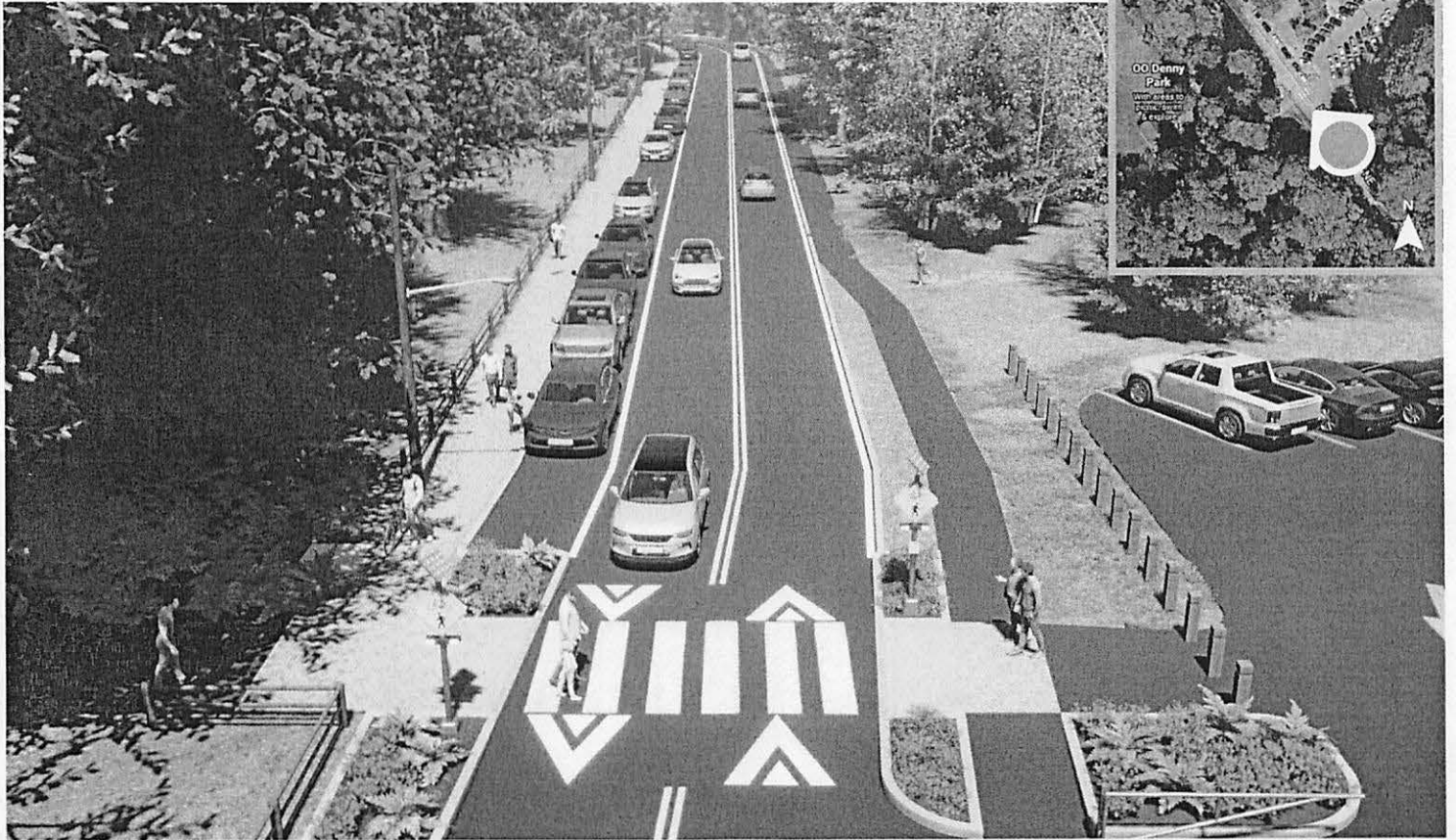


April 15, 2022

Holmes Point Drive at OO Denny Park

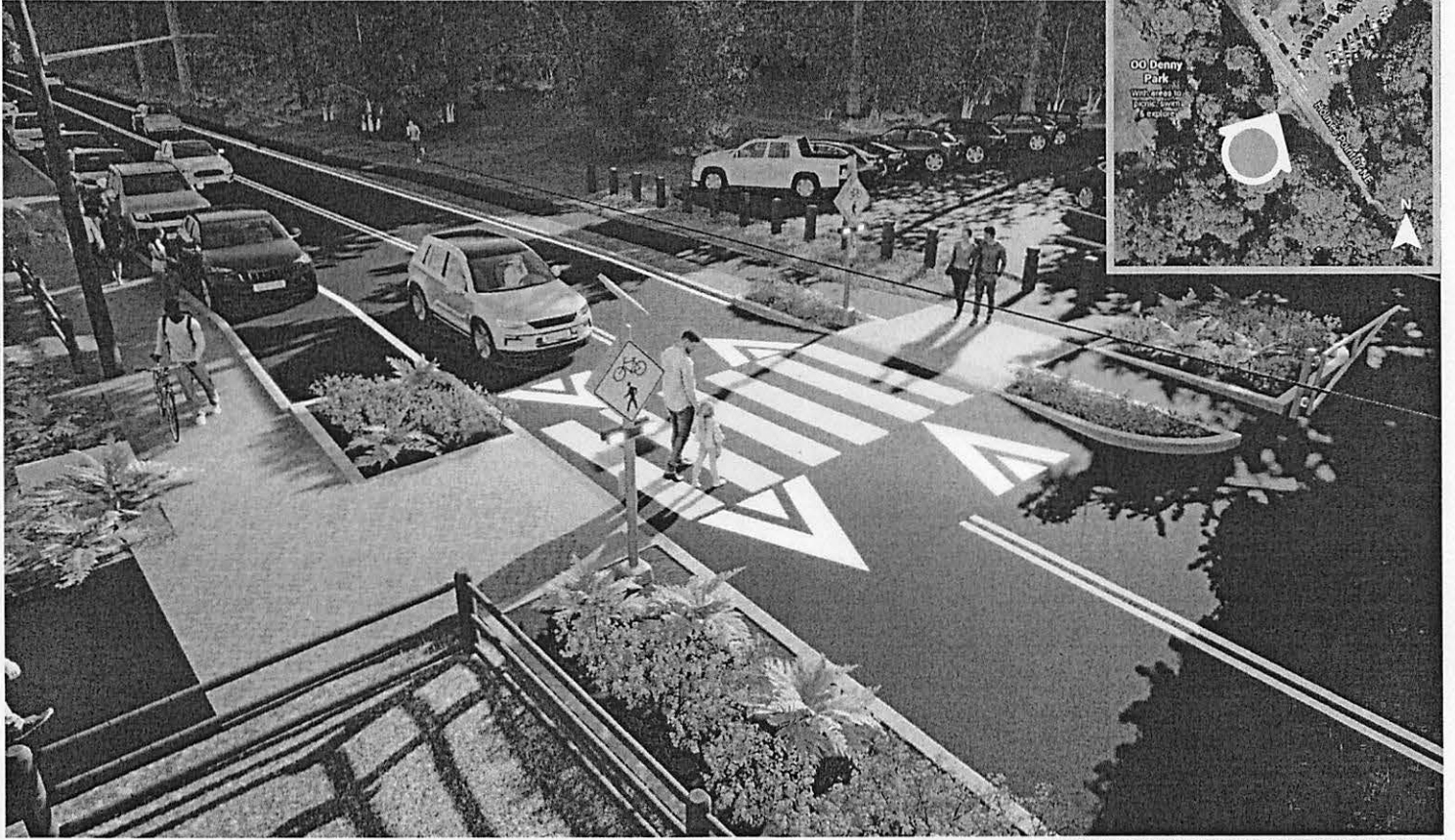


Holmes Point Drive at OO Denny Park

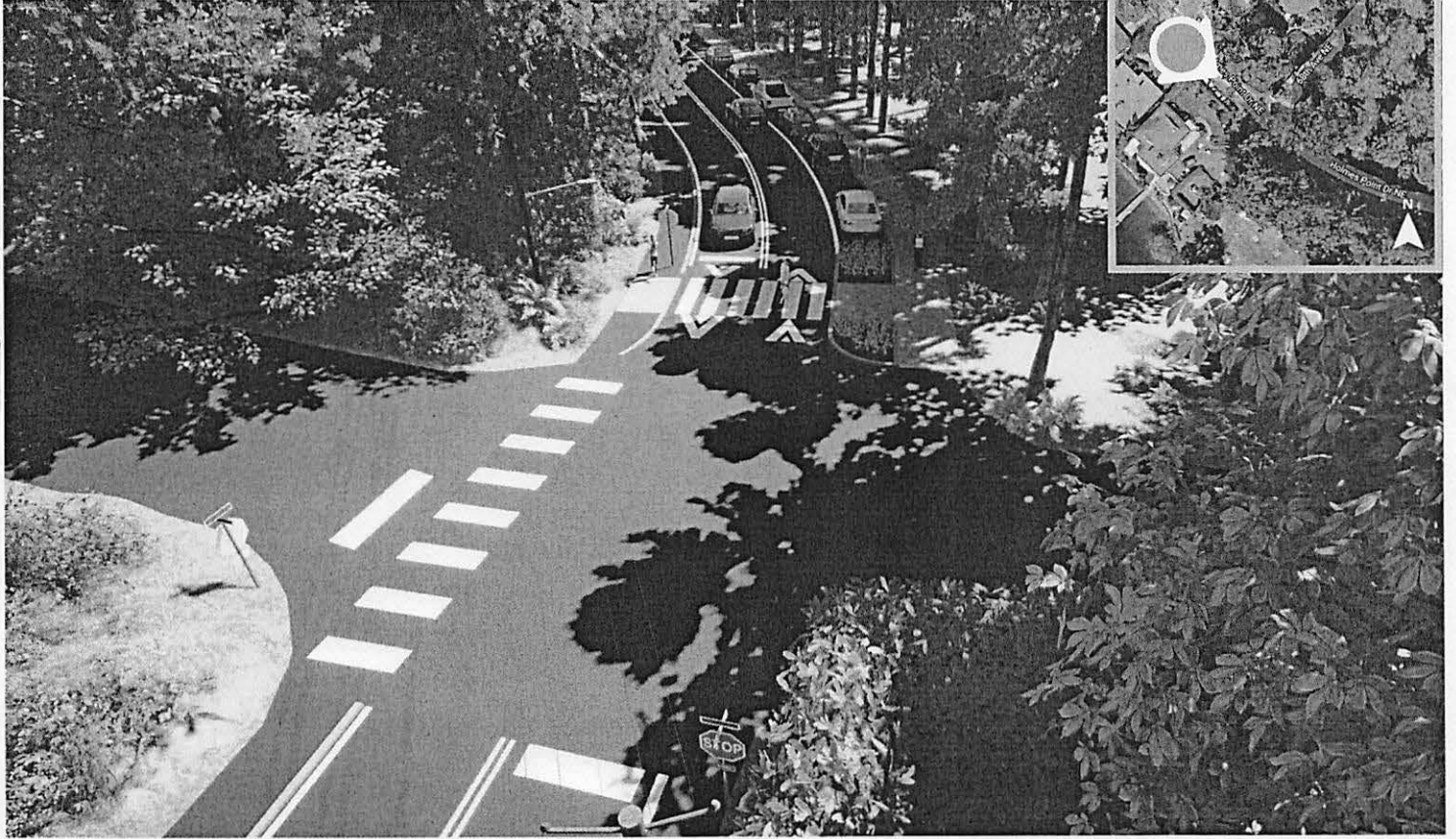


April 15, 2022.

Holmes Point Drive at OO Denny Park



Holmes Point Drive Crossing at 68th St Ave NE



April 15, 2022

Holmes Point Drive Crossing at 68th St Ave NE



Neighborhood Streets

Recommendations

Neighborhood Streets

Street Typologies:

Throughout this study, the treatment of neighborhood street standards has evolved and changed based on community feedback. Originally, the existing conditions field work identified four different types of existing local access neighborhood streets. These streets were divided into the following categories:

- **Type I:** A street that already has sidewalks (or has already been developed to the City of Kirkland street standards)
- **Type II:** A wide street with curbs on at least one side and no pedestrian facilities
- **Type III:** A wide street with no frontage improvements or pedestrian facilities
- **Type IV:** A narrow street with mostly wooded conditions on both sides and without any pedestrian facilities.

The project team intended on recommending a street standard that could be applied to all local access neighborhood streets in order to have a consistent improvement standard. However, through resident engagement meetings and reading resident survey responses and comments, the team received feedback that a single standard should not be applied to all local streets, due to their variability of use. Residents noted that some streets receive almost no street parking, while others are highly used. Additionally, on lower traffic and dead-end streets, separated pedestrian facilities are less necessary than on streets that connect to key pedestrian routes. As a result, the study team decided to provide multiple recommendations for neighborhood streets based on site conditions.

The recommended standards on the following pages should be applied with flexibility. For example, to preserve existing trees. To do this, pathway locations can be separated from the roadway and/or can be designed to meander.

Due to varying road widths, parking needs, and traffic volumes, the original neighborhood street categories were reorganized to better provide recommended standards. The updated street categorizations are listed below and showed in the map at the right.

Neighborhood Streets Classification:

Streets with Existing and/or Proposed Segments of Curb, Gutter, and Sidewalk:

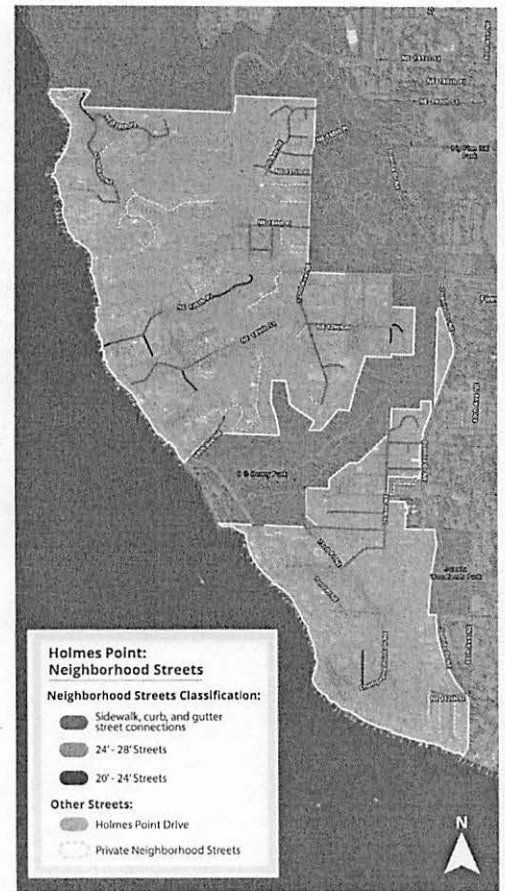
- **Recommendation:** Maintain or continue this standard (with sidewalk on at least on one side of street)

24' to 28' Nominal Pavement Width (Depending on Width):

- **Recommendation A:** Separated mixed-use pathway with planter buffer
- **Recommendation B:** Concrete mixed-use shoulder

20' to 24' Nominal Pavement Width:

- **Recommendation:** Shared roadway for vehicles and pedestrians



Neighborhood Streets: Sidewalk, Curb, and Gutter Street Connections

Existing Conditions:

Streets in this category have already been developed with a curb, gutter, and sidewalk on at least one side, **OR** they connect with other developed neighborhood streets or major pedestrian routes. Streets that were identified as pedestrian connections are connected to Holmes Point Drive or Juanita Drive NE.

Neighborhood Input:

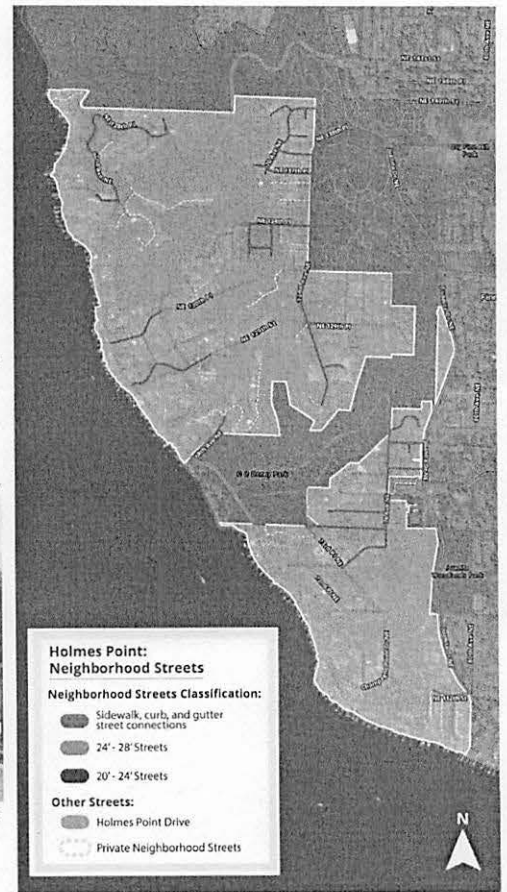
Feedback from residents greatly shaped the final neighborhood streets recommendations. Neighbors shared an interest for having pedestrian facilities in neighborhood streets, but largely thought it was unnecessary to have sidewalks, curbs, and gutters along all local access streets. Due to the community's interest in improving pedestrian connectivity in Holmes Point, residents were supportive of implementing sidewalks in key locations that would connect main pedestrian routes. Many streets in this category have school bus stops for Sandburg Elementary School, Finn Hill Middle School, and Juanita High School, or are used by children to walk to nearby bus routes. Additionally, neighbors told the team that street parking was not used in all neighborhood streets, and space for parking should only be provided in more highly trafficked streets. The neighborhood greatly valued preserving trees in neighborhood streets. For this reason, residents preferred solutions that reduced road widening while providing improved pedestrian facilities.

Recommendations:

Neighborhood streets in this category are recommended to either maintain or build a 5-foot wide sidewalk, 5-foot wide planting strip, curb, and gutter on at least one side of the street. The minimum road width is 24-feet, but can range up to 28-feet in order to provide increased room for street parking. Due to the larger road widths and lower traffic volumes, bicyclists can continue to ride in the roadway, which reduces additional street widening for bike facilities. In order to minimize road widening and reduce removal of trees and private property in the right-of-way, the total width of the roadway can be determined based on existing site conditions. These improvements will be implemented as redevelopment occurs, leading to increased sidewalk connections over time. By providing separated sidewalks along key streets, residents can utilize a safer and more connected option for walking to key destinations.



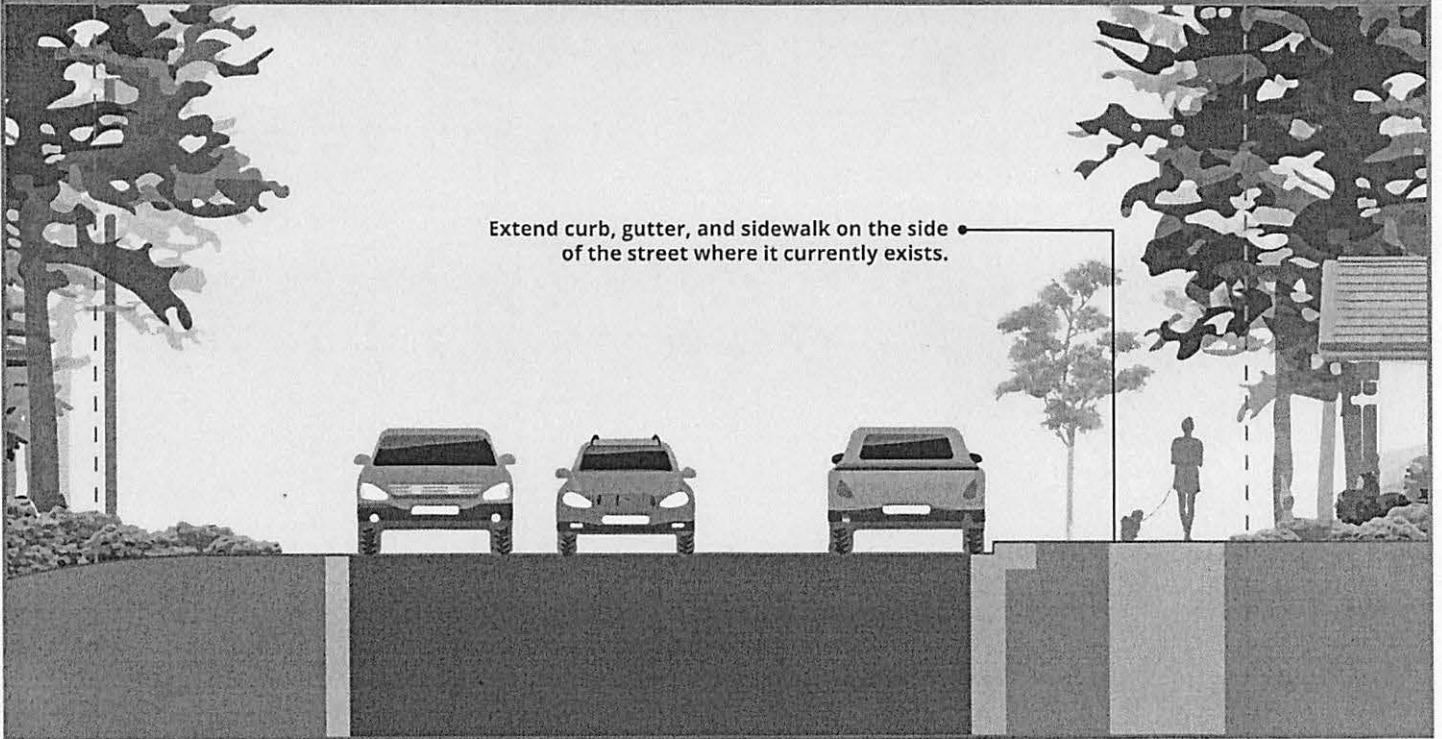
Above: A photo shows existing conditions and approximate dimensions of a redeveloped neighborhood street. This street provides a separated pedestrian sidewalk and wide roadway that can accommodate traffic and street parking.



April 15, 2022

NEIGHBORHOOD STREETS: SIDEWALKS

EXTEND CURB, GUTTER, AND SIDEWALK



24' - 28' Paved Roadway

5' Wide Planting Strip

5' Wide Sidewalk

Neighborhood Streets: 24' - 28' Nominal Pavement Width

Existing Conditions:

Most streets in this category have wider road widths, ranging from about 24-feet to 28-feet. These streets tend to have lower traffic volumes, as they provide local access to driveways. Where the previous streets were considered to be main connection points for pedestrian travel, these streets mainly provide local access to low density residential areas. Streets are often wooded, and may have some private property in the right-of-way.

Neighborhood Input:

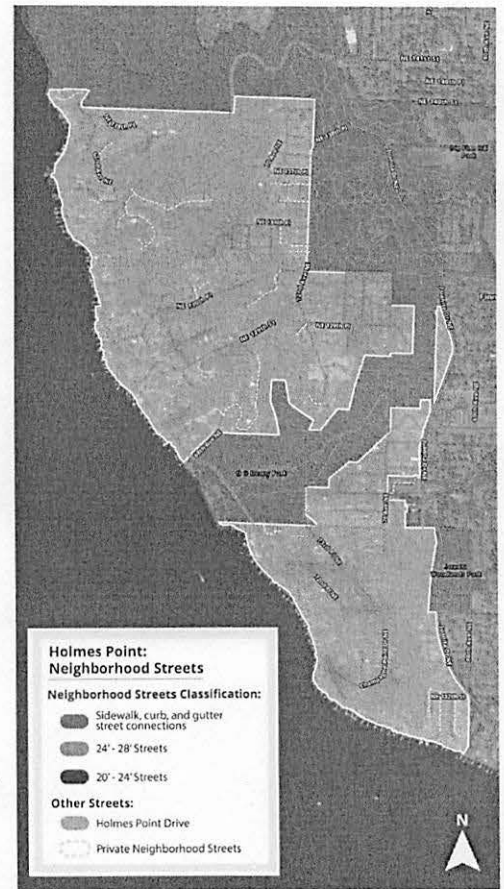
Neighborhood feedback was a major influence on these recommendations, and residents advocated for solutions that would best serve their needs. A majority of neighbors said that they would like to see a separated pedestrian pathway on 24-feet to 28-feet wide neighborhood streets. Several of these streets have school bus stops for Sandburg Elementary School, Finn Hill Middle School, and Juanita High School, or are used by children to walk to nearby bus routes. Residents would like to see enhanced facilities for school children on these streets. However, due to concerns over tree removal and private property relocation, residents felt strongly about developing standards that would reduce the amount of road widening. The community also felt that in many of these streets with lower traffic volumes and speeds, pedestrians, bikes, and vehicles could share the roadway without a separated facility. The project team aimed to take the community's concerns and provide options that maximized pedestrian safety while reducing conflicts due to road widening.

Recommendations:

For this category, two recommendations were developed to allow flexibility, based on site conditions.

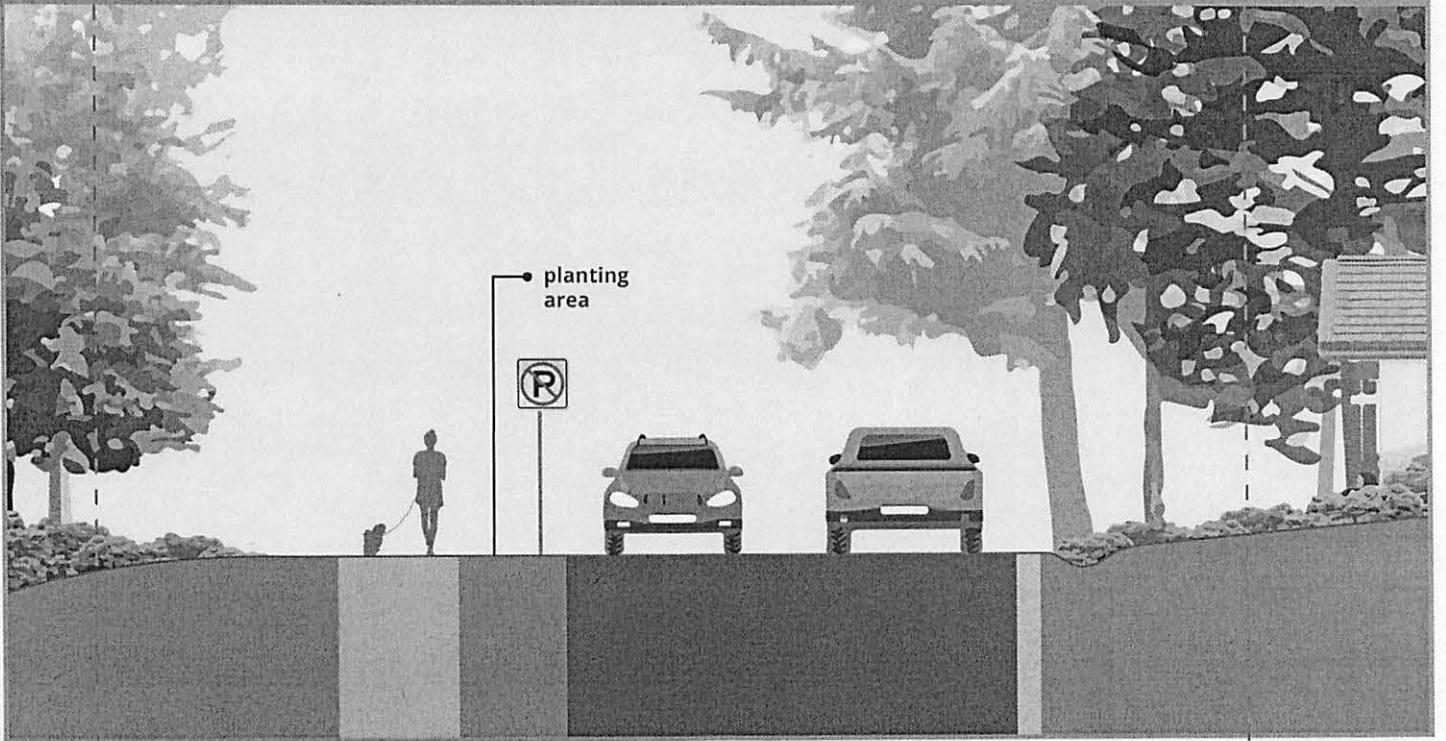
Recommendation A is for a 5-foot wide pedestrian pathway separated from the road by a 4.5-foot wide planting area (based on the City of Kirkland standard). This design will allow for safer pedestrian travel and an attractive street design. To due the lower speed traffic on these streets, bikes can continue to share the roadway with vehicles. A 20-foot minimum roadway width is recommended for these streets, which provides a total minimum width of 29.5-feet for the roadway and pedestrian improvements. This standard could be implemented in areas with wider existing pavement widths, or on streets that have a greater need for separated pedestrian facilities, such as streets that are on the school bus routes.

Recommendation B provides a narrower and lower infrastructure option for neighborhood streets. In this concept, a 5-foot wide concrete shoulder would be provided on one side of the street for pedestrian and bicyclists. This option would provide visual separation from the roadway, while maintaining a smaller pavement footprint in narrower streets. Bicyclists would still have the option of riding in the street, but slower or less confident riders could use the shoulder for more separation from vehicles. With a 20-foot minimum roadway width, this standard could be implemented in the existing footprint of many neighborhood streets, reducing the likelihood of tree removal or property conflicts. This standard prioritizes pedestrians while still working to maintain the existing character of the neighborhoods.



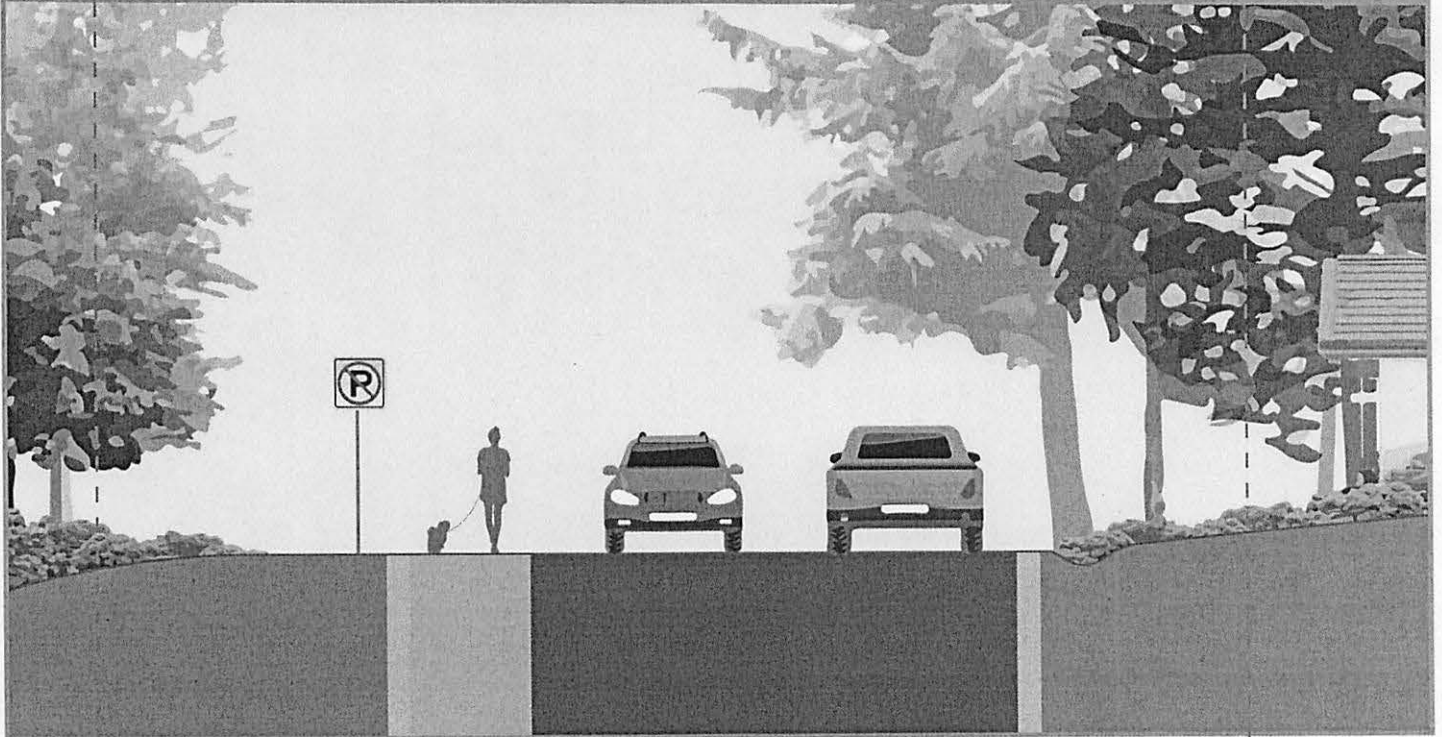
April 15, 2022

NEIGHBORHOOD STREETS: 24' - 28' WIDTH *RECOMMENDATION A: SEPARATED PATHWAY FOR PEDESTRIANS & BICYCLISTS*



5' Separated Pathway 4.5' Planter 20'+ Roadway

NEIGHBORHOOD STREETS: 24' - 28' WIDTH RECOMMENDATION B: CONCRETE SHOULDER FOR PEDESTRIANS & BICYCLISTS



5'
Concrete
Pedestrian
Shoulder

20'+ Roadway

Neighborhood Streets: 20' - 24' Nominal Pavement Width

Existing Conditions:

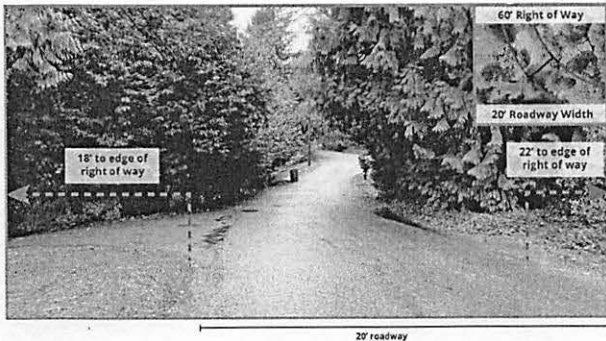
Streets in this category are mainly composed of narrow roadways that often lead to dead ends and driveways. Unlike other neighborhood streets, these roads do not serve as connectors and provide local access for a smaller amount of residents. Traffic volumes on these streets tend to be very low. These streets lack pedestrian facilities and residents tend to walk or bike in the roadway.

Neighborhood Input:

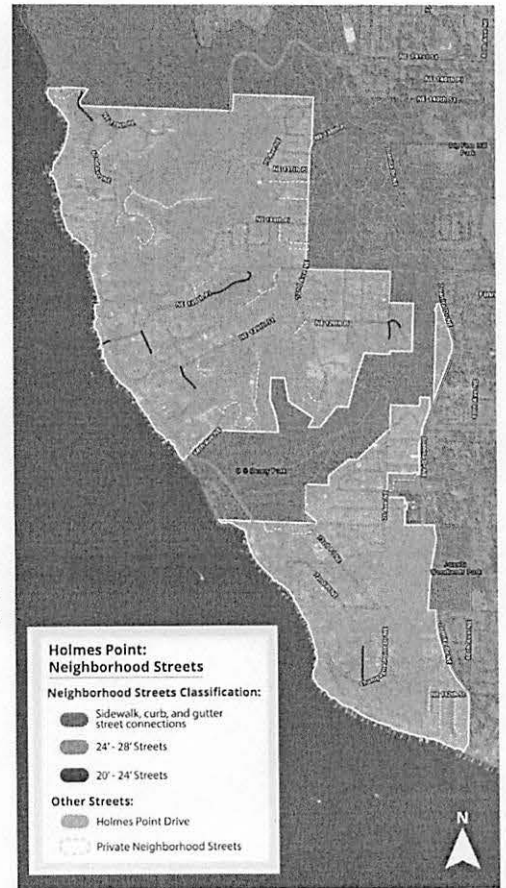
Residents commented that because these streets do not provide connections to main pedestrian routes and traffic is minimal, there is little need to provide improved facilities or widen the roadway. These streets currently serve as mixed-use streets, and the community felt that additional infrastructure was not needed.

Recommendations:

Based on feedback from the community, these streets will continue to operate as shared-use areas and allow for pedestrians, bicyclists and vehicles to operate in the roadway. Due to the low traffic volumes and speeds, roadway widths of 20-feet to 24-feet should provide enough space for these uses. However, parking will not be allowed in order to maintain space for non-motorized use and driveway access. This recommendation aims to minimize any conflicts with existing trees or personal property in order to maintain the character of the neighborhood.



Left: A photo shows existing conditions and approximate dimensions of a 20-foot wide neighborhood street. This street currently operates as a shared roadway for bikes, pedestrians and vehicles.



NEIGHBORHOOD STREETS: 20' - 24' WIDTH

SHARED STREET WITH NO SEPARATION



20'+ Shared Roadway
(Mixed Pedestrian, Bicycle, and Vehicle Use)

20' to 24'



APPENDIX A

Neighborhood Meetings and Public Surveys Summaries

Neighborhood Engagement Meetings

Overview

Public meetings were conducted throughout the project to gather feedback from the local community. Neighborhood residents initially provided input on issues and concerns in the Holmes Point area. Later, workshops were held to present initial recommendation concepts for the Holmes Point Overlay Area and gather neighborhood feedback. Residents provided input on preliminary concepts, which drove the development of the final recommendations. A meeting was also held in December of 2021 to provide revised street section recommendations and gather public input. This last round of input guided the finalization of the recommendations that are included in this document.

December 2, 2020 - Transportation Commission 1

December 3, 2020 - Virtual Public Meeting 1

- In-meeting poll results

- Post-meeting survey results (Survey 1)

May 20, 2021 - Virtual Public Meeting 2

- In-meeting poll results

- Post-meeting survey results (Survey 2)

May 26, 2021 - Transportation Commission 2

December 15, 2021 - Virtual Public Meeting 3

- In-meeting poll results

- Post-meeting survey results (Survey 3)

January 26, 2022 - Transportation Commission 3

December 2, 2020 - Transportation Commission Meeting 1

Page 2



CITY OF KIRKLAND
Public Works Department
123 Fifth Avenue, Kirkland, WA 98033 425.587.3800
www.kirklandwa.gov

MEMORANDUM

To: Joel Pfundt, Transportation Manager
From: Blair Daly, Assistant Transportation Planner / Outreach Coordinator
Armaghan Baghoori, Transportation Program Coordinator
Date: November 24, 2020
Subject: HOLMES POINT STREET STANDARDS AND HOLMES POINT DRIVE CORRIDOR STUDY

RECOMMENDATION:

Staff present progress to date on the Holmes Point Street Standards and Holmes Point Drive Corridor Study to the Transportation Commission on December 2, 2020 and receive their input.

BACKGROUND DISCUSSION:

City Council adopted the Finn Hill Neighborhood Plan unanimously in January 2018. At that time, they also funded a total of \$150,000 for three transportation studies, two of which are the subject of this project: 1) a corridor study of Holmes Point Drive to address some known engineering issues, explore non-motorized options, and examine parking alternatives near O.O. Denny Park; and 2) a study to develop street standards specifically for the Holmes Point area that would help fulfill the purposes of the Holmes Point Overlay zone.

In the Holmes Point area, residents have expressed concerns about having existing Kirkland street standards applied, because that would "suburbanize" their neighborhood and compromise the quality of life. Based on site studies and existing conditions, the City identified that Holmes Point has been developed with inconsistent road standards applied while under King County jurisdiction, and there are several vehicular and pedestrian safety concerns specifically on Holmes Point Drive. Furthermore, due to upcoming new developments within the area, there is a need for adopting compatible street standards to address both pedestrian and bicycle safety as well as storm and surface water issues.

The community is aware that the City does not have funds at this time to implement the solutions that will emerge from this study. It is anticipated that any new street standards adopted for Holmes Point will be implemented over time through infill development.

Project Background

The Holmes Point neighborhood is in the northwestern-most portion of the City, and generally it is west of Juanita Drive and south of Saint Edward State Park (see Study Area Map below). The wooded

residential area was developed mostly under King County's jurisdiction. In 2011, Holmes Point, neighboring Finn Hill, and other neighborhoods were annexed to Kirkland as part of the 7.17-square mile "Juanita/Finn Hill/Kingsgate" annexation.

In 1999, prior to annexation, King County established the "Holmes Point Overlay District." The purpose of the overlay was to be sensitive to the environmental constraints and features of the Holmes Point area, citing steep slopes, landslide hazard areas, erosion hazard areas, limiting impervious surfaces, and protecting tree cover and wildlife habitat. After annexation, Kirkland adopted the overlay and codified it as chapter 70, "Holmes Point Overlay Zone," of the *Kirkland Zoning Code*.

The roadway network in the Holmes Point area does not follow a grid and several different road standards have been applied over the decades as the area has developed. There is one main collector street, Holmes Point Drive, with relatively few through streets, many dead-ends, roads of differing widths and character, and few non-motorized transportation facilities.

Beginning in 2016 and lasting two years, the City undertook a planning process with the community to develop the first City of Kirkland neighborhood plan for the area. With regard to transportation planning, two topics generated a considerable amount of discussion. One was street connectivity both in the Holmes Point and Finn Hill areas. Another was Kirkland's adopted street standards, which are used throughout the City. Even though the community supported a down-zone of the area from 6-8 homes per acre to a maximum of 4 per acre, there still is opportunity for infill development. Many in the Holmes Point area were concerned that the City's street standards would be applied to that infill development, which some felt would change the character of the area and would not respond to its natural features.

December 2, 2020 - Transportation Commission Meeting 1

Study Area Map



Scope of Work

The City has contracted with Otak Inc. to develop recommended street standards for Holmes Point that meet legal and engineering design requirements but also are contextual for the area. There may be a range of standards depending upon the specific type of road or non-motorized facility that is being built or improved. Given the topography of the Holmes Point area, its vegetation, and the character of its development, the standards should incorporate a degree of flexibility.

This project will develop a suite of standards, guidelines, concepts, and solutions, along with analysis and recommendations pertaining to the following elements:

- o Street cross section options for various street types
- o Preferred lane widths (minimum, optimum) for Holmes Point Drive
- o Lighting
- o Holmes Point Drive parking policies and design concepts, particularly at O.O. Denny Park
- o Speed limits
- o Signage
- o Stormwater management and green infrastructure
- o Publicly-built walls and fences
- o Pedestrian circulation and facilities
- o Bicycle circulation and facilities
- o Roadway crossings (mid-block and intersections)
- o Multimodal and road sharing considerations and innovative concepts such as yield roadways or shared street segments
- o Tree preservation and management; natural areas considerations
- o Utilities and roadside features

Project Timeline

November 4	Kickoff meeting
December 2	Transportation Commission – Holmes Point Study on the agenda
December 3	Public engagement meeting – Round 1
Nov-Jan 2021	Otak drafts findings and preliminary recommendations
February	Mid-term meeting to discuss preliminary work
Jan-Feb	Public engagement meeting – Round 2
Feb-March	Transportation Commission – Review and comment on preliminary work
Feb-March	Otak finalizes recommendations
April 2021	Otak submits final report
TBD	Recommendations brought to Transportation Commission and City Council for input

Engagement Plan

City staff will lead the public engagement effort for the project. The consultant team will provide support at one or more virtual public engagements meetings. The first engagement session is scheduled for Thursday, December 3, 2020 at 7:00 PM via Zoom. Throughout this 75-minute online meeting, City staff and consultant staff will explain the study and receive input from the public. City staff have worked closely with Planning Department staff who were involved in the Finn Hill Neighborhood Plan process to coordinate outreach efforts in the neighborhood.

December 2, 2020 - Transportation Commission Meeting 1

Page 5

The second round of public engagement is expected to be scheduled late January 2021. If additional public meetings are needed to finalize the street standards and recommendations, City staff will schedule a third session in February or March of 2021.

December 2, 2020 - Transportation Commission Meeting 1

Major Themes and Comments

- Storymap link: <https://storymaps.arcgis.com/stories/a986ba3bf239483c82f73240c88fb598>
- Round 1 public engagement begins tomorrow with same presentation powerpoint.
- Is there enough time to get feedback since the presentation 131st/90th Project ran long and used this time? This meeting will be the beginning of the outreach conversation. Comment box available throughout meeting based upon topic (1 Holmes Point corridor, 2 OO Denny, 3 Neighborhood streets).
- Concern about residents adding comments when they are not sure what they are looking at. Questions will be based on user experience, and Staff will utilize more time to go through StoryMap. Link will be shared if residents want to review StoryMap on their own time to respond to the survey.

Holmes Point Street Standards & Holmes Point Drive Corridor Study

CITY OF KIRKLAND
PUBLIC WORKS DEPARTMENT

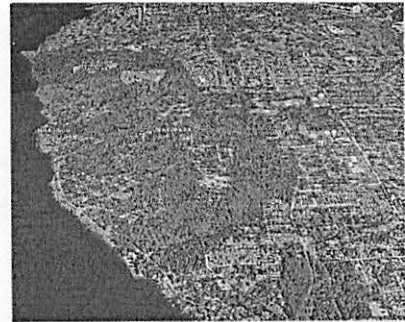
PROJECT CONSULTANT



Greetings and introduction

- Study project
 - Developing street standards for the Holmes Point Overlay area and safety improvements along Holmes Point Drive

- Project team
 - City of Kirkland staff
 - Otak staff



Agenda and format of the session



Poll questions



Project background



Study area review and analysis + feedback



Next steps



Poll questions

December 3, 2020 - Neighborhood Meeting 1

Project background

- Development with inconsistent street standards
- Several vehicular and pedestrian safety concerns along the corridor
- Anticipated development in the area
- Require unique street standards that recognize the specific nature of the Holmes Point area
- No funding allocated yet toward implementing its recommendations



Street Standards

- Street cross sections for various street types, including lane widths
- Lighting, utilities and roadside features
- Parking policies and design concepts for Holmes Point Drive, like at O.O. Denny Park
- Speed limits
- Signage
- Stormwater management and green infrastructure
- Publicly-built walls and fences
- Pedestrian circulation/facilities
- Bicycle circulation/facilities
- Tree preservation and management; natural areas considerations within street right of way



Poll question

Otak's
Presentation

Storyboard



Slide 8

Holmes Point Area Character

WHICH WORD BEST
DESCRIBES HOLMES
POINT AREA
CHARACTER?

[Use one word to be
added to the wordle]

Next steps



SurveyMonkey link via email



Upcoming project webpage



Subscribe via GovDelivery list for
updates



Contact information:

Armaghan Baghoori | abaghoori@icf.com
425 587-3924

December 3, 2020 - Feedback from Public Meeting 1

December 3, 2020 - Virtual Public Meeting Feedback

The notes below summarize the main issues and concerns that were voiced by the community at the virtual public meeting.

Major Themes

Holmes Point Drive

- Needs safer walking conditions.
 - Corridor is used by pedestrians heavily, especially at OO Denny Park.
 - Safe walking conditions don't always require sidewalks. Other creative 'walkway' solutions available too.
- Biking conditions are so-so.
 - "Going downhill on dark, wet curvy road with poor pavement on a bike is sketchy! Riding up hill in a curve where there's not a safe opportunity for cars to pass is unsafe."
 - "Biking on HPD is actually very good, in my view, because most drivers are considerate. I feel comfortable cycling on the road."
- Slow the cars.
 - Add traffic circle at HPD & 62nd Ave to slow southbound traffic (presently there is a radar speed display sign here).

Neighborhood Streets

- Little conflict between cars and pedestrians. May not need sidewalk.
- This could be a compromise when it comes to preserving trees: maybe on Holmes Point Drive community could accept tree loss for pedestrian facilities, but not on neighborhood streets.

OO Denny Park

- Parking scene is chaotic, dangerous, and beyond overflowing.
 - Cars end up in the drainage ditch on east side of road.
- Put in a trail-like walkway on the east side of the roadway, just inside the trees. More affordable than sidewalks.
- OO Denny has soft bulkhead (wood) verses concrete because neighbors told the county at that time they wanted soft.

Preserve neighborhood character

- Add quaint and cozy features like lampposts (instead of overhead street lights) and strategically placed park benches.
- Community should discuss street lighting options.
- Protect trees.
- Have various flexible approaches for standards for different street.

December 3, 2020 - Feedback from Public Meeting 1

December 3, 2020 - Virtual Public Meeting Feedback

Major Themes (continued)

Other requests related to street standards and corridor study:

- Underground the unsightly utility lines.
- Improve 72nd Ave NE between NE 132nd and NE 129th.
 - Add guard rail along the west side at steep cliff.
 - Improve lighting.
 - Create space for pedestrians.
- Avoid the term "rural" as one of the neighborhood street types.
- Consultant should know the results of speed studies carried out in past by City along Holmes Point Drive.
 - Avoid saying that most motorists travel the speed limit.

December 3, 2020 - Feedback from Public Meeting 1

In-Meeting Poll / Comment Summary

Topic	Sub-Topic	Feedback or questions	
Topics	Do you frequently walk or bike in Holmes Point area?	14. No 50. Walk 56. Walk and bike	
	Need for sidewalk on HP Drive	A sidewalk to HP Drive (Specifically Waterfront) would improve safety for walkers. Many people frequent the area and have to walk in the road. Utility poles and ditches are noted also force people to walk in the road.	
Sub-Topics	Need of a stronger vehicle barrier near the playground		
	Data on overflow parking and accidents	Installed "No parking signs" recently addressing "No parking signs" recently Full length of the corridor is more of a bike and pedestrian traffic and parking scene is chaotic. Opportunity to put a trail on the east side of the roadway Complete the trail all the way to connect to Big Horn Hill Park	
Neighborhood streets	Labeling street types are not well descriptive	Construction workers parking along these roads have impeded emergency vehicles. These roads do not have "no parking" signs. Making pavement wider, but not to increase speed	
	Various and flexible approaches on streets	determining whether the street represents a traffic choke point. In other words, is this street a single point of access for people who live in an area. If so, the street should be required to be wider such as 28', or no parking should be allowed along it. Consideration should be given to whether disabling parking would cause parking issues in surrounding streets.	
	Improving ROW	speeds entering and exiting HPD, at least at our southern end, are very high vehicles speeding up to 40 mph coming downhill we get buffeted by the cars as we walk - 45 mph easily is common Traffic calming strategies	
	Sub-Topics	Detailed feedback or questions	14. No 50. Walk 56. Walk and bike
		Need for sidewalk on HP Drive	A sidewalk to HP Drive (Specifically Waterfront) would improve safety for walkers. Many people frequent the area and have to walk in the road. Utility poles and ditches are noted also force people to walk in the road.
		Need of a stronger vehicle barrier near the playground	
		Data on overflow parking and accidents	Installed "No parking signs" recently addressing "No parking signs" recently Full length of the corridor is more of a bike and pedestrian traffic and parking scene is chaotic. Opportunity to put a trail on the east side of the roadway Complete the trail all the way to connect to Big Horn Hill Park
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		Improving ROW	speeds entering and exiting HPD, at least at our southern end, are very high vehicles speeding up to 40 mph coming downhill we get buffeted by the cars as we walk - 45 mph easily is common Traffic calming strategies
		Other topics that were not covered?	Discussion speeding problem on the HPD backed by data and speed studies. The panelist shared that she had the most motorists drove the speed limit was misinformation. There have been many speed studies done on HPD by the city and before them the county and all have found the average speed along segment 3 to be 32mph.
		Overall session feedback	Looking forward to next neighborhood opportunity and HPD gets the priority over neighborhood streets
		%12 Good, %88 Very Good and Excellent	
31 attendees			

December, 2020 - Feedback from Survey 1

External Survey Link
Open 12/3/2020 – 12/8/2020

Participant #	Start Date	End Date	What are the important character-defining features of the Holmes Point area to you?	What are the most important aspects of the Holmes Point area that you want to preserve and enhance when street design standards are adopted?	What is important to you to be considered for the street design standards and recommendations that will result from this study?	Are there any topics you hoped would be covered during the engagement session that were not covered?	Overall, how would you rate the online community engagement event?	What did you like or dislike about the event?	Which age group do you belong to? (Optional)	Do you experience a disability or other condition that affects your choice to walk or bike? Please select all that apply. (Optional)	Disability not listed (Please specify)
			Open-Ended Response	Open-Ended Response	Open-Ended Response	Open-Ended Response	Response	Open-Ended Response	Response		Open-Ended Response
1	2020-12-05 07:30:58	2020-12-05 07:40:43	Access to OO Denny park, the boat and an undeveloped lakefront/trails.	Walkability, biking.	Preserve and better define parking, particularly along HPD and for OO Denny Park. Safer walking on HPD where shoulders don't exist. The side streets have little traffic with little conflict between non motorized and motorized users.	The City has traffic studies on HPD that the consultant didn't send to know about. The data weighted haven been helpful when discussing speeding problems.	Very Good	Well organized, information was well presented. A few of the graphics were difficult to read - sidebar text sometimes too small.	65+		None
2	2020-12-04 10:25:30	2020-12-04 10:28:16	Trees	Lots of tree coverage, pedestrian safety should be improved.	Building street standards that are not "one size fits all." Have flexibility in applying different types of street designs in different areas to solve specific problems without forcing a single standard everywhere.	No	Excellent	The storymap was great and informative, I enjoyed the polls.	25-44		None
3	2020-12-04 08:14:45	2020-12-04 08:28:12	Park-like setting with views of lake. Entries at north and south end are like going into a forest. Pastoral and tranquil	Trees, lake views and neighborhood tranquility	Continuous shoulders along Holmes Point Drive to provide safety to pedestrians and cyclists. A traffic circle on H.P. at 62nd Ave would slow traffic coming from north	pavement on Holmes Point south of Denny Park very bad because of an incomplete subdivision and a subpar repair following a sewer project. Rough for cyclists & cars	Very Good	Spent too much time telling us what we already know about the condition of Holmes Point Drive	65+		None
4	2020-12-03 20:32:53	2020-12-03 20:39:14	Green, trees and soft	Trees. Because of the aesthetic and scientific value they provide	Traffic calming. If people drove the speed limit there would not be a public safety issue for pedestrians, bikers or the motorist	Speed studies. The panelist that shared she felt most motorists drove the speed limit was misinformation. There have many speed studies done on HPD by the city and before them the county and all have found the average speed along segment 3 to be 32mph.	Very Good	Provide us facts and data. Do not share hunches or gut feelings.	45-64		None
5	2020-12-03 20:19:41	2020-12-03 20:31:52	An area where people enjoy walking and spending time outdoors in community areas where they can frequently visit with their neighbors while enjoying the beauty of nature and our wonderful lake.	I'm more concerned with enhancing the soul of Holmes Point than what is likely to be harmed. Trees in yards and parks certainly need to be preserved, but the way needs to be far more pedestrian friendly to enhance the outdoor walking and socializing soul of the neighborhood.	Less on-street parking taking up city land that should benefit all, ample sidewalks in the waterfront area (ideally with planting strips), undergrounding of ugly utility lines. Also, quaint and cozy features like lampposts (instead of overhead street lights) and strategically placed park benches. This should become the most walkable neighborhood in Kirkland, as it is so valued by residents and is inherently suited to that (despite historical neglect in this regard).	We need to talk about the power lines, and the street lighting options.	Very Good	It was a good first start. I look forward to talking about more specific options in future meetings.	25-44		None
6	2020-12-03 20:06:23	2020-12-03 20:09:50	Community within the Kirkland Community, Walking, Hiking, Biking, O.O. Denny Park	Safety for walkers. O.O. Denny Park is a neighborhood park. Not a regional or City wide park.	Shoulders are not sufficient.	You nailed it. Covered them all. Nice work!	Excellent	Excellent presentation. Captured all the issues and challenges. Please focus on Holmes Point Drive. Neighborhood/side streets are way down the list. Thank you!			
7	2020-12-03 20:03:17	2020-12-03 20:07:30	beaches, trees and trails, summer trees and the summer vacation vibe	streets and the summer vacation vibe	how to give pedestrians and bicyclists a safer space to move about the congested beach area and the "access" roads	no	Excellent	the ability to provide input during the event	65+	Physical mobility limitations	
8	2020-12-03 20:04:20	2020-12-03 20:06:28	nature, trees, lake	streets are public spaces	safe access for people walking and biking		Good	limited engagement with community members	25-44		None

May 20, 2021 - Neighborhood Meeting 2



May 20, 2021 

Holmes Point Overlay Zone Street Design Standards & Holmes Point Drive Corridor Study

Tonight's Presentation

1. Project Purpose and Key Challenges
2. December Public Meeting - What We Heard
3. Pedestrian and Bicycle Facilities
4. Holmes Point Drive Cross Section Concept Designs
 - Breakout Room Discussion
5. Local Access Neighborhood Street Cross Section Concept Designs
 - Breakout Room Discussion
6. Wrap Up and Thank You

Project Purpose and Key Challenges

Purpose:

Due to the history of development in the Holmes Point area, there has been development with inconsistent street standards. With anticipated development occurring in Holmes Point, the City needs to develop street standards for the Holmes Point Drive area.

There is no funding allocated yet specifically toward implementing the recommendations that will come from the study. The City will explore options for unique street standards within the Holmes Point Overlay Zone that recognize the unique nature of the Holmes Point area, but it will not compromise on vehicular and pedestrian safety.

Project Purpose and Key Challenges

Key Challenges:

- Existing development occurred without consistent street standards
- Vehicular safety concerns
- Pedestrian safety concerns
 - Pedestrians walking along Holmes Point Drive
 - School children awaiting bus pickups
- Environmental constraints
 - Steep slopes/topography
 - Mature trees
 - Stream crossings/drainage
 - Built features in right of way

Slide 4

May 20, 2021 - Neighborhood Meeting 2

December Public Meeting - What We Heard

December 2020 - Public Workshop for Residents

Key Comments:

- Preserving neighborhood character is a high priority
- Important to have a flexible approach for different streets

Holmes Point Drive Corridor

- Need for safer walking and biking conditions
- Speed is an issue and traffic should be slowed
- Prefer less "urban" pedestrian solutions
- Address the safety challenges on hills

What We Heard — cont.

OO Denny Park

- Challenges with overflow parking and cars parked along the roadway
- Safety concerns for pedestrians
- Additional pedestrian paths are needed
- Interested in more aesthetic improvements that fit the neighborhood

Local Access Neighborhood Streets:

- Few existing safety issues, cars, bicyclists, and pedestrians are able to share the pavement
- Interest in pedestrian connectivity
- Important to consider tree preservation

Holmes Point Neighborhood Street Typologies



Holmes Point Street Concepts

Street concepts represent possible solutions that could be applied to specific street type.

Street Concept Types: Sorted by location, geography, and type of development

- Entry Area
- ||| Transition Area
- Waterfront
- OO Denny Park
- Neighborhood Streets

Pedestrian and Bicycle Facilities

Slide 8

May 20, 2021 - Neighborhood Meeting 2

Pedestrian and Bicycle Facilities

Shared Facilities

Vehicles, pedestrians, and bicyclists share the roadway.

- Advisory shoulder
- Sharrow

Visually Separated

Vehicles, pedestrians and bicyclists have separate painted areas of the road.

- Pedestrian/ bike lane

Physically Separated

Vehicles, pedestrians and bicyclists have physically separate facilities

- Buffered pedestrian/ bike lane
- Separated path

Shared Facilities

Advisory Shoulder

A visually distinct area on the edge of the roadway, offering a prioritized space for people to bicycle and walk

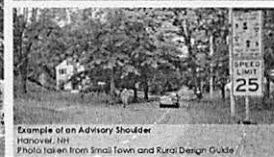
Vehicles share the center lane, and use the shoulder if necessary for passing



Example of an Advisory Shoulder
Hanover, NH
Photo taken from Small Town and Rural Design Guide



Example of an Advisory Shoulder Signage
Votawick, Canada
Photo taken from Oak Bay News



Example of an Advisory Shoulder
Hanover, NH
Photo taken from Small Town and Rural Design Guide

Shared Facilities

Sharrow

Bicyclists share the roadway with vehicles



Example of a Sharrows in the Roadway
Beavertown, NC
Photo taken from Small Town and Rural Design Guide



Example of a Sharrows in the Roadway
Bowling Green, OH
Photo taken from Big Independent News

Visually Separated Facilities

Pedestrian Lane/Bike Lane

Separate path for pedestrians and bicyclists on the roadway



Example of a Shared Pedestrian and Bicycle Lane
Detroit, MI
Photo taken from Small Town and Rural Design Guide



Example of a Bike Lane
Provincetown, MA
Photo taken from Bike Provincetown



Example of a Pedestrian Lane Using Pavement
Calaveras Valley, CA
Photo taken from Small Town and Rural Design Guide

Slide 12

May 20, 2021 - Neighborhood Meeting 2

Physically Separated Facilities

Buffered Pedestrian/Bike Lane
Separate path for pedestrians and bicyclists buffered from vehicles by a curb and/or "candlesticks."



Physically Separated Facilities

Separated Pathway
Separate path for pedestrians that are buffered from vehicles by planting strip.



Physically Separated Facilities

Sidewalk curb and gutter



Speed Management Solutions

Slide 16

May 20, 2021 - Neighborhood Meeting 2

Speed Management Solutions

Neighborhood Slow Zone

The City could consider designating the Holmes Point Overlay as a "Neighborhood Slow Zone"

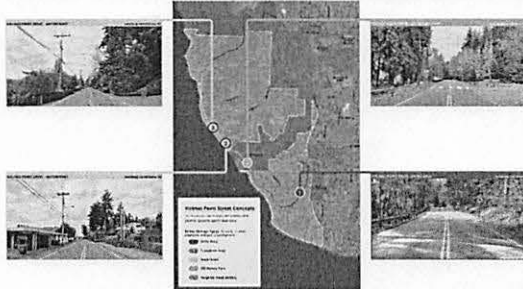
- Reduce speed on all roads from 25mph to 20mph
- Create a gateway experience to the neighborhood by narrowing the roadway at all neighborhood entries
- Install signage and pavement markings



Cross Section Concepts

Existing Conditions Locations

Holmes Point Drive



Holmes Point Drive Entry Area Cross Section Concepts

Slide 20

May 20, 2021 - Neighborhood Meeting 2

Existing Conditions Locations

Holmes Point Waterfront Area

Holmes Point Drive

Holmes Point Waterfront Area

- Waterfront Area
- Waterfront Area
- Waterfront Area
- Waterfront Area
- Waterfront Area

Existing Conditions

Holmes Point Drive

4' shoulder

17' to edge of right of way

10' lanes = 20' driving space

25' Roadway

60' Right of Way

17' to edge of right of way

2' shoulder

20' existing improvements

Proposed Minimum Standard

Holmes Point Drive

5' protected bike lane

10' lanes = 20' driving space

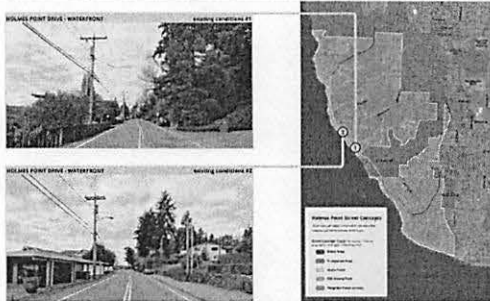
20' roadway

Holmes Point Drive Waterfront Area Cross Section Concepts

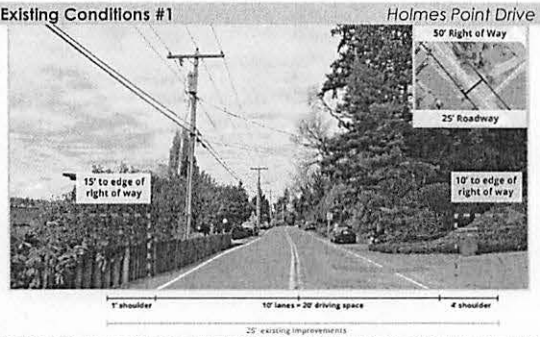
Slide 24

May 20, 2021 - Neighborhood Meeting 2

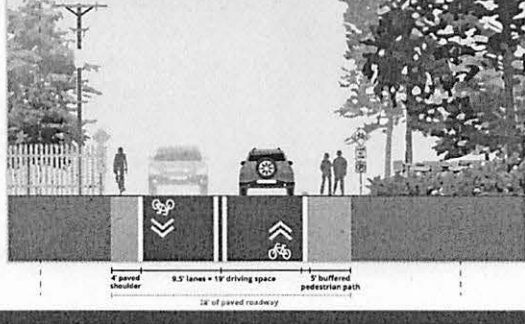
Existing Conditions Locations



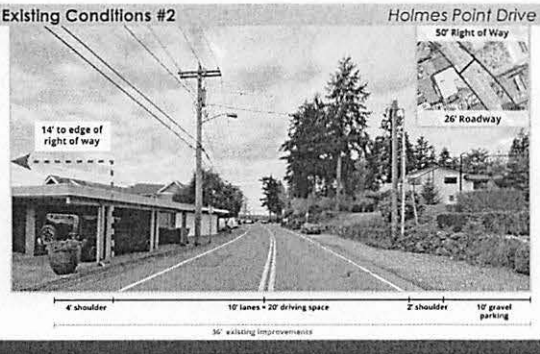
Existing Conditions #1



Proposed Minimum Standard

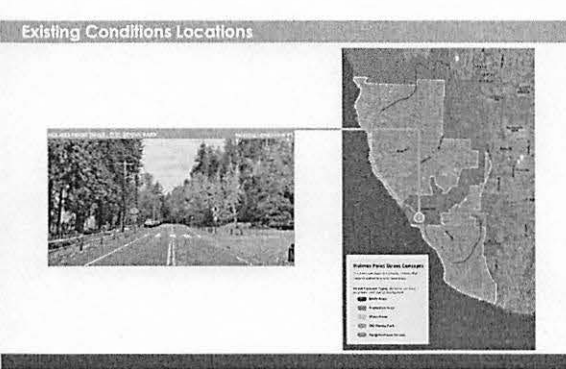
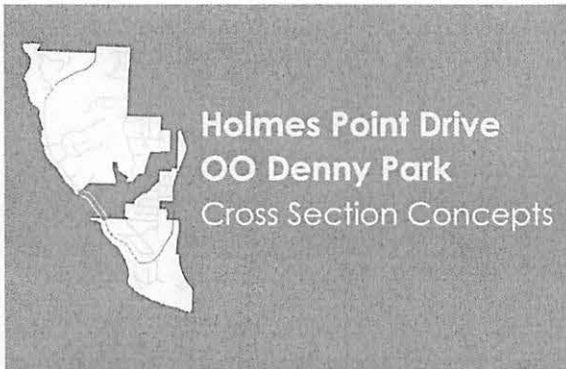
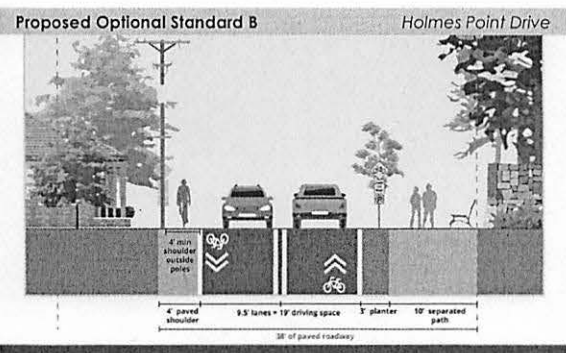
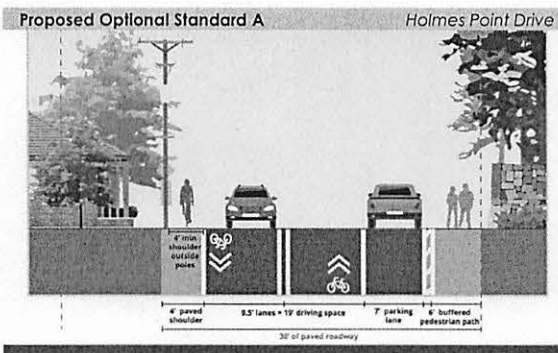


Existing Conditions #2

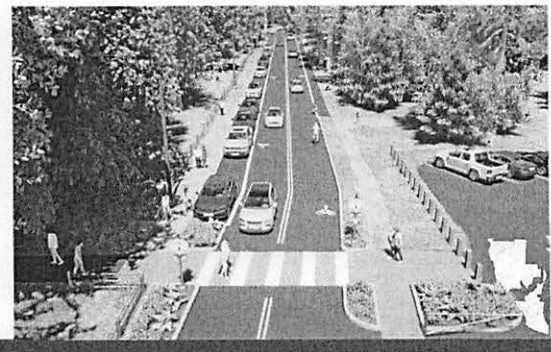
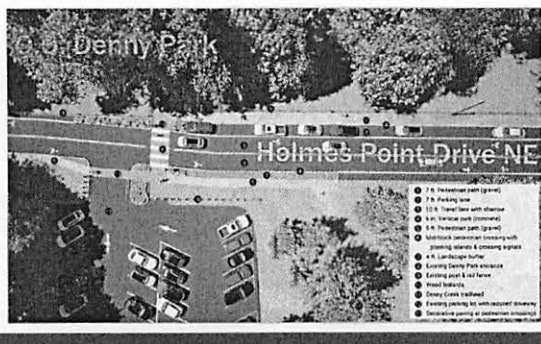
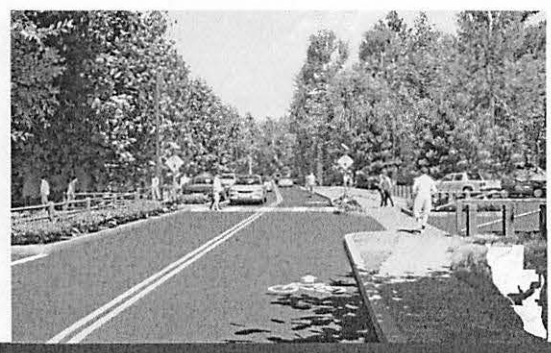
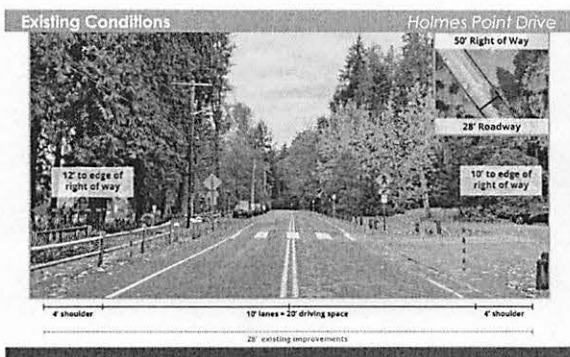


Slide 28

May 20, 2021 - Neighborhood Meeting 2



May 20, 2021 - Neighborhood Meeting 2

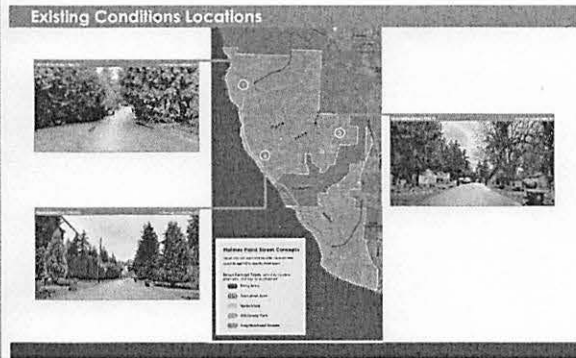
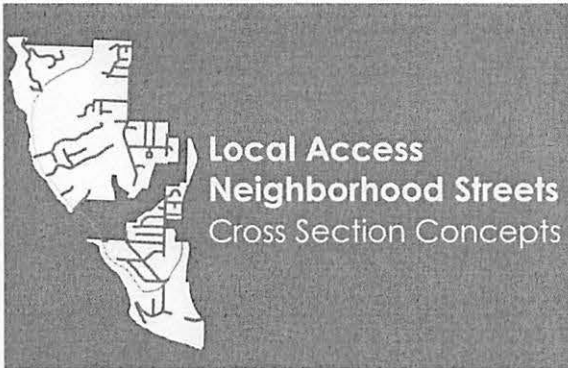


Slide 26

May 20, 2021 - Neighborhood Meeting 2

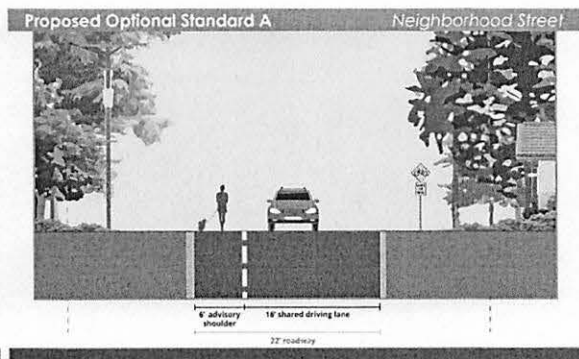
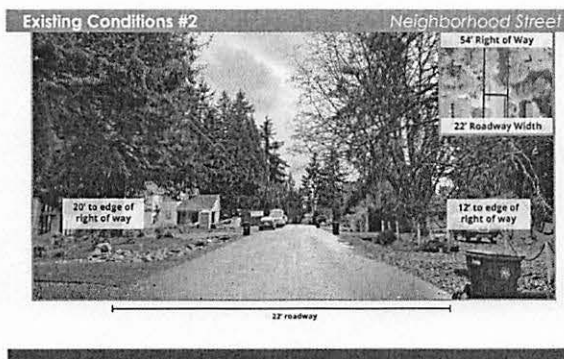
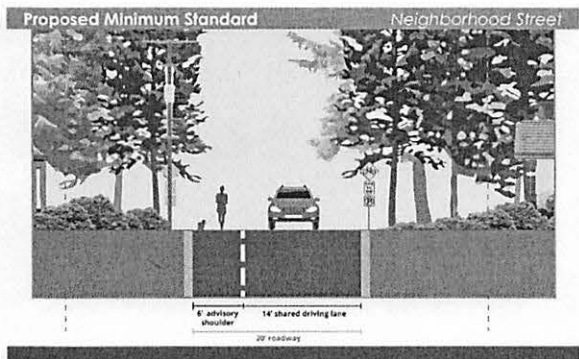
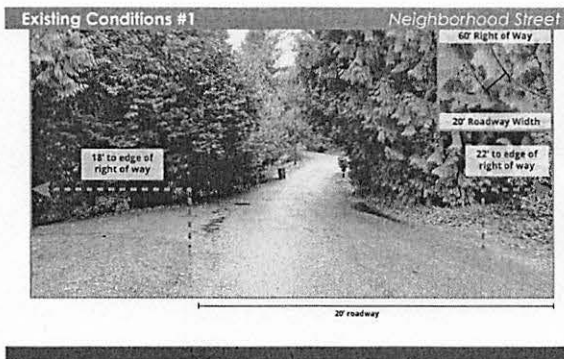


Break Out Group Discussions #1
Holmes Point Drive

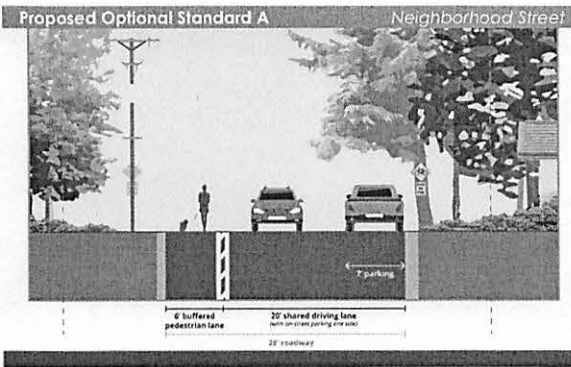
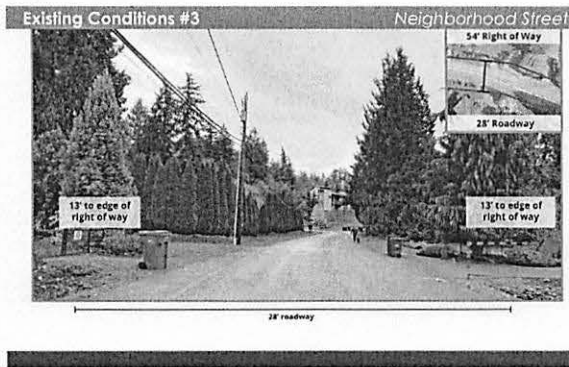
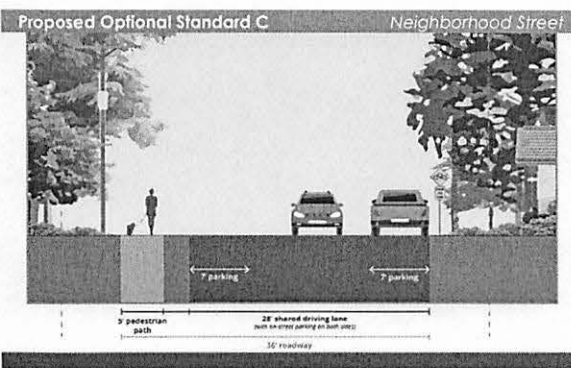
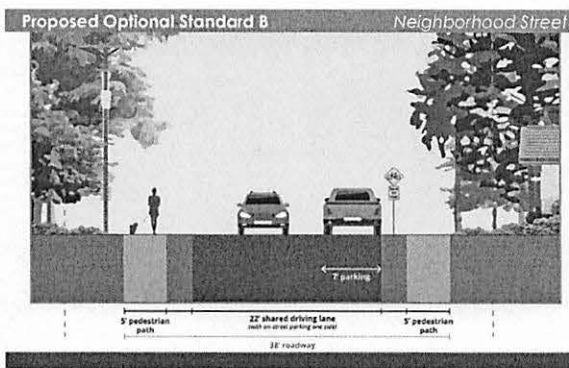


Slide 40

May 20, 2021 - Neighborhood Meeting 2

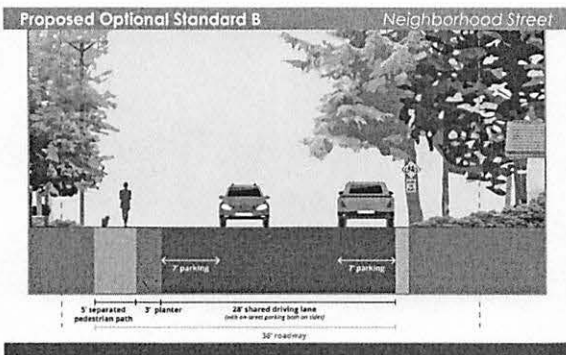


May 20, 2021 - Neighborhood Meeting 2

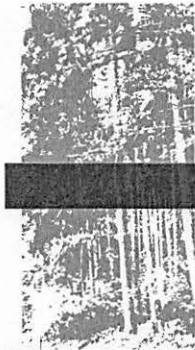


Slide 48

May 20, 2021 - Neighborhood Meeting 2



Break Out Group Discussions #2
Neighborhood Streets



Thank you!

Slide 51

May 20, 2021 - Feedback from Public Meeting 2

May 20, 2021 - Virtual Public Meeting Feedback

During this public meeting, the project team presented the project purpose and key challenges that were heard during the December public meeting. The team presented a potential solution that could be used in Holmes Point Drive to better protect pedestrians, bicyclists, and manage vehicles speeds. The team also presented preliminary street cross section concepts in order to get feedback from the community. During a series of breakout sessions, the neighborhood provided input on the concepts and other key needs and challenges for areas in Holmes Point. The community feedback is summarized below:

Agenda:

1. Project Purpose and Key Challenges
2. December Public Meeting - What We Heard
3. Pedestrian and Bicycle Facilities
4. Holmes Point Drive Cross Section Concept Designs
 - a. Breakout Room Discussion
5. Local Access Neighborhood Street Cross Section Concept Designs
 - a. Breakout Room Discussion
6. Wrap Up and Thank You

Breakout Session 1 - Holmes Point Drive Discussion

Neighborhood Feedback:

Waterfront:

- The concepts put a lot of focus on bikes, but many prefer more pedestrian space.
- Like the option with the separated pedestrian pathway and a shoulder for bikes.
- Because of the width constraints on Holmes Point Drive, we should consider creative solutions like creating a walking path that winds through the woods.
- Not comfortable sharing a lane with bikes, it doesn't feel safe.
- Prefer solutions that have a physical buffer between the street and the pedestrian space, such as a curb.
- It's not safe to have children walking in the street, physical separation is needed.
- For Holmes Point Drive, the minimum standard isn't good enough.
- We need a buffered pedestrian pathway as a minimum, which requires a wider street.

May 20, 2021 - Feedback from Public Meeting 2

Waterfront (continued):

- Enjoy the concept of a pedestrian pathway along the waterfront area
- Pedestrian facilities are more important than bike facilities.
- Taking down a few trees in favor of a dedicated pedestrian pathway would be acceptable.
- Further development along the waterfront seems unlikely how would this be implemented.
- Only people biking the drive are not residents. Residents would not use bike facilities.
- Cross section concepts appear to meet intent of having some sort of separation from vehicles and pedestrians.
- Overhead power, other franchise utilities, and storm drainage should be in considered in design while this work is occurring to complete all needed improvements at the same time.

O.O. Denny Park:

- Worries about the parking still being too chaotic. Formalizing the existing parking layout doesn't seem like it will much calm down the traffic.
- Safety for pedestrians' families is the most important along the park area. Getting out of a car that is parallel parked should be safer.
- The existing parking along the park with the fence works just fine, need to protect the parking from the busy roadway.
- No parking signs are ignored, and vehicles keep parking in dangerous places, need to create a physical barrier to stop cars from parking.
- The image shows a grassy median on the east side of the road that stops approximately 50 feet down, this should be extended.

Entry Areas:

- In downhill or flat sections of the road, bikes and vehicles can share the lane using sharrows. A separated lane is needed for bikes on uphill sections of the road, as shown in the "Entry" cross section.
- It's challenging to make improvements to the waterfront area of Holmes Point Drive, because there are so many features encroaching on the right-of-way and the lots are small.

Speed Issues:

- Expanding the roadway will increase the speed of vehicles. We should increase the existing pavement.
- Instead of expanding the roadway or adding pedestrian infrastructure, we need to treat the speed problem and have people share the existing space. If the speed of the road is slow, then people and bikes can safely share the space.
- Speed programs have already been implemented in Kirkland. Two radar signs have been installed. There is also a "blue sign program" that slows speeds to 25 mph.
- Improvements aren't necessary. Treat speed with traffic calming instead of adding new infrastructure.
- Otak and team needs to see the updated traffic counts. They are currently working with the 2019 data.
- If there is a separated pedestrian space, especially for children and students, it should be a soft surface like gravel or dirt, rather than a hard one.
- Signage is not a solution that has worked in the past.
- Worried about signage only solutions, there should be other physical improvements to reduce speeding.
- Speeds at the ends of Holmes Point Drive are a problem. Traffic calming is needed there, and near O.O. Denny Park.

May 20, 2021 - Feedback from Public Meeting 2

Parking:

- Street parking -- Breakout participants feel it would attract more people to the area, not just in the center section by O.O. Denny Park ("Build it and they will come").
- Safety should be a larger priority than providing more access to existing amenities.
 - Parking adds a factor of safety for visitors that will inevitably come to the park as some level of accommodation. Parking being added away from the park so people can walk to it may prevent congestion (as a compromise).

Pedestrian Facilities:

- More concern about pedestrian access.
- Prefer no middle lane shared cross section.
- Extended pedestrian cross section (10 feet wide) with sharrow on the opposite lane is preferred. A hybrid approach.
- Candle sticks at the north and south ends of Holmes Point Drive might provide some level of protection. A step-up curb might also help. Residents prefer both. Low level lighting also considered.

Breakout Session 2 - Local Access Neighborhood Streets Discussion

Neighborhood Feedback:

Parking:

- We need to understand how much parking is needed vs. existing in the neighborhood streets.
- Has a parking volume study been conducted by the team?
- Parking is needed in some local streets but not others.
 - Near OO Denny Park, some local streets accommodate parking for visitors.
 - 129th hardly ever has parked cars on the street.
 - In the Champagne Point neighborhood, parking is not an issue.
- Open culverts provide challenges for pedestrians and parking.
- Pedestrian areas are the highest priority in these streets.
- People want to walk, but safety challenges prevent them from doing so.
- Is it possible to connect pedestrian routes in local streets?
- Do not need additional parking along the roadway
- See a potential for conflict if existing shared streets are narrowed. Drivers more around corners fast.
- Parking is not a priority. Having an on street buffered pedestrian facility would be ideal.
- Don't see any lack of parking, it's not an issue, NE 129th, focus on walking path that blends with neighborhood character preferably.

May 20, 2021 - Feedback from Public Meeting 2

Pedestrian Facilities:

- Pedestrian facilities are more important.
- Pedestrian infrastructure may not be needed in all streets or areas, only in areas with key safety issues.
- Unprotected walking facilities are a big issue.
- It's ok to have multiple types of pedestrian facilities, as long as they work the best for the specific street.
- Lighting should be installed for pedestrian safety.
- If the roadway is expanded, the priority should be on creating a dedicated pedestrian path, not on creating more room for pedestrians and parking.
- A buffer pedestrian pathway or separated pathway would be the ideal solution.
- A compact gravel path separated by plantings would be ideal.
- Would like to have a planted strip as a buffer but concerned about who would maintain the vegetation.
- Need a flexible approach, there is not a one size fits all options.
- Participants would prefer better pedestrian movement.
- Participants don't see a lack of parking available. They would prefer walking paths with some character reflection Holmes Point neighborhood.
- Standard B cross section is the preferred one with respect to parking.
- Many challenges revolve around needing to move better on foot.
- Bikes can use the road on the residential streets.
- Like separated walking path, separate with green and use gravel for path. Especially, on some of the blind corners.
- Advisory shoulder should be used where there is less space to work with.
- Participants would prefer better pedestrian movement.
- Participants don't see a lack of parking available. They would prefer walking paths with some character reflection Holmes Point neighborhood.
- Standard B cross section is the preferred one with respect to parking.
- Many challenges revolve around needing to move better on foot.
- Bikes can use the road on the residential streets.
- Like separated walking path, separate with green and use gravel for path. Especially, on some of the blind corners.
- Advisory shoulder should be used where there is less space to work with.

May 20, 2021 - Feedback from Public Meeting 2

Speed Challenges:

- Signage is a low-cost improvement, but it doesn't always work.
- 129th – People drive slowly.
- Unsafe drivers will probably drive fast regardless of signs.
- Roadways are already wide and can induce speeding, roadways should not be widened.
- If the roadway is widened, it should only be to create a dedicated pedestrian facility
- There are many sight distance issues along the roadway, cutting back and trimming vegetation is needed to create a safer environment.
- There are certain corners and areas that need safer pedestrian facilities more than others.

Neighborhood Slow Zones

- Participants believe slow zone to be arbitrarily slow and would prefer an increase to 25 mph from 20 mph.
- Slow zone signs would be a message to drivers; participants believe there is a social stigma of shame that would deter higher speeds from people outside of the community.
- Better enforcement may be helpful. Humanizing people and making community will help prevent speeding with people in the neighbors. Visitors may be larger percentage of people speeding. Speed is a bigger problem on Holmes Pt than on residential streets.

Safety Issues

- All sections of streets are not equal.
- Some areas have more safety issues, such as poor sightlines and sharp corners.
- Pedestrian facilities could be implemented in the most dangerous areas.
- Traffic calming is needed for dangerous areas.

Tree Removal

Is it OK to remove trees in order to make pedestrian improvements?

- It's OK to remove trees here and there as long as they are replanted nearby.
- Trees are valuable and need to be considered for removal based on their age, size, species, and neighborhood context.

Lighting

- Lighting can be viewed as an urban feature. Would this fit into the neighborhood?
 - Lighting would be provided for safety rather than illumination.
- Low level lighting could be used.
- Lamp posts would be an ideal lighting option because they would fit the character of the neighborhood

May 20, 2021 - Feedback from Public Meeting 2

In-Meeting Poll

Poll Report							
Holmes Point Street Standards & Corridor Study: Community Meeting 2							
Report Generated: 5/20/2021 19:47							
Meeting ID		Actual Start Time		Actual Duration (minutes)			
988 2085 1154		5/20/2021 17:44		120			
Poll Details							
Participant #	Submitted Date/Time	Do you want to eventually implement speed reduction measures including a slow zone (20 MPH) along Holmes Point drive?	What is your position about retaining trees along neighborhood/local streets?	Do you want to eventually create a continuous pedestrian walkway along Holmes Point Drive, knowing it likely involves tree removals, utility relocations, and right-of-way encroachment relocation?	Do you want to eventually create a continuous buffered bike lane along Holmes Point Drive, knowing it likely involves tree removals, utility relocations, and right-of-way?	What is your position about retaining trees along Holmes Point Drive?	Do you want to eventually create additional parking along Holmes Point Drive, knowing it likely involves tree removals, utility relocations, and right-of-way encroachment relocation?
1	5/20/2021 19:352		1 - Not worried about it	5 - Must have it!	1 - Don't want it!	1 - Not worried about it	1 - Don't want it!
2	5/20/2021 19:361 - Don't want it!		1 - Not worried about it	5 - Must have it!	1 - Don't want it!	1 - Not worried about it	1 - Don't want it!
3	5/20/2021 19:361 - Don't want it!		2	5 - Must have it!	1 - Don't want it!	2	1 - Don't want it!
4	5/20/2021 19:353		1 - Not worried about it	5 - Must have it!	1 - Don't want it!	1 - Not worried about it	1 - Don't want it!
5	5/20/2021 19:355 - Must have it!		5 - My highest priority	1 - Don't want it!	1 - Don't want it!	5 - My highest priority	1 - Don't want it!
6	5/20/2021 19:354		3	5 - Must have it!	4	2	1 - Don't want it!
7	5/20/2021 19:363		3	5 - Must have it!	3	1 - Not worried about it	1 - Don't want it!
8	5/20/2021 19:364		4	5 - Must have it!	3	3	3
9	5/20/2021 19:351 - Don't want it!		4	5 - Must have it!	1 - Don't want it!	1 - Not worried about it	3
10	5/20/2021 19:354		5 - My highest priority	1 - Don't want it!	1 - Don't want it!	5 - My highest priority	1 - Don't want it!
11	5/20/2021 19:354		4	5 - Must have it!	3	3	1 - Don't want it!
12	5/20/2021 19:365 - Must have it!		3	5 - Must have it!	1 - Don't want it!	3	2

May 20, 2021 - Feedback from Public Meeting 2

In-Meeting Poll (continued)

1. Do you want to eventually make a continuous pedestrian walkway along Holmes Point Drive, leaving a 10-foot buffer from sidewalk, utility easements, and right-of-way encroachment situations?	
1. Don't know it	0.0%
2. Yes	100%
3. No	0%
4. Not sure	0%
5. No answer	0%
2. Do you want to eventually make a continuous parking along Holmes Point Drive, leaving a 10-foot buffer from sidewalk, utility easements, and right-of-way encroachment situations?	
1. Don't know it	0.0%
2. Yes	100%
3. No	0%
4. Not sure	0%
5. No answer	0%
3. Do you want to eventually make a continuous buffered bike lane along Holmes Point Drive, leaving a 10-foot buffer from sidewalk, utility easements, and right-of-way?	
1. Don't know it	0.0%
2. Yes	100%
3. No	0%
4. Not sure	0%
5. No answer	0%
4. Do you want to eventually implement speed reduction measures including a 25 mph zone (20 MPH along Holmes Point Drive)?	
1. Don't know it	0.0%
2. Yes	100%
3. No	0%
4. Not sure	0%
5. No answer	0%
5. What is your position about retaining trees along Holmes Point Drive?	
1. Retain all trees	100%
2. Retain some trees	0%
3. Retain no trees	0%
4. Not sure	0%
5. No answer	0%
6. What is your position about retaining trees along neighborhood local streets?	
1. Retain all trees	100%
2. Retain some trees	0%
3. Retain no trees	0%
4. Not sure	0%
5. No answer	0%

May - October, 2021 - Feedback from Survey 2

External Survey Link

Open 5/20/2021 - 9/26/2021

Question	Response	Response	Response	Response	Response	Response	Response	Response	Response	Response	Response		
1	06/10/2021 13:38 PM	Yes they do generally. I favor removing trees in the entry areas to expand the roadway and to work with owners along the waterfront to expand the roadway where possible. Many of the trees along the entry areas are not significant and their loss is not as important as pedestrian and bike safety.	Option C	I'm not in favor of neighborhood slow zone. I do not think the slow zone will have the impact needed to slow down the cars often going 40 mph or more to and from the park. More rigorous traffic calming such as speed bumps and traffic circles are needed.	Not sure	4	3	3	2	3	4	12.1	
2	06/12/2021 9:36 AM	Visually separated facilities as much as possible and physically separated through Denny Park	Shared access.	I would like to see zero, to minimal destruction of trees and surroundings to accommodate improvements. The number one issue, by far, is speed control. Adding signs and further reducing the speed limits will NOT be effective. The attempt to lower the speed limit from 35 to 25 MPH at the entry areas was unnecessary and ineffective. The entry area speeds should be revised back to 35 MPH. The speed limit at the transition and waterfront areas should be 25 MPH, with the park area being 20 MPH. However, there must be physical mechanisms for speed control, such as speed humps or center of road objects to eliminate areas where a vehicle can carry speed or accelerate without an obstacle. I have lived on Holmes Point Drive for 18 years and walk the road frequently. Don't remove any more trees. The developers are already being allowed to butcher the area. Slow the traffic down with physical mechanisms. Create local focused solutions where the road is particularly narrow and unsafe, but don't apply a one solution standard for the entire strip when it is not necessary.	The slow zones using signage will not work. The posted speed limit of 25 MPH is ineffective by itself. Posting signs is a waste of time and money. There must be physical methods to slow down traffic. Speed humps or other obstructions such as center objects that require a vehicle to slow down and alter course. Friendly signs WILL NOT work.	Not needed	5 - Must have it!	4	1 - Don't want it!	5 - Must have it!	1 - Not worried about it!	3	23.7
3	06/13/2021 11:03 AM	Visual separation.	Visual separation.	There needs to be a separated pathway with the amount of foot traffic (strollers, dog walkers, runners, etc.) There just isn't enough room to share the street with multiple people, bikes and cars. Yes, 100% agree with page 17 of the needs of my family.	No	1 - Don't want it!	1 - Don't want it!	1 - Don't want it!	1 - Don't want it!	5 - My highest priority!	5 - My highest priority!	64.6	
4	09/07/2021 18:56 PM	Separated pedestrian pathways	No, being a mother of young children, the minimum standard is not enough. I am most comfortable with a pedestrian path and would absolutely remove trees for my children's safety.	Proposed Option B or C on pages 45 and 46 would meet people, bikes and cars. Yes, 100% agree with page 17 of the needs of my family.	No, I want separated pathways.	5 - Must have it!	3	4	5 - Must have it!	1 - Not worried about it!	1 - Not worried about it!	12.6	
5	09/07/2021 18:26 PM	Visually separated, perhaps bikes and pedestrians can share.	No, making Holmes Point into an urban style through way only leads to later traffic. Roadway expansion should be minimal with sharing.	Proposed standard is best, reducing the speed limit from 25 to 20 is useless if it isn't enforced. I think enforcement of 25mph would slow traffic enough. If there are too many signs, they end up as visual clutter and have no effect.	Just a painted line roadway, has the option of a one-way road enough if repainted been considered? This is a special area with difficult terrain.	2	3	1 - Don't want it!	1 - Don't want it!	5 - My highest priority!	5 - My highest priority!	66.4	
6	09/07/2021 19:35 PM	Proposed optional standard B is my preferred choice given the increasing volume of pedestrian walking traffic and subsequent mounting safety issues, especially during the winter months when it is dark during commutes. Coupled with the high-end design of the park plans should connect with the main thoroughfare of the Holmes Point community. Given that Holmes Point has significant tree coverage, I do not wish to see the pedestrian and bicycle lanes physically separated from the road. The road is shared by a street sweeper frequently. If the ped/bicycle area were inaccessible to the sweeper, those spaces would quickly become cluttered with tree debris and cyclists would not use them.	Yes the cross section is the primary goal, but the minimum standard is not enough. I'm willing to expand the roadway for enhanced pedestrian access, to include tree removals, especially those near powerlines that are falling consistently.	Proposed Optional Standard B is preferred or at a minimum, Proposed Optional Standard A	I'm worried the most about safety as I have a little boy and a growing family walking on the neighborhood streets and along Holmes Point drive is fairly dangerous, especially in the summer and late in winter. My kids safety takes priority over the trees that will need to be removed to make this happen.	5 - Must have it!	2	5 - Must have it!	3	1 - Not worried about it!	1 - Not worried about it!	23.5	
7	09/07/2021 20:25 PM	The streets of Holmes Point need a lower speed limit. There are poor sight lines. The sharp curves need to be approached slowly by drivers, rather than simply making the road wider.	side 48. Parking, driving, walking space all incorporated.	Visually separated facilities	no	3	3	1 - Don't want it!	2	1 - Not worried about it!	1 - Not worried about it!	34.9	
8	09/07/2021 21:17 PM	Buffered pedestrian pathways would fit best. Pedestrians should come first in our neighborhood, as this is the primary for of neighborhood "traffic". Bikes should use shared driving lanes and potentially a paved shoulder. Most bicyclists are not neighborhood residents anyway and tend to need more monitoring and safety awareness. I would not build them a separate path. Prioritize the pedestrians (children, elderly walkers, folks for the majority of Holmes Point cross sections, I would support some tree removal to expand the roadway and provide for pedestrian access.	I don't see the need for much more on street parking. I would support expanding the parking lots at DD Denny on both sides of the roadway and would be fine with tree removal for that reason. But I would not support any tree removal to increase on street parking along Holmes Point. Parking is not an issue for homeowners. As for Denny Park, the existing parking capacity is suitable and matches the land capacity of the park for visitors. No expansion of Denny Park parking is warranted.	Aesthetically and functionally the visually separated pedestrian and biking facilities along the roadway is my preference. The advisory signage proposed would cause more problems for vehicular traffic and would not be preferred. I do not support the proposed neighborhood slow zone option.	Yes. Or better yet, just add sidewalks instead.	4	1 - Don't want it!	2	1 - Don't want it!	4	4	14.7	
9	09/08/2021 10:22 AM	Separated pedestrian pathways. Bicycles can use the road way	Expanding the roadway will involve more than removing trees. Existing driveways and other obstacles will be challenging to widening/	There is only room for a buffered pedestrian lane. The bikes should use the roadway. There is a mistake on slide 15. It is Yarrow Point, not Yarrow Bay.	Only on Holmes Point Drive	5 - Must have it!	1 - Don't want it!	1 - Don't want it!	5 - Must have it!	1 - Not worried about it!	5 - My highest priority!	89.3	
10	09/08/2021 17:02 PM	Shared facilities. I don't think separated pathways will be used by cyclists in most of the area (only uphill) and will change the character of the neighborhood and lead to potential conflict with motorists who don't want faster cyclists in the street.	I don't have an opinion on the neighborhood streets, only the main Holmes Point street as I'm not a resident, only a frequent user of Holmes Point/Denny Park.	I don't have an opinion on the neighborhood streets, only the main Holmes Point street as I'm not a resident, only a frequent user of Holmes Point/Denny Park.	N/A	4	3	1 - Don't want it!	2	1 - Not worried about it!	1 - Not worried about it!	17.7	

May - October, 2021 - Feedback from Survey 2

Feedback ID	Response Summary	What did you think about the idea of a minimum standard for sidewalks?	What do you think about the idea of a minimum standard for sidewalks?	What do you think about the idea of a minimum standard for sidewalks?	What do you think about the idea of a minimum standard for sidewalks?	What do you think about the idea of a minimum standard for sidewalks?	What do you think about the idea of a minimum standard for sidewalks?	What do you think about the idea of a minimum standard for sidewalks?	What do you think about the idea of a minimum standard for sidewalks?	What do you think about the idea of a minimum standard for sidewalks?	
21	08/22/2021 8:52 AM "Proposed Minimum Standard" Separated Pedestrian Pathways CLEARLY MARKED. Bicycles share road with vehicles CLEARLY MARKED. NO BICYCLES INSIDE PEDESTRIAN PATHWAYS.	Sidewalk sections are not numbered, so here's the label for the view that I support: Proposed Minimum Standard	The line does not open. Existing parking on the neighborhood is fine.	Unable open the link and the slides I saw on the previous page are not numbered. The only slide you should be along the waterfront and OO Deney Park. The rest of it means removal of very many trees.	Highest priority must be given to pedestrian safety along waterfront and OO Deney Park with SLOW DOWN of 20 MPH with lots of signs - MANY MANY SIGNS. Bikes should share road with vehicles and absolutely stay off the pedestrian walkways. I hope you called it "Chairman". Trees are inherent to the character of the neighborhood so must be very judiciously selected for removal. Existing utility poles are a hazard and some are an eyesore.	3	1 - Don't want it 1 - Don't want it 3	4	2	49.7	
22	08/22/2021 9:38 AM An advisory shoulder is sufficient. Buffered Pedestrian Pathways. No need for protected bike lanes. YES for Protected PEDESTRIAN LANE. YES for Shared road with CAAs.	I am not against any tree removal, especially given the loss of trees with the development along Lincoln Drive and 122nd/123rd behind the gas station.	Option B. Some parking needs to be added. However, I would have to leave in parking to preserve trees.	I'm not against reducing speed limits to 20 mph. This is not a school zone. Some people will continue to speed.	No, but I don't have strong objections. Thank you for putting this presentation together. It is obvious the result of a lot of hard work.	1 - Don't want it 3	1 - Don't want it 1 - Don't want it 4	4	5	My highest priority!	41.6
23	08/22/2021 11:44 AM YES for Protected PEDESTRIAN LANE. YES for Shared road with CAAs YES Proposed Optional Standard B	YES for Removals for Pedestrian SAFETY. Need more sidewalks and extending Pedestrian Access. Kids and People will get hurt by drivers.	You're not based on your hands if you don't change it ASAP.	25 MPH is OK. NO Need to go to 20 MPH. NEED to increase access to talking streets. YES Bicycles share the roadway with vehicles YES Example of a Pedestrian Path Separated by a Planting Buffer Yellow Bay, WA Slide 34	Yes shoulders are needed for safety Widen streets for dedicated Pedestrian Path.	5 - Must have it 3	1 - Don't want it 1 - Don't want it 2	2	2	37.3	
24	08/22/2021 12:20 PM The road should NOT be widened. It should be designed as shared use at most.	The road should NOT be widened. This will remove very valuable trees and much-needed pervious surface. The pervious surface and the trees soak up rain water coming down the hill from Holmes Point and also keeps the soil from eroding on the waterfront properties. For your particular property, without these trees we WILL HAVE A HUGE RUSH OF WATER ONTO OUR PROPERTY AND LIKELY FLOODING. If the road is widened in front of my house it will take out a hedge with 25 trees that provides much-needed water absorption/retention and much-needed privacy and protection from the road car traffic. There is already ample room to walk and bike in front of our home. The City states that the goal of the project is to preserve the character of the neighborhood. That is not achieved by making it a game park designed through. It has historically been a quiet tree-lined street with row homes. The real problem is too many people accessing the park from about behind the number of people that can go down to the park by keeping parking limited to a safe number, enforcing parking and keeping people from pitching tents and staying all day and night? The park is a four-day use park and not a state fair ground.	Tree removal should not be conducted. The area is prone to flooding. We need the trees. They protect the stormwater, retain the soil and soak up rain/water runoff. The character separated are NOT achievable. This is not the character of the area. It would remove valuable trees and pervious surface. We do not want a huge road in Holmes Point.	No	Please please please do not take our trees. They take hundreds of years to grow, are irreplaceably important to our area.	1 - Don't want it 1 - Don't want it 1 - Don't want it 4	5 - My highest priority!	5 - My highest priority!	32.9		
25	08/22/2021 12:31 PM Shared facilities at most. The Holmes Point road should NOT be widened.	NO tree removal should be conducted. The trees are very important to the area. The minimum cross section is acceptable. Removal of trees and widening of the road will severely impact waterfront homes that receive the bulk of rain water coming down the hill. These trees and vegetated portions of land help with retention, water absorption and protect the homes along the waterfront from car traffic by providing a privacy buffer.	Tree retention and not widening the road are the most important considerations. This is a natural area not an urban park. Please keep our trees!	1 - Don't want it 1 - Don't want it 1 - Don't want it 4	5 - My highest priority!	5 - My highest priority!	7.4				
26	08/22/2021 12:31 PM Vocally separate facilities	Yes, comfortable with the minimum standards	It's a neighborhood park not a state park. Land acquisition and make sure row homes have adequate parking	1 - Don't want it 1 - Don't want it 1 - Don't want it 3	5 - My highest priority!	5 - My highest priority!	5.6				
27	08/22/2021 12:54 PM Shared	Minimum works we shouldn't remove any trees. We already have issues with drainage	No. People need to know the laws and drive accordingly. No increased signage & pavement markings or raised pavement markings, particularly on the flat waterfront section where many pedestrians are out. Improved visibility of stop signs at 230N - I have had many months and observed numerous people running that way again.	1 - Don't want it 1 - Don't want it 1 - Don't want it 3	5 - My highest priority!	5 - My highest priority!	5.4				
28	08/22/2021 12:56 PM Bike: narrow on flat & climbing bike lanes on hills A wider pedestrian footpath on waterfront. No bicycle facilities as they already exceed the 25 MPH on the existing roadway. Would like to see signage to emphasize speed limits for bicyclists.	Yes, minimum standard cross sections OK. Minimize road widening while increasing pedestrian safety.	Additional parking only brings more traffic. Adequate parking is a.	1 - Don't want it 1 - Don't want it 1 - Don't want it 3	4	4	5.1				
29	08/22/2021 1:00 PM Am willing to expand roadway for enhanced pedestrian access	Am willing to expand roadway for enhanced pedestrian access	DO NOT want slow zone to include access. I would like to see it corrected back to 35MPH and reduce speed to 20 MPH for transit, Deney park and waterfront.	No	1 - Don't want it 1 - Don't want it 1 - Don't want it 3	4	2	34.5			
30	08/22/2021 1:15:13 PM Buffered pedestrian pathways with shared roadway for both vehicles and bicycles. The pedestrian pathways likely need to run mostly along the eastern base of the street (basically you'll never have room for both a buffered pedestrian and separate buffered bicycle lane - there is just too much existing encroachment into the right of way on the waterfront side, and unless I land all homes are demolished and redeveloped. No but see my notes above - this should be like slide 23 BUT the bike lane should be the standard for bike lanes (really wide), pedestrian lane, and it doesn't need to be "protected" with those ugly concrete blocks or other account alternatives we end up with a much better. A separate continuous shoulder that is 4' wide would make a great deal of difference vs. what we have now. Bikes shouldn't get priority for bike lanes, so there is MUCH more pedestrian traffic.	For the dead end / cul-de-sac streets, the proposed minimum standard is adequate (most of these streets are fine as is and have enough parking and are generally safe for pedestrians due to their nature of being dead end streets). For the streets that "lead" the "main streets, the proposed "A" which allows for some parking on one side, and pedestrians on the other is more than adequate (this is essentially what we have already on a large portion of NE 122nd, where there is an existing bike lane on the south side).	Painted markings and signs are adequate. "less is more" I don't think this is a good idea. I think the main concern is the width of the roadway. It's a shared roadway which it does not mean on the larger side of the road to be some level of encroachment into the right of way that needs to be rediculously accented. For the neighborhood streets, I really have minimal concerns (and I live on one) once there isn't as much traffic, I don't see a real problem that needs to be fixed (being them as it would actually be my preference).	1 - Don't want it 1 - Don't want it 1 - Don't want it 3	3	4	30.3				

May – October, 2021 – Feedback from Survey 2

Response Number	Response Date	Response Text	Response Summary	Response Category	Response Status	Response Score	Response Rating	Response Priority	Response Action					
31	09/22/2021 15:35 PM	I don't know that you can have one cross section for all the areas of HPD. That said the cross section that best matches the character of HPD is the Waterfront area, Proposed Optional Standard B. Entry Way area: Proposed Minimum Standard was the only one presented. I would love to see this at both the entry areas. northern entry area has many more blind spots and a buffered pedestrian bike lane heading up the hill would be my preference there. Waterfront area: Proposed Optional Standard B is my preference. We need to keep street parking on HPD. Denny Park area: the most open for this area are excellent.	The cross section for the waterfront area (Optional Standard B) does fit my goal for HPD and access to all users. I would welcome expanding the roadway for enhanced pedestrian access, particularly to the entry areas. Removing some trees from these areas would reduce the chances of falling limbs and branches that present. The knockout power lines, especially in the northern entry area, still have vehicles parking wide into the pedestrian path. There are many blind spots, especially in the transition area from the north entry, where vehicles may cut the corner and enter the buffered pedestrian path, unless there are other barriers in those areas.	Proposed Optional Standard A	Yes	4	2	4	1 - Don't want it!	2	4	112.1		
32	09/22/2021 16:31 PM	Side 37 also seems in line with the way it seems the area is used.	Side 30, Proposed Optional Standard B, looks nice. We have mostly pedestrian traffic so having the larger pedestrian path would be ideal.	Side 42 is acceptable but 45 is better	I like Separated Pathways as I worry about my children and the speeding cars. I think a slow zone is a good idea.	Safe areas for children to walk is important	4	1 - Don't want it!	2	5 - Must have it!	2	22.5		
33	09/22/2021 16:47 PM	both buffered bicycle facilities and separated pedestrian pathways, even expanded driveways	It is too narrow. Proposed Optional Standard B is the best. I am willing to expand the roadway for enhanced pedestrian access which likely includes tree removals	Proposed Optional Standard A	no	Separate path for pedestrians that are buffered from vehicles by planting strip I prefer separated paths/sidewalk and curb.	Expansion of Holmes Point Dr with dedicated pedestrian path and bike lane is very important from civilian safety perspective, otherwise they all mixed with vehicles which is dangerous especially during the night time.	5 - Must have it!	5 - Must have it!	5 - Must have it!	4	1 - Not worried about it! 5 - My highest priority!	27.6	
34	09/22/2021 17:19 PM	Separate pedestrian and bicycle paths	The current cross sections are not adequate. I am willing to expand the roadway to accommodate larger paths.	Physically separated facilities	Yes	I think the proposed minimum standard would be adequate and least disruptive. Although it would be very nice to have sidewalks on the neighborhood streets. I do not think having parking on both sides of the street is necessary, but one side parking and one side sidewalk would be a big improvement!	I don't think that's necessary or would change how people drive, but sidewalks would be nice.	5 - Must have it!	1 - Don't want it!	3	3	23.5		
35	09/22/2021 17:35 PM	Proposed optional standard B. There are so many neighbors walking on Holmes point drive as the time in the street. A buffered path/sidewalk would be so much safer.	Yes, the minimum standard is an improvement over what's there. Having a planting strip between the walking path and road would be lovely though. I rarely see people biking but there are TONS of walkers.	don't remove any trees unless absolutely necessary. Extend the parking/overflow parking at the park to ensure people don't park by the side of the road.	no	25mph is a slow zone. 20mph would be ridiculous. Actually enforce the speed limit and place some spikes near high foot traffic / pedestrian crossings.	enforce the rules that are there - including speed limits, people that speed are going to speed no matter what the limit.	1 - Don't want it!	5 - Must have it!	1 - Don't want it!	1 - Don't want it!	4	4	6.4
36	09/22/2021 18:54 PM	I don't see the need to change anything. Repair the road and after that enforce the 25mph speed limit.	No, we should not expand the road/remove trees. We should repair the road to be of good quality, that's it	25mph is a slow zone. 20mph would be ridiculous. Actually enforce the speed limit and place some spikes near high foot traffic / pedestrian crossings.	no	I do not think we need a 20 mph on the entry zone - live on the edge if this means the side streets. I do not think please consider keeping 25 mph and ENFORCING it! Thank you	enforce the rules that are there - including speed limits, people that speed are going to speed no matter what the limit.	1 - Don't want it!	5 - Must have it!	1 - Don't want it!	1 - Don't want it!	4	4	6.4
37	09/22/2021 19:37 PM	First priority: separated pedestrian pathways and speed enforcement as a frequent and frightened walker on HPD. Speed bumps and law enforcement please! I also feel for the bicyclists, buffered bike lanes would be great, too.	Yes, I support widening the road and tree removal! They just fall down and take out the power frequently anyway. There are plenty of trees—we won't miss a few	I do not see a need for more parking Diagnose with any tree removal along Neighbourhood street. The traffic along Neighbourhood street is low given the zoning density of Holmes Points. The street I live in has most plots already maximize the density yet our street is quiet. The big trees (mostly within private lot) create the unique characteristic of Holmes points. All the options besides the "proposed minimum standard" will KILL the charm and beauty of the current neighborhood streets. As an urban designer in many instances and an architect I strongly against all other options for Neighbourhood street; for pedestrian improvement the "proposed minimum standard" is absolutely enough.	no	I do not think we need a 20 mph on the entry zone - live on the edge if this means the side streets. I do not think please consider keeping 25 mph and ENFORCING it! Thank you	enforce the rules that are there - including speed limits, people that speed are going to speed no matter what the limit.	1 - Don't want it!	5 - Must have it!	1 - Don't want it!	1 - Not worried about it!	2	15.5	
38	09/22/2021 20:32 PM	Physically separated facilities	Okay with the minimum standard	Slide 14 - on-street buffered facilities for the local streets. Slow zone would be a good idea for Holmes Point Drive and local streets. Although, it already does take down hitting and applying brakes to stay at the current 25 mph when going down the hill, but it is definitely the safe thing to do given the curvy nature of the road and its blind spots.	Slide 14 - on-street buffered facilities for the local streets. Slow zone would be a good idea for Holmes Point Drive and local streets. Although, it already does take down hitting and applying brakes to stay at the current 25 mph when going down the hill, but it is definitely the safe thing to do given the curvy nature of the road and its blind spots.	Sorry, what exactly is defined shoulders? I searched the slides for delineated and nothing was found.	Trees and nature is the highest character and priority of Holmes Points. We do not need more parking spaces!	5 - Must have it!	1 - Don't want it!	5 - Must have it!	3	5 - My highest priority!	29.8	
39	09/22/2021 21:18 PM	Proposed Minimum Standard - 4' paved shoulder, 5' buffered pedestrian path or really just another 4' paved shoulder - all we need is a consistent shoulder along Holmes Point Drive. Holmes Point needs to be safe for pedestrian and absolutely not a shared lane with bikers. I feel this is proposed by people not living at Holmes Point and not in respect to the residence who is walking and driving here daily. Bikers can share with cars as they drive as fast if not faster than cars. I walk Holmes Point daily and 25 mph is a good speed if people follow the limit. I can agree with 20 mph from 72nd St to the 151st. Please I wouldn't mind a cement separation for walkers and also an improved pavement on the road especially from the south entrance.	Yes, comfortable with minimum standard cross-section.	Proposed minimum standard or optional standard A	4	1 - Don't want it!	1 - Don't want it!	5 - Must have it!	4	5 - My highest priority!	112.2			
40	09/23/2021 7:56 AM		A separate lane for pedestrians which is the majority of people who lives here. Bikers can share with cars.		7	There is no need for 20 mph all the way from Ijuvanti Drive but part of it I can agree on.	Yes	1 - Don't want it!	1 - Don't want it!	3	3	118.1		

May - October, 2021 - Feedback from Survey 2

Feedback ID	Issue	Response	Comments	Priority	Score	
41	08/23/2021 11:52 AM	I like the proposed minimum standard that uses the existing 20' in the entry sections exchanging the 4' and 2' shoulders into a 5' protected bike lane, because it provides a safer riding while not widening the road or impacting the trees.	I feel like the proposed standard widens the road too much, and the existing 20' could be expanded to 26' feet to get the 5' protected bike lane without having as much impact on the trees and homes.	I don't see any need for changes to the neighborhood streets.	5 - My highest priority	21.5
42	08/23/2021 11:58 AM	Adding plus buffered bicycle	Yes	Optional standard B	5 - My highest priority	19.3
43	08/24/2021 8:55 AM	None of the minimum standards that are proposed in the study meet our view of the potential character of Holmes Point Drive. These plans all dilute the low-volume, low-speed character of the road and its surrounding area. It is important to maintain the character of the road and its surrounding area. It is important to maintain the character of the road and its surrounding area. It is important to maintain the character of the road and its surrounding area.	Not neither one of these standards reflect goals that this community would set here. We would never be willing to expand the roadway to accommodate more bike traffic. It is important to maintain the character of the road and its surrounding area. It is important to maintain the character of the road and its surrounding area. It is important to maintain the character of the road and its surrounding area.	We have walked and closely navigated every connected street off of Holmes Point Drive over our 35 years of residency. There is only one street which has critical parking trouble today which is 60th Street adjacent to the park. It's not a resident road, the street is empty and the park is empty. It is being caused primarily as an overflow for park visitors. Park overflowers disregard the signage and road markings. Park overflowers disregard the signage and road markings. Park overflowers disregard the signage and road markings.	5 - My highest priority	\$253.9
44	08/24/2021 11:54 PM	SHARED FACILITIES	I am not in favor of expanded roadway and am not willing to expand the roadway.	I want to keep the existing conditions	5 - My highest priority	18.5
45	08/24/2021 11:17 PM	physically separated facilities would be my preference.	Not sure, I want to maintain the current favor of the minimum and keep it neutral, but of course safety is important.	I am not in favor of expanded roadway and am not willing to expand the roadway.	5 - My highest priority	12.8
46	08/24/2021 11:53 PM	important.	I am not in favor of expanded roadway and am not willing to expand the roadway.	I am not in favor of expanded roadway and am not willing to expand the roadway.	5 - My highest priority	11.8
47	08/24/2021 14:04 PM	Homes Point Drive on Holmes Point Drive	I don't see any need for changes to the neighborhood streets.	Optional Standard A	5 - My highest priority	10.2
48	08/24/2021 14:36 PM	Proposed minimum Standard go down Holmes Point Drive (Slide 21)	Yes, we need safe walkways down along the waterfront.	Proposed Optional Standard A (Slide 44). Our neighborhood streets aren't that busy so don't need wide sidewalks. I really like the Advisory Shoulder (Slide 10) for the neighborhoods.	5 - My highest priority	73.4
49	08/24/2021 10:23 PM	essentially be impossible without cutting into the woods	Not in favor of any of these new changes. None of the changes since our occupation have benefited or improved the neighborhood by any noticeable metric, and particularly adding sidewalks has made it worse.	I do not think we need to reduce the speed. Speed is not really a problem if we can add additional walkways for pedestrians.	5 - My highest priority	9.1
50	08/24/2021 17:53 PM	Proposed cut on B	I would prefer wider to improve parking and would be willing to remove trees. I would prefer wider to improve parking and would be willing to remove trees.	Proposed option C (21 feet of roadway) not enough for traffic to flow through without cars being blocked.	5 - My highest priority	15.7

The neighborhood streets do not have speed or safety issues. Please do not widen them. Our neighborhood is part of the Holmes Point Overlay, but not Holmes Point Drive. You access it via NE 138th Pl off of Jacobs Drive, and the main road in the neighborhood is 72nd Ave NE. Some of our smaller streets were captured in some of the "neighborhood" pictures in the PFI. We could definitely benefit from better pedestrian facilities along 72nd Ave NE, particularly considering that kids walk that need to get to the only Post-Pre middle school bus stop for the neighborhood at the corner of 72nd Ave NE and NE 127th St. Beyond that one main street, the other streets in our neighborhood need no improving. Traffic is minimal, speeding isn't a problem, there is ample parking, and there are many mature trees (some quite majestic) which would have to be removed if the streets were widened. Kids ride their bikes, neighbors go for frequent walks, there are no passing shortages, and it has never felt unsafe. Modifying these small streets would be strongly opposed by the neighborhood, and would cost money much better spent elsewhere in the city to address real problems. Thank you!

Please no expanded cutting, no landscaped operations to maintain are needed. The number one safety concern for crossing, pedestrians, accidents and deaths is the Denry Park Zone itself. Nine months a year the park is closed and passed through by the walking community, residents, wildlife and their families, parking is always adequate. It is certainly one of the plus features of HFD that makes it so unique and in demand. For a three month period in summer at mid 70 degree days the park gets crowded and the Denry Park Zone is a problem. Downings occur each year when with warning signs, opening car running stop signs and raising HFD is initiated and an early rain coming with vectors loaded with gear drying traffic. Some "slow down signs" are unlikely to have much effect on this road. Solar solar road sign already are in place in HFD and it's a good reminder but don't waste help the safety problem as visitors reach the fat intersections and pick up vehicles. We are generally surprised that you didn't ask feedback regarding the Denry park issues that I've outlined to address. It's a big deal to residents here and we are all concerned that an before plan could make the problem worse. As increasing community planning volume alone might crowd the facility, create more auto traffic, and lead to a more dire set of solutions. It's really a small park facility, we should respect it and it's a character issue for the neighborhood on 229th and 130th Pl.

The HFD community needs added city focus on aging infrastructure like improved storm drains in key sections to counteract greater development impact, bank erosion fixes in both N/S transitions, plans to reduce power outages and related tree management before this project is finalized. As a daily walker on Holmes Pl Dr I see why it would be nice to have more safe space. The problems with accomplishing this are huge and personally we would lose our support. The potential cost of this project is too huge and too harmful for property owners.

I really like the Advisory Shoulder (Slide 10) for the neighborhoods. I've driven, walked, and ridden bikes on these types of roads and they are neighborhood friendly. I would like to see signage that pedestrians walk toward to encourage traffic (walk on left side of road). Hand signals for people to go 25 mph left would be really hard but I like it!

I'd rather have delineated shoulders rather than sidewalks on the narrow neighborhood streets. Holmes Point along the waterfront could use more delineation than the side streets.

This is a waste of tax money. I do not think we need to reduce the speed. Speed is not really a problem if we can add additional walkways for pedestrians.

There needs to be more consideration for areas that are restricted to one road in and out. They are considered "neighborhood streets" or "local streets" but are effectively connectors. These should be wider than the minimum.

May - October, 2021 - Feedback from Survey 2

Feedback ID	Feedback Text	Response	Priority	Category	Score				
51	09/24/2021 19:00 PM Pedestrian Lane/Bike Lane or Buffered Pedestrian/Bike Lane on shoulder Physically separated facilities if it could be done NOT using curbs/sidewalk and NOT widening existing vehicle lanes. If this is done it's very important to do with speed management devices. In the corridor design the speed must be treated. HOV would be safe if cars drive 25 in the 25 zone. Treat the Speed. Also, in the design phase remember it is proven wider pavement increases vehicle speed.	My primary goal is safe walking paths along the road, buffered bike lanes that are safe for all skill levels (kids through adults). Prefer proposed Optional Standard A. If speed is treated. The over arching goal of this project is to improve safety along the corridor. If people drove the speed limit, safety would not be a problem. I am comfortable with min std cross section if the pavement is not widened. The wider the pavement the more comfortable motorists feel, the more comfortable a motorist feels increase the speed in which they drive	4	2	3	5 - Must have it	3	4	12.8
52	09/25/2021 10:55 AM I'd like to see Holmes Point Drive be safe enough for elementary aged children to take a bicycle or walk to the park or to school. This requires buffering and absolutely no "sharrows" at any point.	I am not as comfortable with these. The presence of sharrows is not inclusive of families who want to ride bicycles safely with children and there is not practical pedestrian space for someone in a wheelchair, someone pushing a stroller, etc. We have pathways in the area which are safe for these purposes so why not here? Expanding the roadway or providing an alternate entirely pedestrian and bikeway separate from the road would be preferable. A consistent bike and walking path would be desirable, as would speed management. However, that desire must be realistic from a price and deployment perspective. Perhaps a 4' foot, paved combined bike and pedestrian path is possible, but no more. And even that would likely cause danger for houses whose front doors will open directly on to the path.	Most of these seem practical. Where there is a room I think it's better to have a sidewalk. To me the best case is "can a 2nd grader walk to the bus stop alone before the sun comes up" and I don't think that is the case in some places due to the fact that there would be a visible delineation of pedestrian space versus the current unstriped neighborhood roads but I am ambivalent about the speed I'm not concerned with this as long as there is pedestrian space.	1 - Don't want it	1 - Don't want it	1 - Don't want it	5 - My highest priority!	5 - My highest priority!	27.3
53	09/25/2021 11:34 AM Unfortunately, none of the proposed sections are realistic for most sections of Holmes Pt. It is unrealistic to believe that various house front steps will open directly on to a bike path, or that the structures, hedges, electrical poles could be removed along the entire street.	No. I don't feel a physically separated bike lane is feasible (slide 23). As I stated above, most bike riders use the street since they are riding very fast due to hills easily traveling 25-30 mph. They would probably not use the physically separated bike lane - biking at that speed is incompatible with pedestrian use. A sharrows concept would be better to alert motorists they are sharing with cyclists, or visually separated as in slide 27. We do not support expanding the roadway.	I like the slow zone. I prefer the sharrows or the visually separated concept for the neighborhood streets. Regarding speed, the problem is not the posted speed, but speed enforcement. Motorists are not adhering to the existing 20 mph limit so they won't adhere to a 20 mph limit. I believe the electronic warning sign has been effective. I do not support the concept of speed bumps. They also are not conducive to the bicycle traffic we get, since the cyclists are riding at a fast pace. Sharrows concept should work fine.	1 - Don't want it	2	5 - Must have it	4	3	15.3
54	09/25/2021 12:13 PM Proposed minimum 5' bike/pedestrian lane with 20' driving lanes	Slide 23 is preferred without any tree removal	2	1 - Don't want it	1 - Don't want it	1 - Don't want it	4	4	13.4
55	09/25/2021 11:45 PM Nothing additional needed along OODenry except some traffic calming devices-like speed bumps. Cars fly through the park. It would be nice if Holmes Point Drive had a consistent shoulder to walk on. Bicycles are fine on the road, except a climbing lane would be nice in the steeper areas.	Most neighborhood streets are just fine. We walk often- usually like a day-and rarely encounter any cars on the neighborhood streets.	Most local streets are okay as is. An exception would be 2nd Ave NE along Finn Hill Park. A gravel walkway along the edge of the park would help create a safer walking area along the busier part of the road.	1 - Don't want it	1 - Don't want it	1 - Don't want it	5 - My highest priority!	5 - My highest priority!	44.2
56	09/25/2021 14:49 PM Proposes standard A is better. Proposed standard B is expensive and is an over-engineered solution in terms of safety.	Comfortable with the minimum standard cross-section. The current cross section is fine. No need to expand or adjust anything. No expansion for pedestrian access is required. Maintaining the tree canopy should be the priority not adding pedestrian access. People have and can continue to move around the area just fine.	No. Keep it rural. Do not like the concrete barrier. Maybe it's just me, but this whole survey was difficult to navigate. Our thought is keep it rural feeling. Slow traffic by OODenry.	1 - Don't want it	1 - Don't want it	1 - Don't want it	5 - My highest priority!	5 - My highest priority!	37.3
57	09/25/2021 20:40 PM Character should be to leave everything as is. No improvements are required	I don't think parking is a significant issue for neighborhood streets. The proposed minimum standard is fine. The current cross section works just fine. Nothing additional is needed. People are able to move about and additional bike lane or pedestrian walk ways are not needed. No tree removal should be considered. Probably the most minimally invasive - e.g. the one with a pedestrian path, but not much else. With that said, I'm happy with all options. I think any pedestrian path would be an improvement.	I am not sure what this means. But advisory lanes may be useful on portions of neighborhood streets where lines of sight are blocked by slopes or curves. The current configuration is fine and nothing more needs to be modified. Of all the areas in the city that need modification, this is one of the lowest. Just leave the area as is.	3	1 - Don't want it	1 - Don't want it	5 - Must have it	3	60.6
58	09/26/2021 6:23 AM All work for me, although I'm fondest of the pedestrian pathway.	I am happy with enhanced pedestrian access and some tree removals, if necessary to facilitate a pedestrian path.	I would be happy with a neighborhood slow zone. The local streets are used heavily by hiker/bikers and probably should have a lower speed limit.	1 - Don't want it	1 - Don't want it	1 - Don't want it	5 - My highest priority!	5 - My highest priority!	43.0
59	09/26/2021 7:58 AM I'd like to see Holmes Point Drive be safe enough for elementary aged children to take a bicycle or walk to the park or to school. This requires buffering and absolutely no "sharrows" at any point.	I'd like to see Holmes Point Drive be safe enough for elementary aged children to take a bicycle or walk to the park or to school. This requires buffering and absolutely no "sharrows" at any point.	I would be happy with a neighborhood slow zone. The local streets are used heavily by hiker/bikers and probably should have a lower speed limit.	5 - Must have it	1 - Don't want it	2	3	4	16.1
60	09/26/2021 7:58 AM I'd like to see Holmes Point Drive be safe enough for elementary aged children to take a bicycle or walk to the park or to school. This requires buffering and absolutely no "sharrows" at any point.	I'd like to see Holmes Point Drive be safe enough for elementary aged children to take a bicycle or walk to the park or to school. This requires buffering and absolutely no "sharrows" at any point.	I would be happy with a neighborhood slow zone. The local streets are used heavily by hiker/bikers and probably should have a lower speed limit.	5 - Must have it	1 - Don't want it	2	3	4	22.3

May - October, 2021 - Feedback from Survey 2

Item	Comments	Response	Response	Response	Response	Response	Response	Response	Response	Response	Response	Response	Response	Response	Response	Response	Response	Response		
61	06/24/2021 11:03 PM Physically Separated Facilities. There are many blind curves on Holmes Point. Yes.	No more additional parking spaces needed.	Yes.	Yes.	Development of condensed housing needs to be slowed in Holmes Point. I would like to see a few speed bumps added.	5 - Must have it	5 - Don't want it	5 - Must have it	5 - Don't want it	5 - My highest priority	5 - My highest priority									
62	06/24/2021 13:44 PM The proposed minimum solution for the waterfront section is closest to the best solution. All that is really needed is a continuous walking path along the waterfront section do to heavy use by the neighborhood residents.	While the park area may need additional parking, additional parking is not needed in the waterfront area or transition areas. In addition, a separated bike path is also not needed as bike traffic appears to share this roadway easily and safely with cars. The waterfront area is also extremely limited by space, particularly on the waterfront side. These lots are already like restricted and any further impingement on those properties would result in significant alterations to structures and would result in the roadway surface and travel being too close to the building structures. Any widening of the roadway in the waterfront area should be limited and be performed on the east side of the roadway where the structures are much further set back.	The minimum standard would be the best option except that separating the path from the roadway when possible would be preferred.	I like the bicycle share in the traffic lane and the advisory pedestrian shoulder. Speed is an issue on Holmes Point Drive. I think that the use of speed tables could work. I know that restrict my speed to under 25 when there are in use. Some drivers use the straight section of Holmes Point Drive as a drag strip.	No	Please bury the utilities as part of any significant project.	3 - Don't want it	1 - Don't want it	3	4	4	4								
63	06/24/2021 14:22 PM The road and shoulder show not be expanded except in very limited areas where there is currently zero shoulder.	No. The road and shoulder current width in 95% is fine. What we need are lower speed limits (20mph on Holmes Point and 30 MPH in Park), speed limits being enforced, parking restrictions enforced. We do not want Holmes Point Turned in a major thru street. There has already been fatalities let's not encourage more.	Remember the community voted against Annexation, we don't want to do another Kirkland.	A slow zone is a fantastic idea, let's do it tomorrow, but leave the roads 99% as they are. In areas where there is zero shoulder one should be created.			1 - Don't want it	1 - Don't want it	1 - Don't want it	5 - Must have it	5 - My highest priority	5 - My highest priority								
64	06/24/2021 14:44 PM Separated pedestrian pathways and buffered bike lanes. Additionally, can we add pedestrian ramps or large speed bumps to eliminate all the speeding?	I am not comfortable and would be more comfortable if we made Holmes point drive a one way road with pedestrian ramps and make the other lane a bike/pedestrian path.	Property owners should be responsible for parking on their property and access should be encouraged on facades of house lots, encouraging people to build responsibly and not push things like parking off to their neighborhood.	I don't think it will work like you want it. That's why I recommended pedestrian ramps.	I'd rather have sidewalks, but if this is the only way we can get them done.	I think there is a middle ground if you get creative. These are all make the low stakes. Please respect our wishes, but look for ways to better care for the community around. We have been fighting for our trees but every development takes them out like they don't matter.	5 - Must have it	3	5 - Must have it	3 - Must have it	5 - My highest priority	5 - My highest priority								
65	06/24/2021 15:18 PM I would prefer separated pedestrian pathways, but would prefer that they be separated by physical barriers (curbs, sidewalks, etc.). The bicycles that ride down Holmes PT DR are largely competitive bikers in large groups that ride on the road anyway, so I wouldn't want a dedicated pathway for bikes - the pathway is needed to keep pedestrians safe. There are lots of older residents, and also a recent influx of young families, so having a safe walking path is the most important in my view. Even just improving the sidewalks of the road shoulders and re-striping would be a huge improvement.	As a resident of Holmes Point Drive, I'm not willing to expand the roadway for enhanced access. The trees provide substantial character which would be lost upon removal. Additionally, there isn't room for expansion in many parts of the roadway without significant property destruction or significant engineering challenges (billboards, ditches, power poles, etc.). You would put residents at risk with cars and bicycles right up against existing driveways and parking spots. I'm in favor of the minimum standards for the fact that they make use of the existing road use (both minimum expansion), but provide some additional safety for pedestrians. I would prefer to keep bikes on the road itself, and keep the shoulder areas for pedestrians only. I would also prefer no physical barriers (concrete curbs or plastic dividers) on the roadway.	The minimum standard is sufficient. Nearly all homes have driveways and sufficient parking, we don't need to cut down beautiful mature trees to create parking lots (and for who to use?). I think having a marked advisory shoulder, and improving some of the shoulder paving, without removing trees, we've a nice add on to the existing streets - these don't need a major overhaul.	For the buffered facilities, I do like having a marked area for walkers and bikers (all areas currently unmarked). However, I would NOT recommend any physical buffers or barriers. Certain roads (such as 8th Ave NE), there is barely enough room for cars to pass each other on the street, so any physical barriers (curbs or separated pathways) would make the road very difficult to use.	In favor of 'pedestrian only' no bike' pathways encroaching off the existing road as little as possible. You can definitely shift the road a bit, shove the road width and clearly mark off a pedestrian only zone, which would be a wonderful addition, while maintaining the mature trees and avoiding major impacts to utilities and existing homes.	1 - Don't want it	1 - Don't want it	1 - Don't want it	5 - Must have it	5 - My highest priority	4	5.0								
66	06/24/2021 15:49 PM Bicycle chevrons and foliage cleared from existing roadway.	I DO NOT believe the HPD roadway needs to be expanded. My goal for HPD would be to make the existing roadway safe - clear the foliage that encroaches on the side of the road and mark with bike chevrons.	I do not believe we need additional parking in a residential parking park - we need a neighborhood park - not a City Park.	NO	Creating a neighborhood slow zone would be awesome. We have too many from outside our neighborhood that do not respect that HPD is a neighborhood and O'Donoghue Park is a neighborhood park - not a City Park.	Spend city time and money in enforcing the current rules and regulations. Also clear encroaching foliage so that we may walk without having to walk into the roadway.	1 - Don't want it	1 - Don't want it	1 - Don't want it	5 - Must have it	4	4								
67	06/24/2021 16:10 PM Proposed optional standard B with separated pedestrian pathways	I am fine with the proposal suggested in the slides 23, 27.	Proposed Optional Standard A	Thanks for making the questionnaire.		4	2	4	4	4	4									
68	06/24/2021 16:31 PM CDA sections A seem enough to me, considering how many barriers there are to that path. So many cyclists, trees and landscaping in the way. What else of the street will take the space? I don't think separated walk ways of bike paths make sense for this street.	There needs to be more solutions to slow traffic. Especially on the down hill sections, also on the straight away. The north 'transition designation' should be longer and extend to where the road flatters out. I've lived here almost 40 years and when the past 10 years there has been a big increase in speeders. Not just the folks going 30,35, but those going 50 and more! Even tho, most of those 50+ are later at night, the cars going 35+ when you're walking on the small shoulder, is very concerning.	I think A is better, however I live on Holmes Point and care speed directly to the neighborhood streets.	In general wider streets for bikes and pedestrians for neighborhood street.	The 'slow zone' may help, but something more drastic is needed.	Not sure	I responded with no worried about tree only if it means improving roads. Trees need to be kept for any other instances.	5 - Must have it	3	2	5 - Must have it	3 - Not worried about it	3 - Not worried about it							
69	06/24/2021 17:23 PM For the Entry area, the proposed option looks okay. It's critical that the protected bike path be on the 'climbing' side of the road. Also, NEVER use BICYCLES SHARE THE ROAD signs. Motorists interpret them as a message to cyclists, instead of drivers. BIKES MUST USE FULL LANE is much better. Also, arrows should be in the middle of the lane, as cyclists may use the full lane.	For the Transition/Waterfront areas, the minimum standard could use buffers (with bollards or planters) on both sides of the roads. Cyclists and pedestrians are the most vulnerable road users, and need the most protection. For the optional standard, Option B is definitely nicer for walkers. But if residents are concerned about parking availability, Option A could be acceptable if the buffer were more substantial, with bollards or planters providing more visual separation from 'car' space and 'people' space.																		
70	06/24/2021 17:49 PM Separated Pathway	Yes	Proposed A	Speed bumps on waterfront, but don't change all speed limits to 20	no		2	2	2	3	2	4								

May – October, 2021 – Feedback from Survey 2

Participant #	Response Submission Date/Time	Holmes Point Drive: Which cross section matches your view of what the character should be for Holmes Point Drive? Is a buffered bike lane/bike boulevard preferred?	Holmes Point Drive: Do you feel the cross section for the street is still primary?	Neighborhood/local streets: Which cross section best meets the needs as you see them for walking in the neighborhood? Should there be a potential need for additional space?	Neighborhood/local streets: What do you like about the on-street buffered section, like #13, which most meets the needs for the character of the neighborhood? Why? Do you think about creating a neighborhood like zone here? If so, Holmes Point Drive & for local streets?	Do you want dedicated shoulder or neighborhood street?	Additional feedback	Do you want to eventually create a consistently wide continuous sidewalk along Holmes Point Drive, including a 5' wide tree canopy? If so, how would you like to see it implemented? (e.g., tree relocation, and/or tree encasement, etc.)	Do you want to eventually create a consistently wide continuous sidewalk along Holmes Point Drive, including a 5' wide tree canopy? If so, how would you like to see it implemented? (e.g., tree relocation, and/or tree encasement, etc.)	Do you want to eventually create a consistently wide continuous sidewalk along Holmes Point Drive, including a 5' wide tree canopy? If so, how would you like to see it implemented? (e.g., tree relocation, and/or tree encasement, etc.)	Do you want to eventually create a consistently wide continuous sidewalk along Holmes Point Drive, including a 5' wide tree canopy? If so, how would you like to see it implemented? (e.g., tree relocation, and/or tree encasement, etc.)	Do you want to eventually create a consistently wide continuous sidewalk along Holmes Point Drive, including a 5' wide tree canopy? If so, how would you like to see it implemented? (e.g., tree relocation, and/or tree encasement, etc.)	Do you want to eventually create a consistently wide continuous sidewalk along Holmes Point Drive, including a 5' wide tree canopy? If so, how would you like to see it implemented? (e.g., tree relocation, and/or tree encasement, etc.)	Do you want to eventually create a consistently wide continuous sidewalk along Holmes Point Drive, including a 5' wide tree canopy? If so, how would you like to see it implemented? (e.g., tree relocation, and/or tree encasement, etc.)	Time Taken to Complete (minutes)
81	N/A (responding to abridged survey through email)	N/A	N/A	N/A	N/A	N/A	None.	5	2	4	1	1	1	1	
82	N/A (responding to abridged survey through email)	N/A	N/A	N/A	N/A	N/A	None.	1	1	1	5	5	5	5	
83	N/A (responding to abridged survey through email)	N/A	N/A	N/A	N/A	N/A	<p>Use the available width to create a safe bike/ped lane. I'm a 37 year resident in the Holmes Point Overlay. I walk extensively throughout Finn Hill, OO Denny and Champagne Point daily. I would consider the tree canopy and wooded nature of the neighborhoods the most important feature of the area. I support the efforts of the FINA to preserve and expand the green space and tree canopy. I would support a shared ped and bike lane along Holmes Point Dr. but not adding parking along its length. I like the proposed improvements to added parking and walking path for the OO Denny Park as described. The trickiest problems are all the non-standard neighborhood streets (I live on one at NE 116th St of Holmes Point Dr). This is my street on a typical day with families out walking, people getting mail, dropping off/picking up kids at bus stops, walking dogs, and with cars and trucks, all sharing that single lane, unmarked lane. I think flexibility will be needed to design a good solution. Proposed Optional Standard A (advisory shoulder with shared single lane) would be a good default, but on a block by block basis, some limited accommodation should be made to cleverly use space when available to include passing turn outs, limited parking, improved path marking/lighting, etc. Many of these non standard streets from the old "non county maintained road" pre-annexation days are old private lanes and shouldn't/couldn't have curbs, separated/buffered ped/bike lanes or parking.</p> <p>Thanks for your efforts and looking forward to the next steps.</p>	Yes—similar to "proposed minimum standard" illustration, with 5' painted out bike/ped lane.	1	3	4	5	4		
84	N/A (responding to abridged survey through email)	N/A	N/A	N/A	N/A	N/A	<p>The removal trees in the Holmes Point Overlay should be a nonstarter in this project. The tree coverage in this area is what gives this neighborhood its character and it is why many chose to live here.</p>	1	1	1	4	5	5		

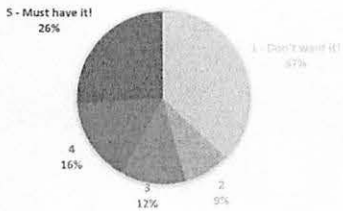
May – October, 2021 – Feedback from Survey 2

External Survey Link

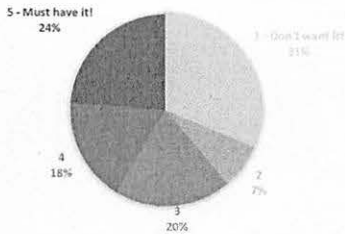
Open 5/20/2021 – 9/26/2021

Summary of Ranked Responses 1 – 74

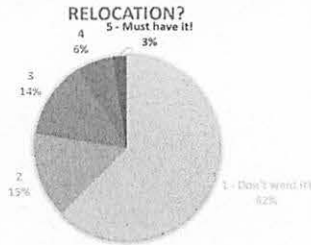
1. DO YOU WANT TO EVENTUALLY CREATE A CONTINUOUS PEDESTRIAN WALKWAY ALONG HOLMES POINT DRIVE, KNOWING IT LIKELY INVOLVES TREE REMOVALS, UTILITY RELOCATIONS, AND RIGHT-OF-WAY ENCROACHMENT RELOCATION?



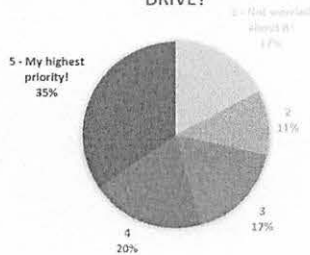
4. DO YOU WANT TO EVENTUALLY IMPLEMENT SPEED REDUCTION MEASURES INCLUDING A SLOW ZONE (20 MPH) ALONG HOLMES POINT DRIVE?



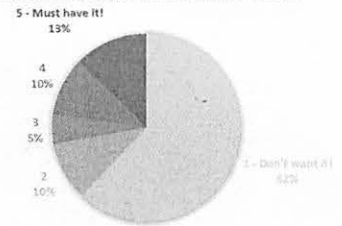
2. DO YOU WANT TO EVENTUALLY CREATE ADDITIONAL PARKING ALONG HOLMES POINT DRIVE, KNOWING IT LIKELY INVOLVES TREE REMOVALS, UTILITY RELOCATIONS, AND RIGHT-OF-WAY ENCROACHMENT RELOCATION?



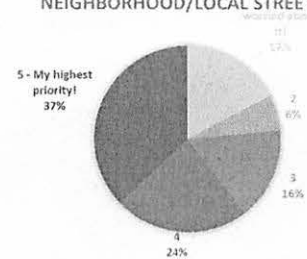
5. WHAT IS YOUR POSITION ABOUT RETAINING TREES ALONG HOLMES POINT DRIVE?



3. DO YOU WANT TO EVENTUALLY CREATE A CONTINUOUS BUFFERED BIKE LANE ALONG HOLMES POINT DRIVE, KNOWING IT LIKELY INVOLVES TREE REMOVALS, UTILITY RELOCATIONS, AND RIGHT-OF-WAY?



6. WHAT IS YOUR POSITION ABOUT RETAINING TREES ALONG NEIGHBORHOOD/LOCAL STREETS?



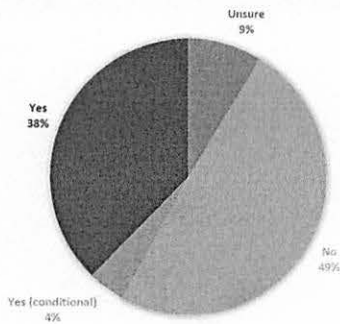
May – October, 2021 – Feedback from Survey 2

External Survey Link

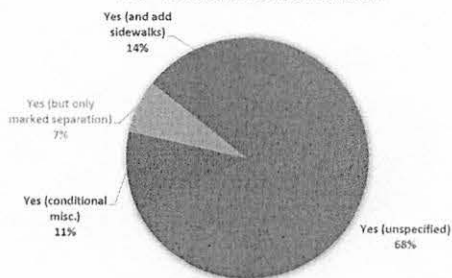
Open 5/20/2021 – 9/26/2021

Summary of Ranked Responses 1 – 74

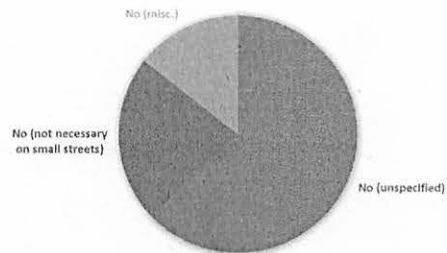
DO YOU WANT DELINEATED SHOULDERS ON NEIGHBORHOOD STREETS?



"YES" COMMENT SPECIFICATION



"NO" COMMENT SPECIFICATION



May 26, 2021 - Transportation Commission Meeting 3

Memorandum to Transportation Commission
May 21, 2021



CITY OF KIRKLAND
Department of Public Works
123 Fifth Avenue, Kirkland, WA 98033 425.587.3800
www.kirklandwa.gov

MEMORANDUM

To: Transportation Commission
From: Hunter Richards, Capital Projects Coordinator
Joel Pfundt, Transportation Manager
Date: May 21, 2021
Subject: HOLMES POINT STREET DESIGN STANDARDS AND CORRIDOR STUDY

Staff Recommendation:
It is recommended that the Transportation Commission receive an update and provide input on the Holmes Point Street Design Standards and Corridor Study.

Background:
Over many years the Holmes Point Overlay Zone has developed with inconsistent street standards. There are several vehicular and pedestrian safety concerns along Holmes Point Drive and the adjoining neighborhood streets, such as pedestrians walking along Holmes Point Drive and school children awaiting bus pickups. With anticipated development in the area, the City needs street standards, so the community knows what to expect. The City is exploring options for unique street standards that recognize the specific nature of the Holmes Point area, but, as always, the City must balance any proposed changes with its duty to provide vehicular and pedestrian safety.

This study was an action identified in of the 2018 Finn Hill Neighborhood Plan update. City Council later allocated funding to carry out this study; however, there is no City funding allocated toward implementing any recommendations that result from the study. Implementation will occur though as part of future private development in the area.

On December 3, 2020 an initial online community engagement was held via Zoom. Staff and the consultant team, Otak Inc, presented early progress on the study and received input from participants. Based on the input received at the first meeting, staff and the consultant team developed a series of street cross section concepts that could be applied to each of the street concept types described in Figure 1. These street cross sections along with a variety of pedestrian and bicycle facility types and speed management solutions were presented at a second online public engagement held on May 20, 2021. The presentation used at this event is included as Attachment A.

The project team will provide the Commission with a summary of the presentation that was made to the community as well as the community feedback received. This information, along

with input from the Transportation Commission, will be used to inform final recommendations that will be presented at a future community engagement session.

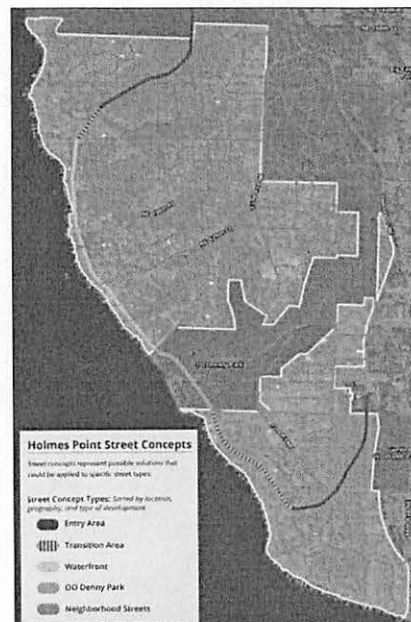


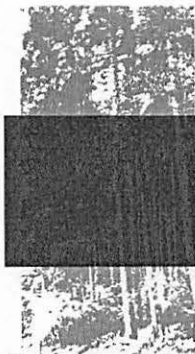
Figure 1. Study Area and Street Concept Types

May 26, 2021 - Transportation Commission Meeting 3

Major Themes and Comments

- This presentation includes feedback from December's public engagement. Another round of revisions will be incorporated based upon these May resident and transportation commission comments.
- Planting strips add to maintenance - who will maintain these?
- Clarify that if using sharrows, cyclists will be in the street. Physical separation for pedestrians would be beneficial; avoid bicycle facilities mixing with pedestrian facilities. Curb or candlestick buffers? Will ask community.
- Why is pedestrian strip on east side running along the road? Limited space; drainage ditch and utility conflicts. Path could switch sides based upon constraints but minimize street crossings.
- Understand the need for flexibility based upon topography, but have design be as consistent as possible. Prioritize pedestrian developments (minimum standard) over parking (optional standards).
- Park path could continue on both sides of the park. Will clarify shoulder shown in rendering along the east side of park.
- Explore options for physical traffic calming structures and increasing enforcement. Maybe add more lighting for pedestrians crossing the park corridor.
- Suggested option to add winding/separated path around established trees in neighborhood area instead of straight path. Some lanes are very narrow in the neighborhood area; focus on some consistency between extreme road width variations.

December 15, 2021 - Neighborhood Meeting 3



Otak
works

Holmes Point Overlay Zone Street Design Standards & Holmes Point Drive Corridor Study

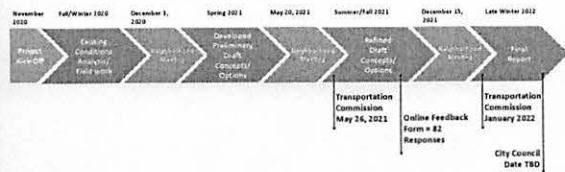


Presentation Topics

- Process to Date and What We've Heard
- Holmes Point Drive Recommendations
- OO Denny Park Frontage Recommendations
- Neighborhood Streets – Recommended Options
- Next Steps

Summary of Process to Date and What We've Heard

Process



Slide 4

December 15, 2021 - Neighborhood Meeting 3



Pedestrian Paths

- Desire for continuous pedestrian path along Holmes Point Drive
- Little interest to widen existing road widths to build buffered pathways
- Concern that adding impervious materials will increase drainage issues
- Gravel path/shoulder would provide separation from vehicles



Tree Retention

- Retaining trees is a priority along HPD and along neighborhood streets
- Tree removal should be on a case-by-case basis, mostly based upon sight distance.



Bike Lanes

- Some community interest for continuously shared mixed-use bike/pedestrian paths; pedestrians are a priority
- Vehicles to share the road with cyclists on downhill and flat sections
- Separated bike lane needed on uphill climbing sections of the road, when cyclists are slower



Parking

- Concerns that parking facilities along Holmes Point Drive would require road widening and excessive tree removal
- Additional parking facilities are not the priority in OO Denny Park area
- Mixed-use neighborhood streets regularly used for temporary parking

Slide 8

December 15, 2021 - Neighborhood Meeting 3



Speed Reduction

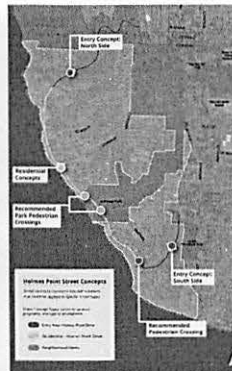
- Speed reduction along Holmes Point Drive and in OO Denny Park
- Posted speeds do not need to be reduced, but vehicles need to follow them
- Suggestions to add raised crosswalks in OO Denny Park to slow traffic



Additional Comments

- There needs to be a flexible approach, specific to Holmes Point– not a one-size-fits-all
- Stormwater management will be a major consideration with future development

Final Draft Recommendations for Street Design Standards

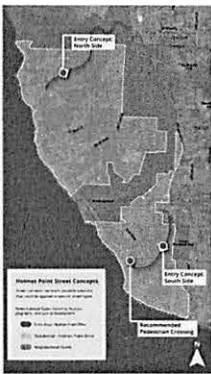


Holmes Point Drive

- **Entry** segments (forested, steeper gradient) = buffered pedestrian/bike lane
- **Residential** segments = protected pedestrian/bike lane

Slide 12

December 15, 2021 - Neighborhood Meeting 3

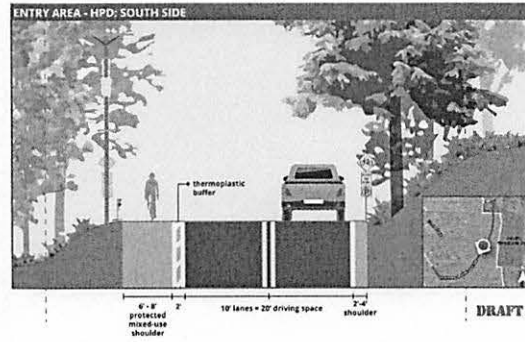
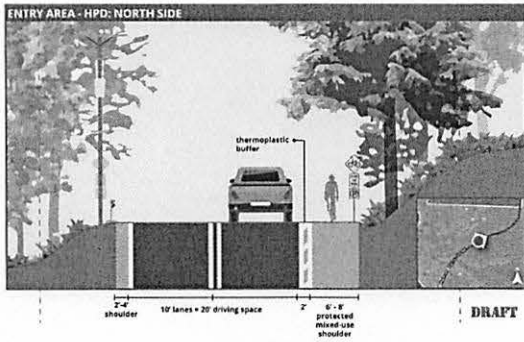
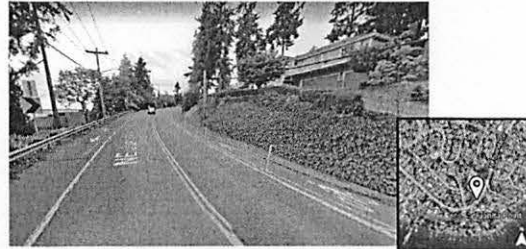


Entry Area

- Entry segments are forested and have a steeper gradients
- Proposed Option: Buffered shoulder for bicyclists and pedestrians on the uphill/ climbing side of the road.

Buffered Bike/Pedestrian Shoulder

Example of a buffered shoulder climbing lane – NE Juanita Drive



December 15, 2021 - Neighborhood Meeting 3

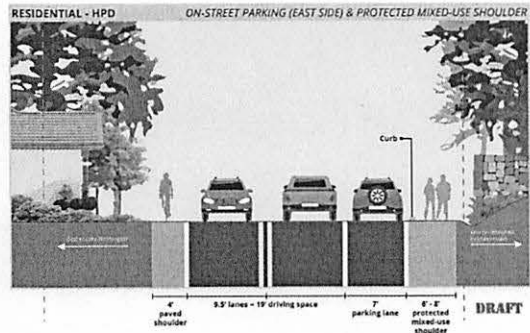
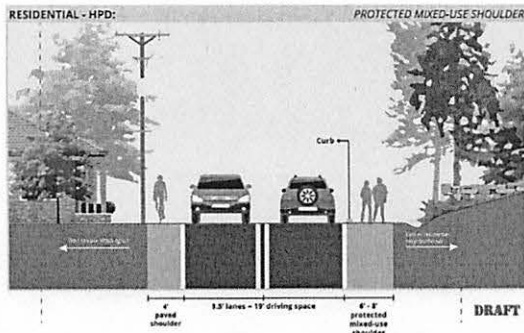


**Holmes Point Drive:
Residential Area**

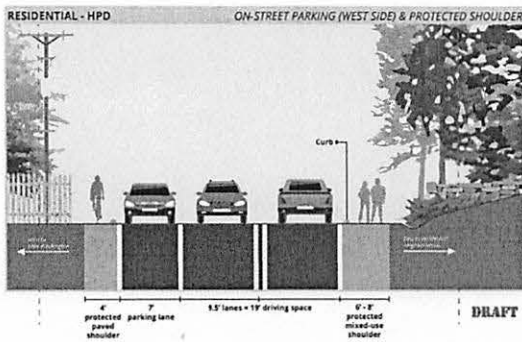
- Residential areas of Holmes Point Drive vary in available road width
- Curb protected mixed use (for pedestrians and bicyclists) shoulder on the east side of the road
- Standard four-foot shoulder on the west side of the road
- Could provide on-street parking on the east or west side of the road (if west, would add curb).

Curb Protected Pedestrian/Bike Shoulder

Example of a curb protected shoulder— 84th Ave NE



December 15, 2021 - Neighborhood Meeting 3



Questions and Discussion

- Holmes Point Drive – Entry Areas
- Questions or comments?
- Holmes Point Drive – Residential Area
- Questions or comments?

Please put questions/comments in the Q&A.

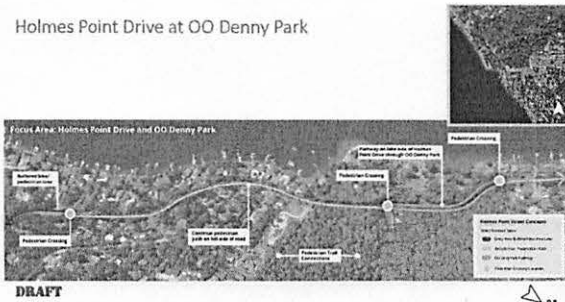


OO Denny Park Frontage

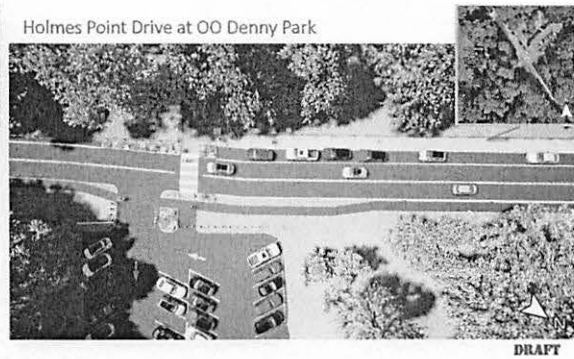
- Paths along both sides of Holmes Point Drive
- Improved crossing locations
- Parallel parking spaces on west side

December 15, 2021 - Neighborhood Meeting 3

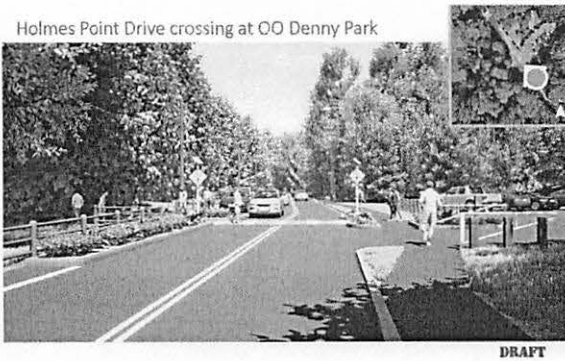
Holmes Point Drive at OO Denny Park



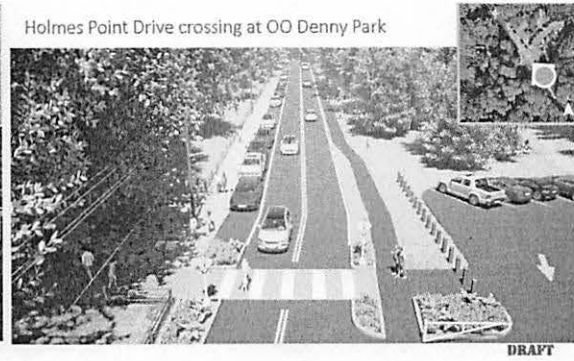
Holmes Point Drive at OO Denny Park



Holmes Point Drive crossing at OO Denny Park



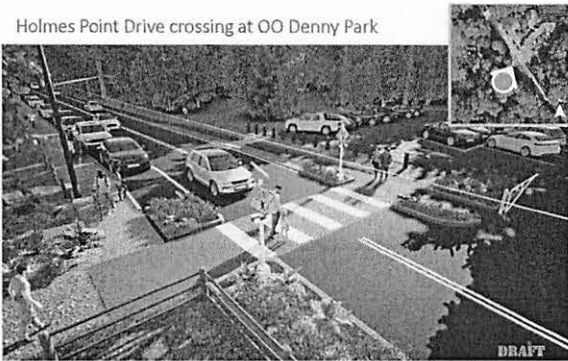
Holmes Point Drive crossing at OO Denny Park



Slide 28

December 15, 2021 - Neighborhood Meeting 3

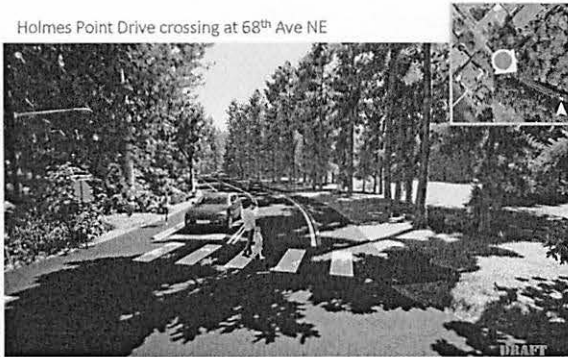
Holmes Point Drive crossing at OO Denny Park



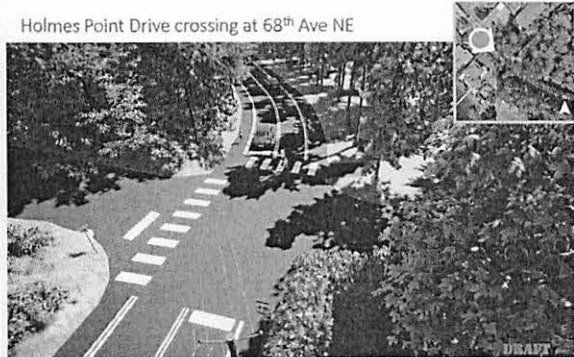
Raised Crossings Provide Safety and Traffic Calming Benefits



Holmes Point Drive crossing at 68th Ave NE



Holmes Point Drive crossing at 68th Ave NE



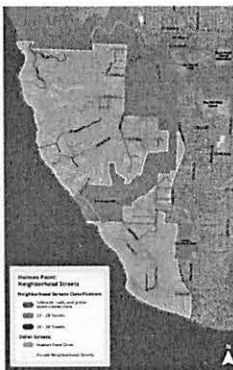


Questions and Discussion

Holmes Point Drive – OO Denny Park Frontage

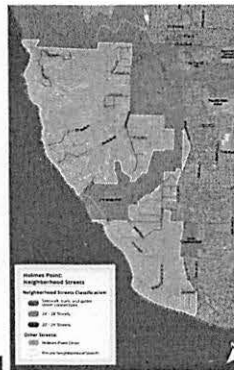
- Questions or comments?

Please put questions/comments in the Q&A.



Neighborhood Streets Plan

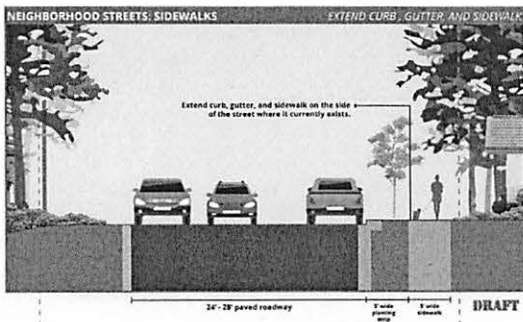
- Streets with existing and proposed segments of curb, gutter, and sidewalk = maintain or continue this standard (with sidewalk on at least on one side of street)
- 24' to 28' Nominal Pavement Width = discuss options
- 20' to < 24' Nominal Pavement Width = discuss options



Sidewalk, curb, and gutter street connections

- Streets with existing and proposed segments of curb, gutter, and sidewalk; or streets that connect to other pedestrian facilities
- Maintain this standard (with sidewalk on at least on one side of street)
- Streets connecting to Holmes Point Drive and NE Juanita Drive NE

December 15, 2021 - Neighborhood Meeting 3

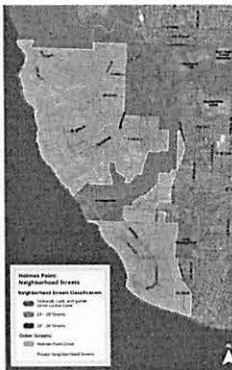


Questions and Discussion

Neighborhood Streets

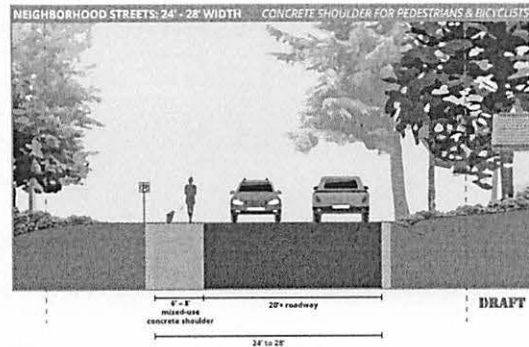
- How do you feel about the concept for continuing sidewalk, curb, and gutter on the proposed streets?

Please put questions/comments in the Q&A.



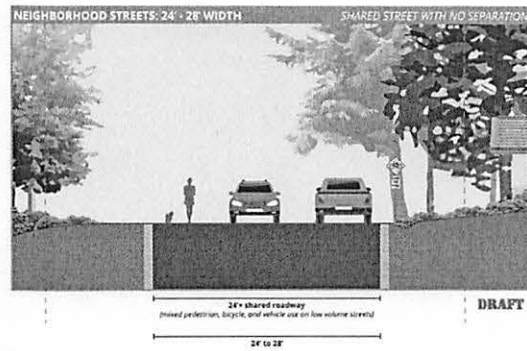
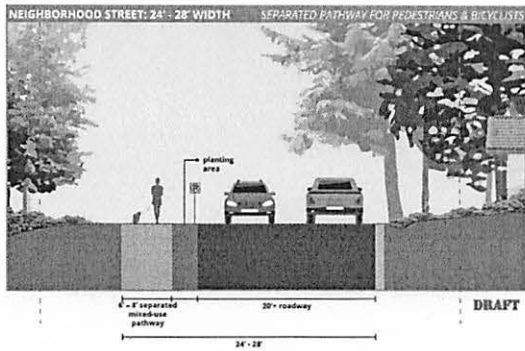
24' – 28'
Nominal Pavement Width

- Option A: Mixed-use concrete shoulder for pedestrians and bicyclists
- Option B: Separated mixed-use pathway for pedestrians and bicyclists
- Option C: Shared street with no separation between drive lanes, pedestrian, or bicycle use.



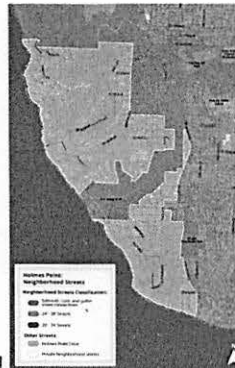
Slide 40

December 15, 2021 - Neighborhood Meeting 3



24' - 28' Neighborhood Streets

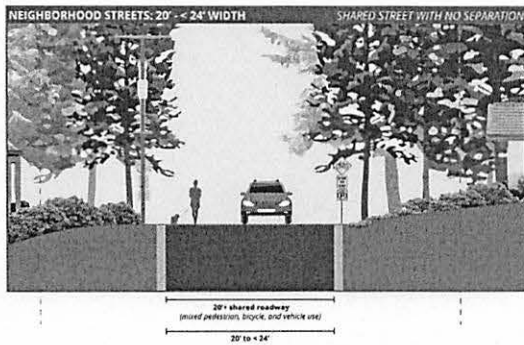
- What option do you like for 24' - 28' streets?
 - Concrete mixed-use shoulder
 - Buffered mixed-use pathway
 - Shared roadway (with share the road signs)



20' to <24'
Nominal Pavement Width

- Narrow streets with lower traffic volumes
- **Proposed Option:** Shared street with no separation between drive lanes, pedestrian, or bicycle use.
- Streets will include "Share the Road" signage

December 15, 2021 - Neighborhood Meeting 3



Questions and Discussion



Questions and Discussion

Neighborhood Streets

- How do you feel about the concept for shared use streets on streets with widths of 20' to < 24'?

Please put questions/comments in the Q&A.



Questions and Discussion

Holmes Point Drive – Entry Areas

- *Questions or comments?*

Holmes Point Drive – Residential Area

- *Questions or comments?*

Holmes Point Drive – OO Denny Park Frontage

- *Questions or comments?*

Neighborhood Streets

- Sidewalk, curb, and gutter
- 24' to 28' streets
- 20' to < 24'

Other questions or comments?

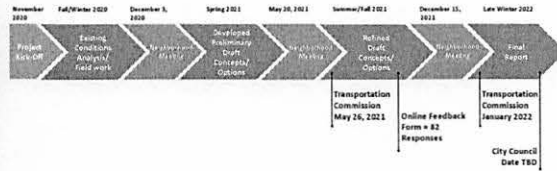
Slide 48

December 15, 2021 - Neighborhood Meeting 3

Next Steps

Next Steps

- Transportation Commission
- City Council
- Pre-Approved Plans/Development Standards



Thank You!

Slide 51

December 15, 2021 - Feedback from Public Meeting 3

December 15, 2021 - Virtual Public Meeting Feedback

During this public meeting, the project team presented the feedback that was received at the May 2021 public meeting, results from the online survey, and how this public input had shaped the recommendations for the Holmes Point Overlay Area. Revised recommendations were presented to the community for each key area in Holmes Point. After each section, the public had an opportunity to ask questions and make comments in the chat about recommendations for each location. This feedback was used to further refine and finalize the recommendations. A summary of feedback and questions that was received during the question and answer portion of the presentation is summarized below:

Agenda:

1. **Process to Date and What We've Heard**
2. **Holmes Point Drive Recommendations**
 - a. *Questions and Discussion*
3. **OO Denny Park Frontage Recommendations**
 - a. *Questions and Discussion*
4. **Neighborhood Streets - Recommended Options**
 - a. *Questions and Discussion*
5. **Next Steps**

Summary of Neighborhood Feedback and Questions on Holmes Point Recommendations

Holmes Point Drive - Entry Areas:

- Support for buffered shoulder for bicyclists and pedestrians.
- Interest in possibly narrowing the width of the protected shoulder.
- Concern over possible road widening effects of implementing buffered shoulder.

Holmes Point Drive - Residential Areas:

- Desire for a continuous, protected pedestrian path that minimizes road widening.
- Support for incremental sections of parking where existing width allows, rather than continuous street parking.
- Many residents are concerned about the impact that road widening may have on private property and trees.
 - Strong desire to protect private property that may be impacted by improvements.

December 15, 2021 - Feedback from Public Meeting 3

Holmes Point Drive - Residential Areas (continued):

- Interest in preserving trees that may be in the footprint of the pedestrian shoulders. Very concerned about impacting natural character of the neighborhood.
- Neighbors wonders if they will be compensated for relocated/removed private property in the right-of-way.
- Concern about if utility poles will need to be relocated, and if they will be moved into private property.
- Interest in seeing more speed calming solutions, such as raised crossing or speed bumps.
 - Would like to see raised crossings further north than just OO Denny Park.
 - Interested in seeing speed bumps near major crossings.
- Questions about if the center line will be shifted to accommodate larger shoulders, or if the shoulders will be built out from the current roadway. Concerns about excessive road widening.
- Questions about how much the road would need to be widened to accommodate the recommendations.
- Concerns about topographic issues on Holmes Point Drive. Major cut or fills could require retaining walls and drive up the development cost.
 - Concerns about funding availability.
 - Concerns about erosion or slide risks.
- Concerns about dangerous conditions such as sharp turns and blind corners on Holmes Point Drive.
- Some residents expressed worry that a 6 - 8 foot pathway will remove most street parking, and would prefer a smaller path.
- Community is wondering how these improvements will occur, since redevelopment has already occurred in many places along Holmes Point Drive. There may be less opportunities for infill.

Holmes Point Drive - OO Denny Park:

- Strong support for raised crossings. Many feel that raised crossings will benefit speed calming.
- Curb on the east side of Holmes Point Drive will prevent illegal parking and improve safety along park.
- Interest in preventing chaotic parking.
 - Will parking be permit only?
 - Concerns that expanding parking may negatively affect nearby neighborhoods.
 - Interest in removing parking from the east side of the road. Can be dangerous due to the open ditch.
 - Some concern over parking being removed on the north and west side of Holmes Point Drive.
 - Some concern that parking may overflow in residential areas.
- Concerns about possible noise from raised crossings.
- Concerns about open ditches - will they be covered?
- Desire for more raised crossings and speed reduction solutions throughout Holmes Point Drive.

December 15, 2021 - Feedback from Public Meeting 3

Holmes Point Drive - OO Denny Park (continued):

- Interest in adding a raised crossing to the north side of the park.
 - Some feel the flashing lights would be more effective at slowing cars than a raised crosswalk.

Neighborhood Streets - Sidewalk, Curb, and Gutter Street Connections

- Neighborhood feels that proposed sidewalks would encourage pedestrian connectivity in key neighborhood streets.
- Concern about what the timeline is for implementing sidewalk, curb and gutter on these streets.
- Concern about private property and trees that may be impacted by improvements.
 - Could buffered sidewalks be designed to preserve trees?
 - Concern about removing trees to install sidewalk, curb, and gutter.
- Interested in enclosing ditches for safety purposes.
- Some residents feel that drivers are more likely to slow down if they are sharing the road with pedestrians, rather than separating them with sidewalks.
- Some interest in installing sidewalks on both side of the street.
- Some concern about street width - desire to avoid parked cars blocking traffic
- Desire to look at installing sidewalks where children catch the bus.
- Interested in improving NE 130th St, as it connects Holmes Point Drive to 72nd Ave to Juanita Drive.

Neighborhood Streets - 24' - 28' Nominal Pavement Width

- Concern about the timeline for making improvements to neighborhood streets.
 - If improvements are triggered by infill, there are concerns about the amount of time it will take to install infrastructure.
 - Concerns that it will take many years for pedestrian facilities to be continuous.
- Concern about private property that may be impacted by improvements.
- Desire to install pedestrian facilities that maximize safety and minimize road widening.
 - Some feel that a buffered path may not provide additional safety benefits, but will require more space.
- Concerns about cost of implementing recommendations.
- Interest in using paint rather than concrete for pedestrian shoulders.

Neighborhood Streets - 20' - 24' Nominal Pavement Width

- Support for maintaining streets that are less than 24 feet wide as shared streets
- Narrow streets with no through traffic or low traffic volumes require less pedestrian facilities.
- Interest from neighborhood about curb and gutter - are these a standard code that will apply to side streets?

December 15, 2021 - Feedback from Public Meeting 3

In-Meeting Poll

24'-28' Neighborhood Streets Preference Poll

Poll ended | 2 questions | 17 of 24 (70%) participated

Concrete mixed-use shoulder	(6/17) 35%
Buffered mixed-use pathway	(8/17) 47%
Shared roadway (with share the road signage)	(3/17) 18%

2. After seeing the 24'-28' neighborhood streets concepts, please select your LEAST preferred concept (Single Choice) *

17/17 (100%) answered

Concrete mixed-use shoulder	(2/17) 12%
Buffered mixed-use pathway	(5/17) 29%
Shared roadway (with share the road signage)	(10/17) 59%

December, 2021 – January, 2022 – Feedback from Survey 3

External Survey Link
Open 12/15/2021 – 1/15/2022

Participant #	Response Submission Date/Time	Time Taken To Complete (minutes)	Comments	Neighborhood Street Concepts: How do you feel about the concept for Concrete sidewalks, curb and gutter on the proposed street? (link streets on the map)	After seeing the concepts, what is your preferred neighborhood street standard for 24'-30' streets?	Why is this your preferred concept for the 24'-30' neighborhood street standard?	Of the remaining two concepts, which is your LEAST preferred street standard for 24'-30' streets?	Why is this your LEAST preferred street standard?	How do you feel about the concept for the shared use streets on streets with widths of 30 to 54' (link streets on the map)
1	12/17/2021 16:31 PM		Holmes Point Drive: Do you have any questions or comments on Holmes Point Drive entry area, residential area, or OO Denny Park?		Shared roadway (with share the road signage)	If bicycles and pedestrians must share space, that space must be wide.	Buffered mixed-use pathway	Don't force bicycles and pedestrians together.	Great, share the space
2	12/19/2021 0:13 AM	3.3No		Good	Buffered mixed-use pathway	less intrusive, cost effective	Shared roadway (with share the road signage)	Expense	Good
3	12/18/2021 10:30 AM		I oppose use of curbs on east side of residential area because it will eliminate much existing parking which is needed given difficulty in providing off-street parking on properties with steep driveways. Plan ignores need for traffic calming measures on residential area of Holmes Point Drive particularly to reduce speed of cars coming from north. Traffic island at 62nd Ave NE or 64th Pl is needed to slow traffic & entering the residential area from the north.		Concrete mixed-use shoulder		Buffered mixed-use pathway	shared roadway	Okay
5	12/26/2021 13:32 PM	15.6visual clutter and ineffective.	Entry areas okay for protected mixed-use shoulder, but I would prefer 4'6" width instead of 6'-8" width. Residential concepts: No on street parking except at OO Denny Park. Park Footage: pathway on lake side is ok. Either raised crossing or pedestrian crossing light; not both. Neighborhood streets plan: Streets with existing curb etc. keep as is. No extension is needed. 24'-28' streets- prefer option C, shared roadway. 20'-26' streets no signs are needed. After 30 days they are just visual clutter and ineffective.		Shared roadway (with share the road signage)	Wider. Streets encourage faster vehicle speeds	Concrete mixed-use shoulder	More runoff leads to drainage issues	Excellent.
6	01/07/2022 11:35 AM	10.5	I was shocked the draft did not include any traffic calming treatments. The number one problem with this is safety due to average car speed is over 32mph. Consequently, the number one purpose of this draft to address the safety issue regarding vehicle speed. If motorists drove 25mph there would be no safety issue. Many of the draft plans would increase vehicle speed as evidence based results show any time a road width is increased motorist speed increases. Shockingly, many aspects of your plan increase the safety risk on foot. Read the plan and make that actually safer by treating the speed.	I do not support if these treatments if trees are removed and the treatments change the character of the neighborhood.	Buffered mixed-use pathway	Any other treatments would change the character of the neighborhood. Design must include traffic calming to improve safety	Concrete mixed-use shoulder	Out of character of the feel on the neighborhood	fine if design includes traffic calming and no tree cutting
7	01/08/2022 12:34 PM	5.4		Don't need separate sidewalks.	Shared roadway (with share the road signage)		Concrete mixed-use shoulder		Let's avoid spending hundreds of millions of dollars.
8	01/12/2022 17:54 PM	12.5	I was unclear as to whether the pedestrian area is throughout ALL of Holmes Point or only along the flat. I then switches to a bike path.	I think that there should be a raised sidewalk throughout HPD, that shared spaces is responsible for cars and bikes, but that pedestrians should have their own designated area. I am unconvinced that a raised curb will inhibit vehicles from just jumping over it and parking wherever they like. We already have a LOT of signs indicating No Parking that are completely disregarded.	Concrete mixed-use shoulder	It isn't, you don't have my preferred concept, which is a sidewalk on HPD. I don't believe a shoulder will be honored by vehicles.	Shared roadway (with share the road signage)	My least preferred concept is a shared roadway for Pedestrians, Bikes and cars may share.	The option that I prefer is not listed above, which is for there to be designated pedestrian sidewalks. There is no need for bicycles to have their own place on the road, as they either ride at the pace of traffic, or can have a little painted bike to indicate shared roads. Pedestrians, especially children, need to have safe travel throughout the neighborhood.
9	01/13/2022 18:45 PM	16.3	My neighbors and I are very disappointed that there is no plan for sidewalks on Holmes Point Road. I attended every meeting and feel that every resident I heard from expressed a strong desire for sidewalks as a key aspect of a viable plan. That they were not included is a strong indication to me that we were not listened to in any meaningful way. I do not know anyone in the neighborhood who does not strongly desire real sidewalks on Holmes Point Road (a lane of pavement is not a sidewalk). The road is not safe for children or pedestrians, yet is a heavily walked route. The proposed plan does very little to improve this. We do not want to share asphalt walking lanes with bicycles, while being unseparated from 16.3 traffic. My family and neighbors are encouraging all to oppose this plan.	This is necessary, but strongly prefer a buffered mixed-use pathway over a shared roadway. A buffered pathway is so much nicer and safer.	Buffered mixed-use pathway	This is by far the most attractive option. The buffer also adds considerable safety. There are so many small children in the neighborhood that I worry about being hit by a car.	Shared roadway (with share the road signage)	This is the least safe option for families with small children around and is also the least attractive visually.	I would prefer sidewalks on these streets as well. However, they are not as critical as the other streets. It is far more important that the plan be changed from its current state to include buffered sidewalks on Holmes Point Road.
10	01/13/2022 18:58 PM	20.6	Yes, the residential area north of OO Denny Park where I live on the lake, I'm concerned that if you take any property away in front of my house, I won't have room to park in front of my garage. I would be forced to use street parking along with my guests that I don't normally use. To create a four foot shoulder/buffer in front of my house on the west side would also require moving the telephone pole @ 20.great expense.	Curbs are fine, to protect pedestrians & bikes. This will NOT slow traffic down, which is a huge problem with the straight away north of the park. In the summer there is street racing in front of my house with speeds in excess of 60 mph. This is very scary & dangerous. I have attempted to report it with no results. Can speed humps be installed to slow the cars down? I would suggest raised crosswalks, however there are no cross streets in this area. The bikes riders are very aggressive & ride in excess of the speed limit. Speed humps will slow down the bike riders down as well. In regard to OO Denny Park parking, when the park is full in the summer, cars use all the street parking far north of the park. If you leave the park parking the same, it doesn't solve anything. It just puts more pressure on the street parking which the residents need to use for our guests. Maybe have permit parking like Seattle for the residents?	Shared roadway (with share the road signage)	This is the character of neighborhood since I have lived in this area for 25 years	Concrete mixed-use shoulder	This will take away property & current street parking	I don't live in this area so I don't have an opinion.

December, 2021 – January, 2022 – Feedback from Survey 3

Participant #	Response Submission Date/Time	Time Taken to Complete (minutes)	Response	Neighborhood Street Concepts: How do you feel about the concept for continuous sidewalks, curb and gutter on the proposed streets? (link streets on the map)	After seeing the concepts, what is your preferred neighborhood street standard for AL? 2E streets?	Why is this your preferred concept for the 2E-2E neighborhood street standard?	Of the remaining two concepts, which is your LEAST preferred street standard?	Why is this your LEAST preferred street standard?	How do you feel about this concept for the shared use streets (all streets with widths of 20' to 24')? (blue streets on the map)
			<p>Holmes Point Drive: Do you have any questions or comments on Holmes Point Drive entry area, residential area, or OO Denry Park?</p> <p>Yes I do have overall comments about the project. The particular interest to me & was the solidarity on many of the comments that were posted here among residents and neighbors of the community. It gives the impression that the comments are recognized and collected but they're not weighted very seriously, retaining the concerns don't elevate them from the principle needs of the project. I truly wonder whether the board or the people in the authority chain of responsibility are heeding this advice. When I see bike lanes as wide as they were in the first go around, now resulted in the hierarchy of priority a baffles me. But after what downtown Seattle experienced with lobbying by bicyclists I guess I shouldn't be surprised. They seem to be lobbying everywhere for rights that they desire but not despire as opposed to the rights of homeowners and residents. Holmes point community is a walking community not a bicycling community, somebody needs to recognize the facts not the fiction. Again, confirm this fact yourselves, get out and see it for yourself. I certainly give credit for the projects in which we have now more clearly separated the lanes. The best solution for the process outcome is to customize these concepts according to need. (MHC)</p> <p>Could we add city transportation to consider adding bus stops at the two ends of Holmes Point Drive where it meets Juanita Dr? During big winter snow, it's extremely hard for residents to reach urgent care (one near evergreen hospital), one in downtown Kirkmore.</p> <p>30.4. Pharmacy (Starbucks or Walgreens), grocery (SFCU), by walk.</p> <p>On all neighborhood streets (dead ends) use the shared street option with no separation. Mixed ped, bikes and vehicles. Reason: these streets have no through traffic, it is low volume, and the neighbors know about pedestrians and bike use.</p> <p>13 11:24 AM 5.1.41 the intersections of 12th and 13th with Holmes Point Drive, enlarge for a safe school bus stop location.</p> <p>We fully support the proposed design/ideas around improving the "Entry Area". As residents of the neighborhood, we use both the north and south entrances with our vehicles on the daily and encounter walkers, runners, and bikers who are forced to use the road due to the lack of space. We also use the road for walks and runs but will only use the south entrance due to the space limitations on the north side. We would love to be able to walk/run on the north side as it is a beautiful area and would allow us to connect to more of the local trails without having to use our vehicles.</p> <p>We fully support the proposed design/ideas for improvements to OO Denry Park, especially around parking and crossings. As a family with a young child, we go to the park almost every weekend and are amazed at how quickly vehicles drive through the area. The crossing between the east parking lot and the park needs improvement. We often witness service trucks racing through this straight stretch of the road and they are not aware of the crossing area. An improved walking path next to the park is also needed as it can be challenging to walk near the park with fast vehicles and random parking spots along the park. In the summer months, the parking around the park often drains access through the area as people decide to make up their own way to park in any space they can find, causing issues both for walking and driving.</p> <p>As we do not live on the side streets and do not use them frequently, we do not have any input, positive or negative, on those recommendations.</p> <p>We do have major concerns with the design/ideas for the Holmes Point residential area, as we live on this road. We recognize the desire and need to offer a safer path for pedestrians and to slow down vehicles, however the current recommendations for widening the road would cause significant challenges for the residents and not necessarily address the issues.</p> <ol style="list-style-type: none"> 1. Removal of Vegetation (part 1) - Holmes Point is a protected area and as such it can be challenging to remove any trees on our property, even when diseased and at risk for falling and causing significant damage. We are requested to do everything possible to save trees, pay for arborists and other specialists, write up a case for removal, and apply for a permit. For the city to heavily monitor and regulate this area to keep it green in support of both the Holmes Point center and the city's tree canopy goals, only to come through to rip out a significant amount of trees and vegetation to add pavement is completely out of sync with this. 2. Removal of Vegetation (part 2) - Our property has significant mature vegetation along the road to provide privacy, reduce road noise, and provide safety to our family (that would be at risk for removal with the proposed plans. If they were to be removed, our house would be essentially on the sidewalk or parking spaces at some points with little space to add back any natural elements to this area of the property. This appears to be the case for a majority of the homes along this area. This would negatively impact the value of all of the properties as well as the general aesthetics of the neighborhood for adding more pavement and losing the road-side aged houses. 3. Parking - The current recommendation would significantly cut into our property's existing parking spaces and would result in us needing to park our vehicles in the public parking strip. Our house was built in the 1920s, has the same footprint today, and the property's single garage was not designed for the size of today's vehicles. This results in the need for parking in front of the house but still on our property today. With the proposed changes, we would need to park on the street, taking up spaces ourselves and our neighbors rely on for visitors. How many other houses would be in the same situation as ours, resulting in increased need for parking on the street? This would cut us in the same spot as today or worse. Additionally, in the summer months when the park is full the public parking strip would be filled during the day resulting in significant challenges for us returning from work and finding parking. 4. Storm Water Runoff - In the past couple of years, several of our neighbors have had challenges with basements flooding. We are concerned that the increase in pavement along with the removal of vegetation will only exacerbate this issue in the neighborhood causing continually issues for our neighbors and expand the problem to more properties. <p>We support continuing to explore ways to slow down traffic through this area to allow for safer conditions for pedestrians but do not believe the current designs will provide the benefit needed to justify the cost or the impact to the residents.</p> <p>14 01/15/2022 12:11 PM 4.0</p> <p>I am concerned that there are still no sidewalks planned for Holmes Point Drive. The current proposed plan does not feel safe and I do not understand why sidewalks are not included in the waterfront area. Also, I am not sure why a bike lane is included. The bicyclists relying on as fast or often faster than cars can see, they routinely break the speed limit. Putting them with walkers seem dangerous. There are several school 7. Also stops on Holmes Point Drive and there is not a good place for small kids to stand or for them to walk along the road safely to the bus stop.</p> <p>15 01/15/2022 22:45 PM</p> <p>I would like to make a comment in addition to the survey answers I provided to your December presentation. The proposal to date do not include an effective traffic calming strategy for traffic coming down Holmes Point Drive from the north and entering the residential section. That traffic is coming down through a seemingly rural area and as a result many drivers are going well over 30mph, even though the posted speed limit is 25. That, most of them enter the residential section of Holmes Point Drive where I live at a high rate of speed. I have lived at 13215 Holmes Point Drive for 36 years and have observed this problem first hand. I have had two dogs hit by cars coming too fast down Holmes Point Drive. It is a dangerous condition which would easily be mitigated by installation of a traffic island at either the intersection of Holmes Point Drive at 62nd NE or its intersection at NE 131st Place. A traffic island at the entrance into the residential area would slow down the traffic and with appropriate signage will make drivers realize they need to honor a slower residential zone. The traffic island could be similar to the ones on 7th Avenue and 3rd Street which have successfully calmed traffic in the Northpark neighborhood. Please consider adding this traffic calming strategy to your report or pass on to the Commission and Council the concern of this long time resident for addressing a serious problem with traffic entering the residential area of Holmes Point Drive from the north at high rates of speed. I know many of my neighbors share this concern and would like to see a traffic island installed as part of a traffic calming strategy. Thank you for your efforts.</p>	<p>I think that is an overbroad of what is required there. Although a need for improving storm water removal and gutter effective ability to shed water, building curbs is old fashioned and unnecessary in my opinion. Expensive Curbs scratch and dent more tires and wheels and create obstacles for traffic and people and bikes. Looking at Seattle for example in their residential backstreets, all of that was deemed to be the end all and be all for the community in the 50's to a code ideal. Today the curbs are rubbled in many areas and the street plantings aren't serviced by the city any longer and there's not enough pride in ownership for the residents themselves to take on the responsibility. They look good for a period of years until someone else in future leadership decided that there was not enough budget to service or maintain these areas. So my recommendation is don't over construct back streets. The neighborhoods don't want or need. The more coverage over certain areas in the street margin just means that water will be directed down to where the stormwater runoff problems exist on the lower levels. They've been covering too much of the Greenback for decades. It's a greater task base I understand, but it makes many suffer in flooded areas. Find a balance and maintain it.</p> <p>It's nice to have when the road width allows.</p> <p>As said above, I don't like this concept on neighborhood dead end streets.</p>	<p>Concrete mixed-use shoulder</p> <p>Concrete mixed-use shoulder</p> <p>Shared roadway (with share the road signage)</p> <p>Shared roadway (with share the road signage)</p> <p>Buffered mixed-use pathway</p> <p>Buffered mixed-use pathway</p>	<p>Not much traffic anyway so, preserve the natural beauty of the margins. This is why it is popular among visitors and residents.</p> <p>more cost effective, more flexible</p> <p>Shared roadway (with share the road signage)</p> <p>See first comment</p> <p>Shared roadway (with share the road signage)</p> <p>This is safest and most attractive.</p>	<p>Overbuilt expensive solutions infrastructure before facts structure. Bury power lines, improve storm drain capacity. Create a more beautiful park that is limited in parking and increase safety for pedestrians and their families and pets. There are very good things visualized here but the shoulders curbs. A complete vision is blurred by so many outside interests and influences.</p> <p>dangerous for joggers or seniors/children</p> <p>See first comment</p> <p>This seems dangerous and it is ugly.</p>	<p>I feel the same. Don't overbuild, concentrate on the key areas on the drive itself. Prioritize core infrastructure before facts structure. Bury power lines, improve storm drain capacity. Create a more beautiful park that is limited in parking and increase safety for pedestrians and their families and pets. There are very good things visualized here but the shoulders curbs. A complete vision is blurred by so many outside interests and influences.</p> <p>very appropriate.</p> <p>I favor this.</p>	

additional enabled comment

January 26, 2022 - Transportation Commission Meeting 3



CITY OF KIRKLAND
Department of Public Works
123 Fifth Avenue, Kirkland, WA 98033 425.587.3800
www.kirklandwa.gov

MEMORANDUM

To: Transportation Commission
From: Aimee Alcock, Project Coordinator
Joel Pfundt, Transportation Manager
Date: January 20, 2022
Subject: HOLMES POINT STREET DESIGN STANDARDS AND CORRIDOR STUDY –
DRAFT RECOMMENDED STANDARDS

Staff Recommendation:

It is recommended that the Transportation Commission receive a briefing on the draft recommended improvements of the Holmes Point Street Design Standards and Corridor Study.

Background:

Over many years, the Holmes Point Overlay Zone has developed with inconsistent street standards. There are several vehicular, pedestrian and bicycle safety concerns along Holmes Point Drive and the adjoining neighborhood streets, such as pedestrians walking along Holmes Point Drive and school children awaiting bus pickups. With anticipated development in the area, the City is exploring options for unique street standards that balance the specific nature of the Holmes Point area with vehicular and nonmotorized safety considerations.

This study was an action identified in the 2018 Finn Hill Neighborhood Plan update. City Council later allocated funding to carry out this study, but there is no City funding allocated toward implementing any recommendations that result from the study. Implementation will occur primarily as a part of future private development in the area.

Since the project began, resident meetings have been held for input on the study. In each meeting, City Staff and the consultant team, Otak Inc, presented progress on the study and received input from participants. Resident meetings were made available for playback on the Holmes Point Street Design Standards and Corridor Study's website. After the second and third meetings, online feedback forms were advertised to provide further input on the study.

The Transportation Commission received an update for this project on May 26, 2021 which outlined the study area, history, and anticipated schedule for Commission input. Since May, design development has included these additional outreach programs:

- Updates and event reminders – sent to the Finn Hill Neighborhood Alliance and 251 registered email contacts
- Summer/Fall, 2021 – 82 responses through online feedback form

- December 15, 2021 – 52 registered attendees and 27 live participants at neighborhood meeting
- December 2021/January 2022 – 16 responses through online feedback form
- January 26, 2022 – Transportation Commission recommendation review
- Date TBD – City Council recommendation presentation

Based on the input received from these meetings and feedback forms, Staff and the consultant team developed a series of street cross section concepts that could be applied to each of the street concept types described in Figure 1.

The project team will provide the Commission with a summary of December's community presentation and include community feedback received after December's resident meeting. This information, along with input from the Transportation Commission, will be used to inform final recommendations that will be presented to City Council.

Study Area:

This study recommends different standards for sections of Holmes Point Drive and sections of residential neighborhood streets. The segments include:

Holmes Point Drive

- Entry segments
- Residential segments
- OO Denny Park segment

Neighborhood Streets

- Streets with existing and proposed segments of curb, gutter, and sidewalk
- 24 to 28' nominal pavement widths
- 20 to < 24' nominal pavement widths

January 26, 2022 - Transportation Commission Meeting 3



Figure 1. Study Area and Street Concept Types

January 26, 2022 - Transportation Commission Meeting 3

Major Themes and Comments

- Do the dimensions for 6-8 foot mixed use width continuous, or do poles/signs encroach on the area? Minor encroachments exist.
- Comparison to Juanita drive treatment, no candlesticks are suggested, mentioned that treatments should be looked at closer in design on the uphill side of Holmes Point Drive.
- What is the current lane width? 10 to 11+ feet, varies along the drive. Has a wide shoulder, not many significant changes were suggested here.
- Currently the shoulder is not wide enough on uphill sides for bikes; lanes proposed to be reduced to increase non-motorized space.
- Question about garbage pickup and mail delivery. Answer) it's a shared space, all improvements are within the ROW.
- People angle park at the north end of the park, and that we should prevent people from doing this, make it parallel only, so that people don't walk in front of parked cars. Otak indicated that they would modify the image near the 68th intersection to include the pedestrian pathway.
- Could include a pathway near OO Denny, possibly permeable pavers. Potential avenue to generate park donation, people can donate to purchase a paver that has their name on it with a fish stamp/art, etc.
- Acknowledgment, pulled off a good optimization with many constraints through the corridor, compromised not conflicting suggestions, creates a viable path.
- Looking forward to hearing what Council has to say about the final report.



APPENDIX B

Toolbox Solution Concepts

Toolbox Solution Concepts

Overview

A toolbox of solutions and ideas is provided with this submittal that gives a description and example photos of design elements that are presented in the Holmes Point Overlay Area recommendations. These solution concepts can be used independently or in conjunction with each other. Street section concepts have been developed that show how some of these solutions can be used together to create a safer environment for all road users.

1. Speed Management/Traffic Calming Solutions

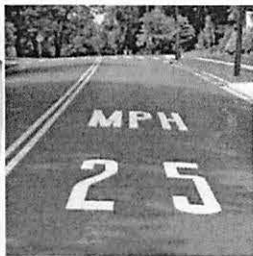
2. Pedestrian and Bicycle Solutions

3. Parking Management Solutions

1. Speed Management/Traffic Calming Concepts

1.1 Reduce Speed Limits

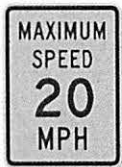
Reduce the speed limit from 25 mph to 20 mph. This could be a simple signage solution, but would need to be used along with improvements in recommendations and potentially enforcement in the early phases of implementation. Signage alone would not be effective without the physical improvements and enforcement.



Speed limit signs can also be painted on the roadway

Signage solutions such as these would be applicable for all types of streets within the study area.

Increased signage could be an interim solution while other physical street improvements are installed through infill development.



Examples of different speed reduction signs

1.2 Designate a "Neighborhood Slow Zone"

Add to the existing sense of community by designating a "Neighborhood Slow Zone" which would lower the speed limits on all streets to 20 mph and incorporate new distinct signage. This solution concept would add to the sense of place, would include signage and define an entrance to Holmes Point Overlay Zone. Signage could be designed to create a sense of "gateway" to the neighborhood. This solution concept may require a planning process or other neighborhood process to be approved.



Examples of different signage in use



1.3 Install Speed Radar Signs

Install additional speed radar signs in areas where speeding is prevalent, such as around OO Denny Park. This solution concept would need to be used along with other improvement concepts as signing alone is typically not as effective.



One speed radar sign has already been installed along Homes Point Drive and 62nd Ave NE (Photo from Ken Goodwin)



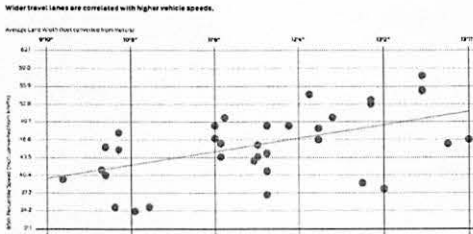
Radar signs can be installed permanently or temporarily

1. Speed Management/ Traffic Calming Concepts

1.4 Narrow Vehicle Lane Width

Existing lane widths along Holmes Point Drive range from 11 to 9 feet wide. Re-stripping for a continuous lane width of 9.5-foot wide in residential areas of Holmes Point Drive would allow for more room to install pedestrian and bicycle facilities. Narrow lanes would also encourage drivers to slow down along high pedestrian use area

Narrow vehicle lanes would be appropriate along any portion of Holmes Point Drive with two striped lanes of traffic.



1.5 Raised Pedestrian Crossing

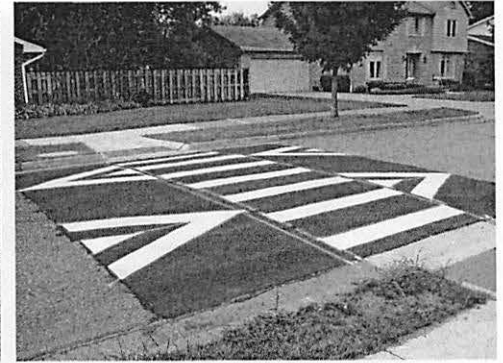
A raised pedestrian crossing consists of a ramped speed table placed at a key location, often a midblock crossing. Paint or paving materials are used to visually draw attention to the crossing. These solutions make pedestrians more prominent in the eyes of drivers and reduce the risk of vehicle collision. When combined with flashing 'pedestrian crossing' signs, raised crossings are especially successful in reducing pedestrian crashes. In addition to protecting pedestrian safety, raised crossings are an effective traffic calming measure, and would work well to slow traffic on Holmes Point Drive.



Example of a raised pedestrian crossing at a vehicle intersection.



Example of a raised pedestrian crossing at a midblock intersection.



Example of a raised pedestrian crossing at a midblock intersection.

2. Pedestrian and Bicycle Solutions

2.1 Protected Shoulder

A protected shoulder for pedestrians and bicyclists would add physical separation between users and vehicles. A curb placed between the shoulder and roadway creates a physical barrier and prevents unwanted street parking in the mixed-use shoulder.



Example of a curb protected walkway in Seattle, Washington



Example of a curb protected shoulder on 84th Ave NE in Kirkland, Washington

2.2 Separated Path

A separated pathway, pulled away from the roadway and with an intermediate planting buffer/strip, would be a low impact way to providing more continuous access for pedestrians. This is a better fit for some areas in the Holmes Point Neighborhood than more urban looking concrete curb, gutter, and sidewalk solutions. The path would be constructed at grade and designed to meander around mature trees and other features. The planting buffers/strips could be used as drainage swales and filter areas (green stormwater infrastructure solutions).



Examples of a 5 foot wide separated pathway in Kirkland, WA

2.3 Mixed-Use Shoulder

A mixed-use concrete shoulder can be installed to create visual separation between vehicle space and pedestrian space on a roadway, where there is insufficient width to accommodate a sidewalk/pathway. Using a paving material, such as concrete, that contrasts with the roadway asphalt will help draw visual attention to the shoulder. Motorists may only enter the shoulder when no pedestrians and bicyclists are present and must overtake these users with caution due to potential oncoming traffic.



Example of an advisory shoulder using paint as a visual buffer

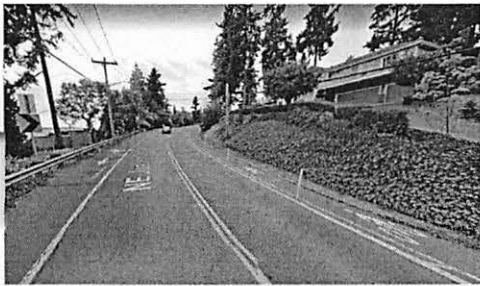


Example of a shoulder separated from the roadway with a rumble strip

2. Pedestrian and Bicycle Solutions

2.4 Climbing Lanes

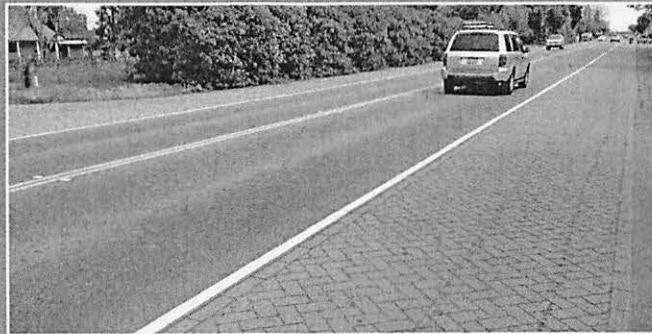
For areas of the subarea where there are significant changes in grade, a buffered mixed-use lane in the uphill direction on Holmes Point Drive is recommended to give bikes room to slow down while going up. Because of grade changes, bikes are recommended to share the roadway on downhill portions.



Buffered climbing lanes have already been installed along uphill portions of Juanita Drive (above).

Alternate Materials and Markings

Use of permeable pavement or unit pavers for pathways, shoulders, or parking areas adjacent to the park could be used to mitigate drainage impacts, based on levels of support from City maintenance procedures and community interest.



Example of an Advisory Shoulder installed using pavers to indicate the pedestrian space.

Shoulders and crosswalks can also be painted to signify their use as pedestrian spaces. Painting shoulders can also act as a community building exercise.



Example of an community painted crosswalk and shoulder.

3. Parking Management

3.1 Managing Street Parking

Most streets within the Subarea prioritize vehicles parking over dedicated pedestrian facilities such as sidewalks or paved shoulders. Many streets do not have room to incorporate both pedestrian facilities and street parking without needing to be widened. New street parking will not be proposed in recommendations. Instead, concepts provide options for maintaining most street parking where it currently exists along Holmes Point Drive and in wide neighborhood streets. Additionally, parking along the west side only of Holmes Point Drive at OO Denny park will be maintained in order to provide visitor access. These strategies will maintain incremental parking opportunities for residents and visitors while prioritizing pedestrian safety and connectivity.



On street parking displaced to accommodate a buffered bike lane

3.2 Alternative Parking Signage

Set aside a few spaces at the park for shared transportation options such as high occupancy vehicle, ride share or vanpool. The addition of short term loading and unloaded spaces also would give more options for visitors to be dropped off at the park without needing to park a single occupancy vehicle.

Alternative parking signage would be an appropriate parking management solution at OO Denny Park.



Examples of alternate parking signage



APPENDIX C
Recommendations:
Cross Section Diagrams and Maps

Residential Area - Holmes Point Drive: Recommendations

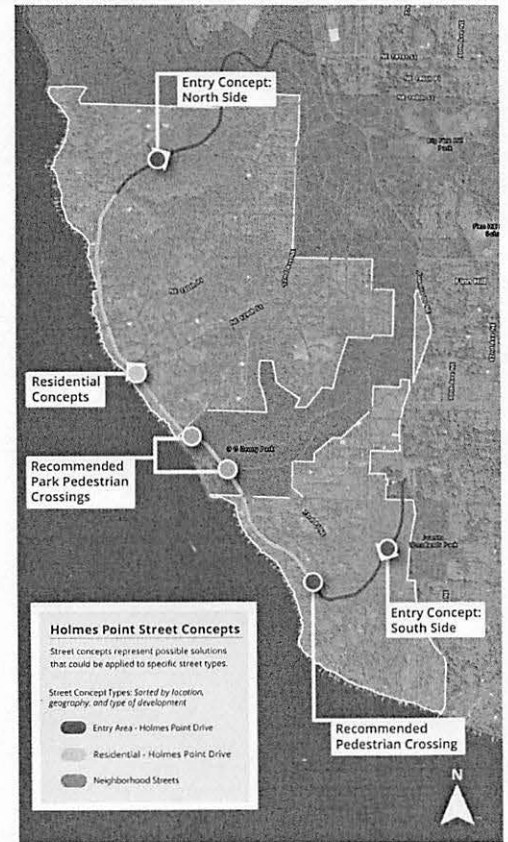
Street Typologies

In order to develop site-specific recommendations, the Holmes Point Overlay Zone was categorized by street type. The two main categories were Holmes Point Drive/76th PI NE and neighborhood streets. Subcategories for the Holmes Point Drive Corridor are listed below:

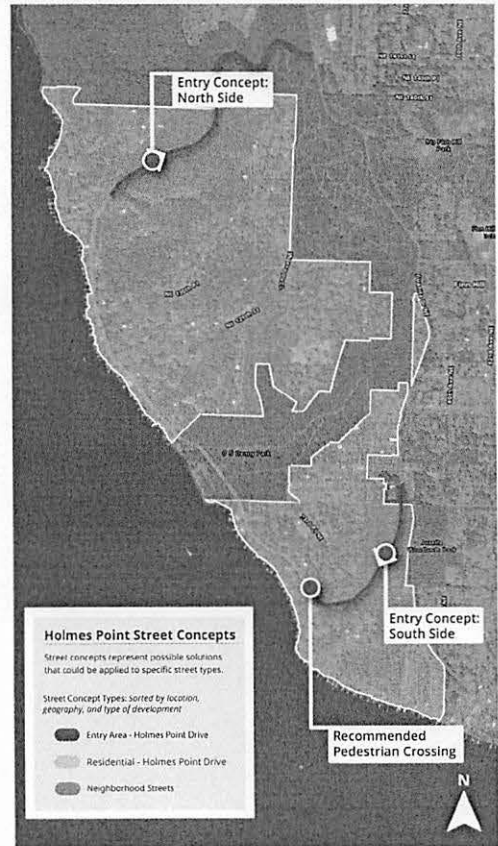
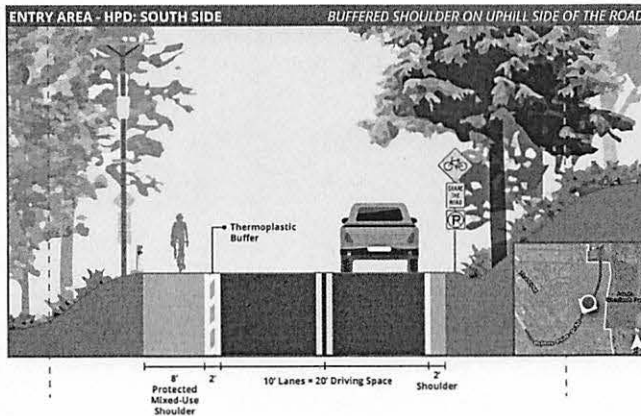
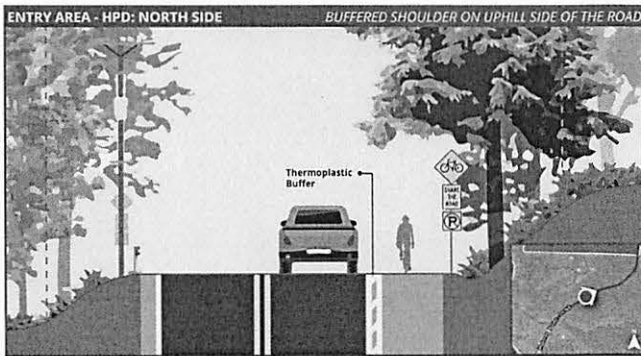
Holmes Point Drive Corridor

- **Entry Areas** - These areas occur at either end of the Holmes Point Drive loop where the corridor rises to meet intersections with Juanita Drive NE. These areas are heavily wooded, with steeper curving roadway grades and minimal private development along the corridor.
- **Residential Areas** - This area provides access to homes located along the Lake Washington waterfront. This section of Holmes Point Drive corridor is relatively level or flat in grade and more densely populated with homes, driveways, and other residential features and structures. This area is relatively straight.
- **OO Denny Park** - This portion of the Holmes Point Drive corridor passes through OO Denny Park, with public open space on both sides of the corridor and no private development. Similar to the residential area, this section is also relatively level in grade and includes minimal curvature. This area tends to be the most congested area for all modes of transport and currently provides space for parking along the street for those accessing the park.

The recommended standards are intended to be applied with flexibility to preserve trees (such as pathway locations that can be separated from the roadway and/or can be designed to meander) and in consideration of encroachments that may not be feasible to remove.

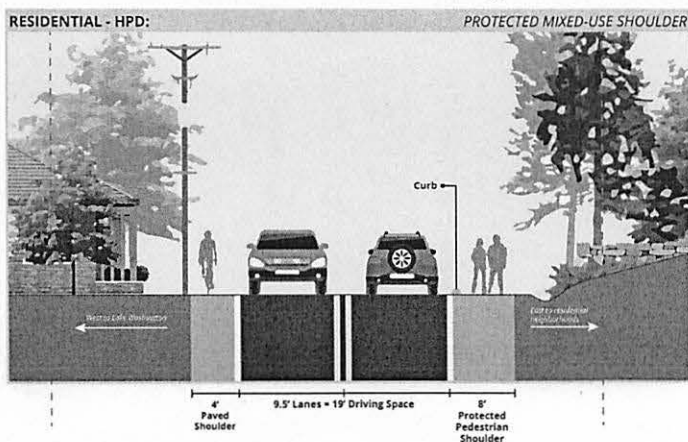


Entry Area - Holmes Point Drive: Recommendations

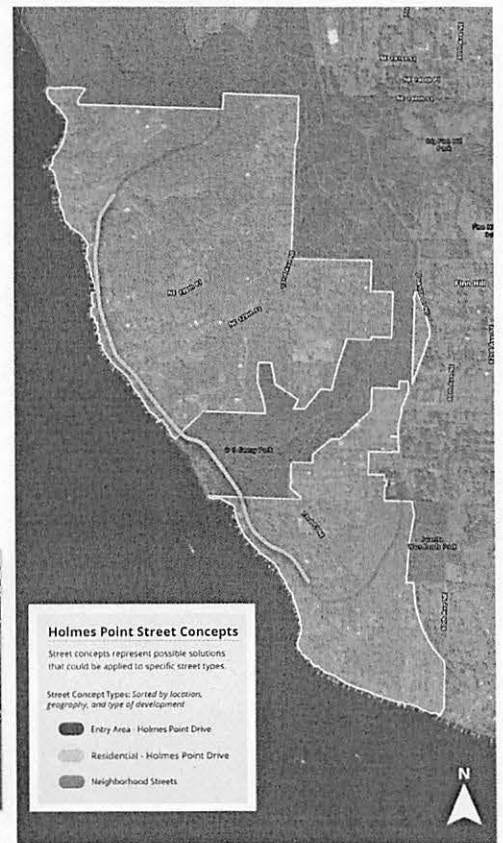
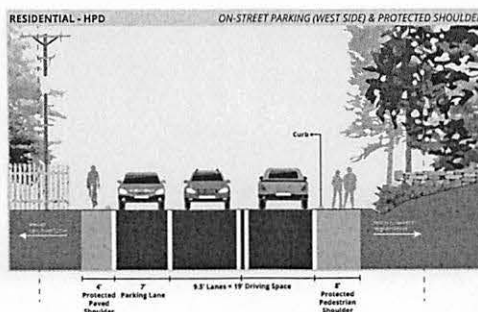
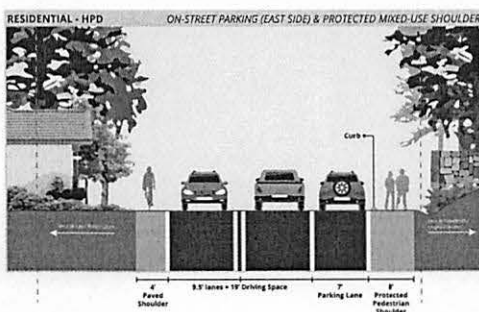


Residential Area - Holmes Point Drive: Recommendations

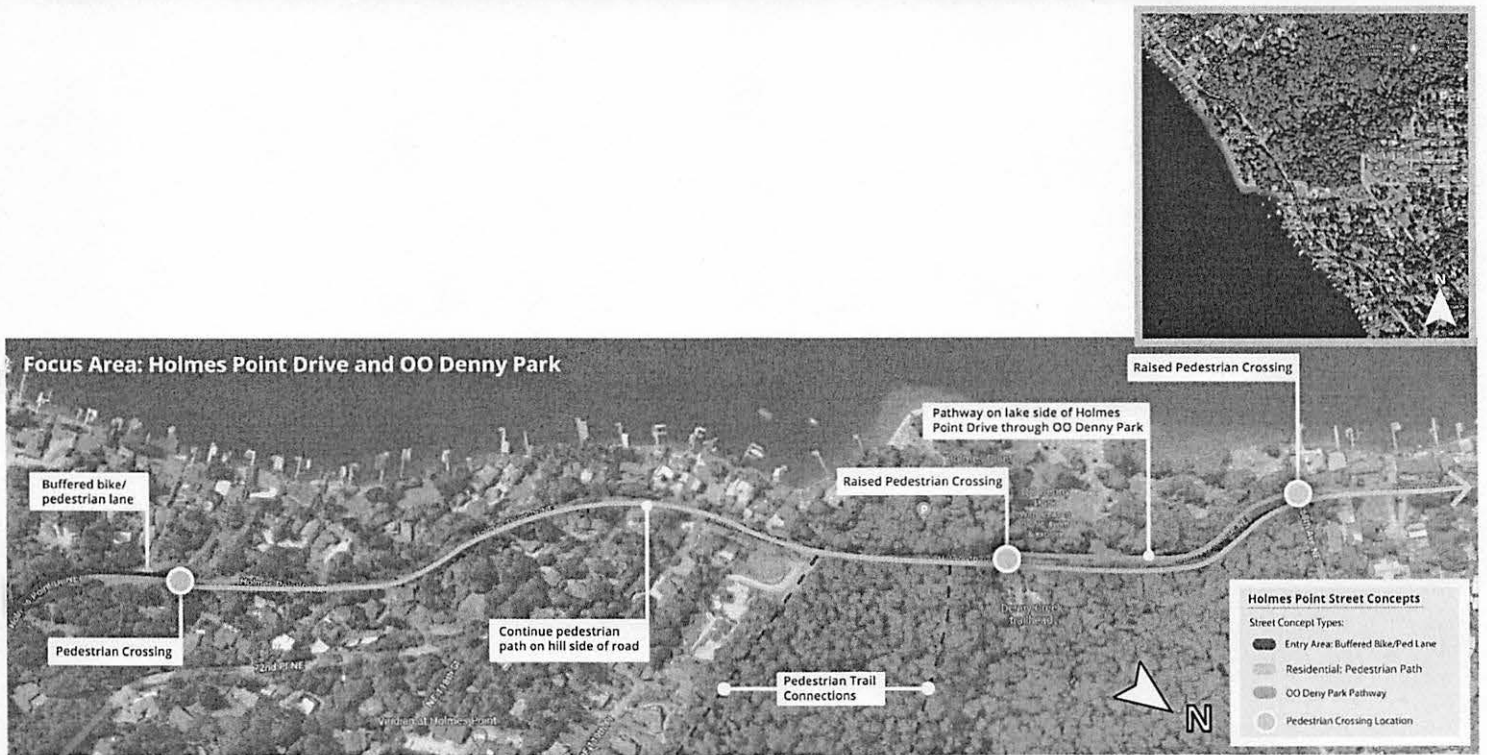
Recommendation Without Parking for Residential Area



Recommendations with Parking for Residential Area



OO Denny Park Frontage - Holmes Point Drive: Recommendations



Neighborhood Street Typologies

Neighborhood Streets Classification:

Streets with Existing and/or Proposed Segments of Curb, Gutter, and Sidewalk:

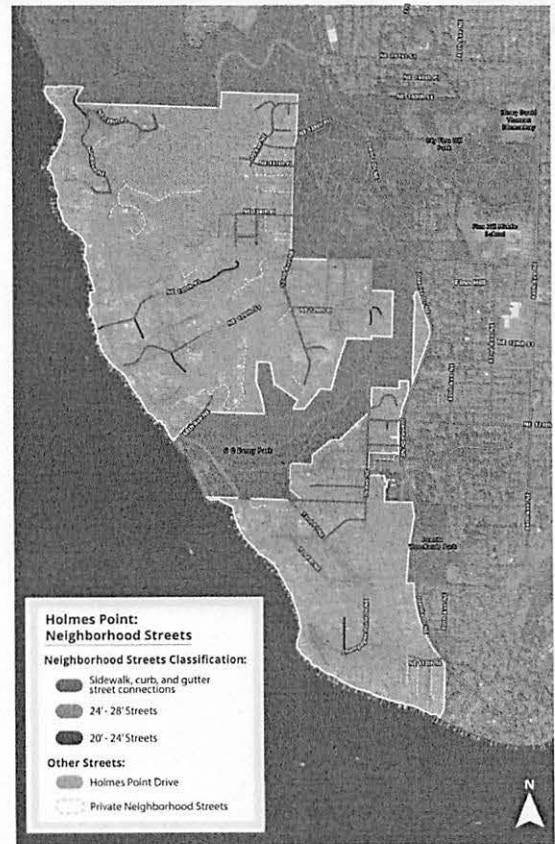
- **Recommendation:** Maintain or continue this standard (with sidewalk on at least on one side of street)

24' to 28' Nominal Pavement Width (Depending on Width):

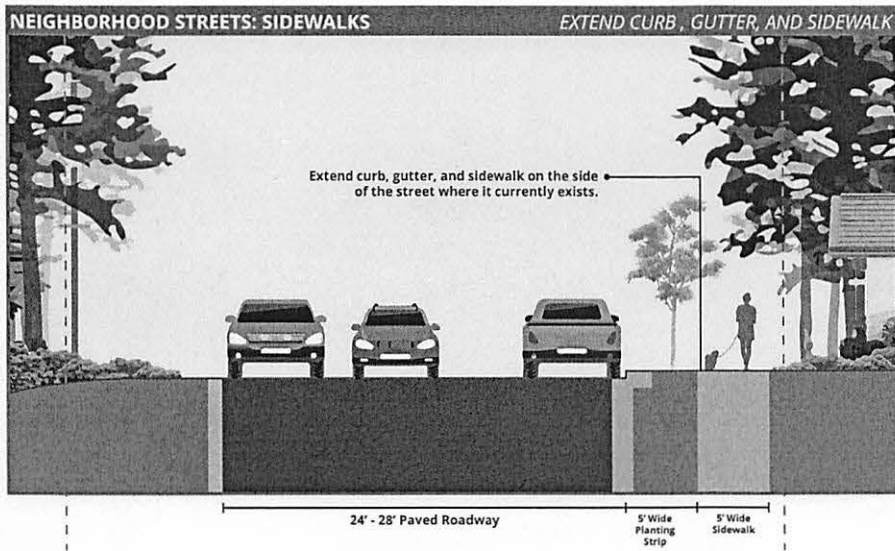
- **Recommendation A:** Separated mixed-use pathway with planter buffer
- **Recommendation B:** Concrete mixed-use shoulder

20' to 24' Nominal Pavement Width:

- **Recommendation:** Shared roadway for vehicles and pedestrians

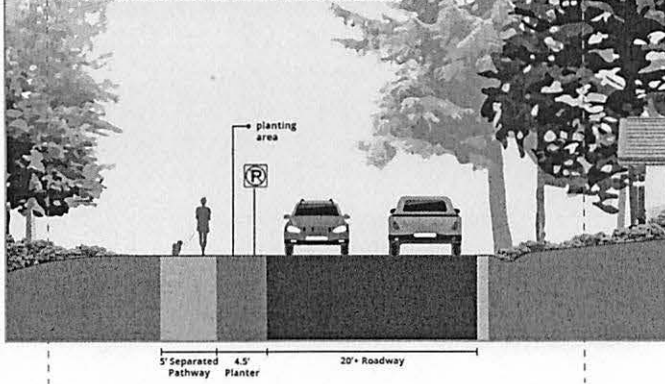


Neighborhood Streets - Sidewalk, Curb, and Gutter Street Connections: Recommendation

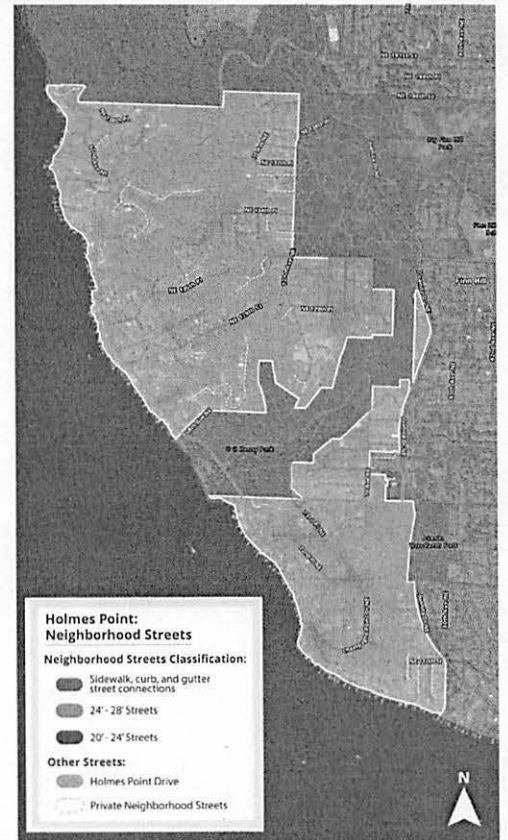
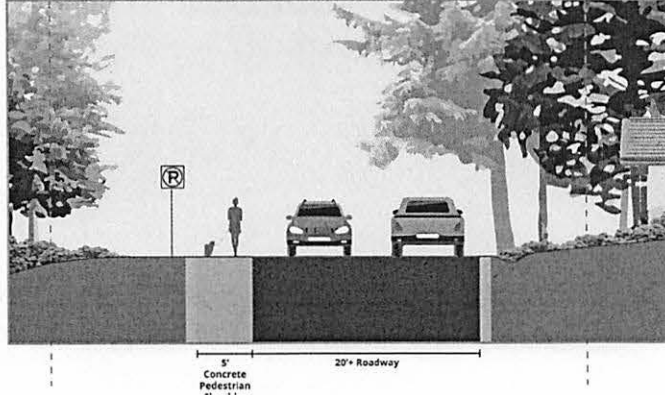


Neighborhood Streets - 24' - 28' Nominal Pavement Width: Recommendations

NEIGHBORHOOD STREETS: 24' - 28' WIDTH RECOMMENDATION A: SEPARATED PATHWAY FOR PEDESTRIANS & BICYCLISTS



NEIGHBORHOOD STREETS: 24' - 28' WIDTH RECOMMENDATION B: CONCRETE SHOULDER FOR PEDESTRIANS & BICYCLISTS



Neighborhood Streets - Sidewalk, Curb, and Gutter Street Connections: Recommendation

